



AGENDA

CITY COUNCIL CLOSED SESSION AND REGULAR MEETING OF THE CITY OF COACHELLA

THE COUNCIL SITTING AS THE COACHELLA SANITARY DISTRICT,
COACHELLA FIRE PROTECTION DISTRICT, COACHELLA FINANCING AUTHORITY,
COACHELLA EDUCATIONAL AND GOVERNMENTAL ACCESS CABLE CHANNEL CORPORATION,
COACHELLA WATER AUTHORITY, AND SUCCESSOR AGENCY TO THE COACHELLA REDEVELOPMENT AGENCY

October 23, 2019

5:00 PM Closed Session

6:00 PM Regular Meeting

CALL TO ORDER: - 5:00 P.M.

ROLL CALL:

APPROVAL OF AGENDA:

“At this time the Council/ Board/Corporation/Authority may announce any items being pulled from the Agenda or continued to another date or request the moving of an item on the agenda”

PUBLIC COMMENTS (CLOSED SESSION ITEMS):

ADJOURN TO CLOSED SESSION:

1. Public Employee Performance Evaluation, Pursuant to Government Code Section 54957(b)(1)
Council Appointed Position – City Manager

RECONVENE REGULAR MEETING: - 6:00 P.M.

PLEDGE OF ALLEGIANCE:

CLOSED SESSION ANNOUNCEMENTS:

APPROVAL OF THE MINUTES:

2. Regular Meeting Minutes of October 9, 2019, of the City Council, Coachella Fire Protection District, Coachella Sanitary District, Coachella Financing Authority, Coachella Educational and Governmental Access Cable Corporation, Coachella Financing Authority, Coachella Education and Governmental Access Cable Corporation, Coachella Water Authority, and Successor Agency to the Coachella Redevelopment Agency.

PROCLAMATIONS/PRESENTATIONS:

- [3.](#) Imagine a Day Without Water
4. Public Private Partnerships in the Downtown

WRITTEN COMMUNICATIONS:

CONSENT CALENDAR:

(It is recommended that Consent Items be acted upon simultaneously unless separate discussion and/or action is requested by a Council Member or member of the audience.)

- [5.](#) Voucher Listings — Manual Checks/Utility Billing Refunds/FY2019-20 Expenditures as of October 23, 2019, \$986,462.93.
- [6.](#) Quarterly Reports
- [7.](#) Notice of Completion for City Project Number ST-106, Coral Mountain Academy Traffic Signal Project
- [8.](#) Authorize execution of Amendment No. 1 to West Coast Arborist Maintenance Agreement extending their term for one year.
- [9.](#) Fee Waiver Request for Veterans Park Stage and Special Event Fee for the Confronting Climate Change Forum
- [10.](#) Authorize Beer Garden to be operated at Dateland Park for the Synergy Festival, on November 9, 2019 from 11:00 a.m. to 8:00 p.m.
- [11.](#) Investment Report – August 2019
- [12.](#) Reimbursement Agreement with Cabazon Band of Mission Indians for the Vista Del Norte Pavement Rehabilitation Project No. ST-121

NEW BUSINESS CALENDAR (LEGISLATIVE AND ADMINISTRATIVE):

- [13.](#) Resolution No. 2019-58, Appeal of Planning Commission's denial of Time Extension Request for Tentative Tract Map No. 31978 (Bellissima) for the subdivision of approximately 38 acres into 158 single-family residential lots located at the south east corner of Avenue 53 and Frederick Street. Coachella Investments, LLC, Appellant.
- [14.](#) Resolution No. 2019-59, Pueblo Viejo Implementation Strategy Plan

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

15. Zona Central – Downtown Expansion

- a) Resolution No. 2019-57 to adopt a Negative Declaration pursuant to California Environmental Quality Act Guidelines for the Zona Central - Change of Zone (CZ 18-10) project on approximately 206 acres of land located on the west side of Tyler Street between Avenue 50 and ¼ mile north of Avenue 52.
- b) Ordinance No. 1143 approving the City-Initiated Change of Zone (CZ 18-10) from M-S (Manufacturing Service) and R-S (Single Family Residential) to MS-IP (Manufacturing Service – Industrial Park); C-N (Neighborhood Commercial); CN-PD (Neighborhood Commercial Planned Development); R-M (Residential, Multi-Family); O-S (Open Space); and OS-PF (Open Space Public Facilities) on approximately 206 acres of partially developed land on the west side of Tyler Street between Avenue 50, and ¼ mile north of Avenue 52. *1st Reading.*

PUBLIC COMMENTS (NON-AGENDA ITEMS):

The public may address the City Council/Board/Corporation/ Authority on any item of interest to the public that is not on the agenda but is in the subject matter jurisdiction thereof. Please limit your comments to three (3) minutes.

REPORTS AND REQUESTS:

Council Comments/Report of Miscellaneous Committees.

City Manager's Comments.

ADJOURNMENT:

*ⁱAny writing or documents pertaining to an **open session** item provided to a majority of the City Council less than 72 hours prior to the meeting, shall be made available for public inspection at the City Clerk's office at 53-462 Enterprise Way, Coachella, CA during normal business hours.*

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES



MINUTES

CITY COUNCIL CLOSED SESSION & REGULAR MEETING

OF THE CITY OF COACHELLA

THE COUNCIL SITTING AS THE COACHELLA SANITARY DISTRICT,
COACHELLA FIRE PROTECTION DISTRICT, COACHELLA FINANCING AUTHORITY,
COACHELLA EDUCATIONAL AND GOVERNMENTAL ACCESS CABLE CHANNEL CORPORATION,
COACHELLA WATER AUTHORITY, AND SUCCESSOR AGENCY TO THE COACHELLA REDEVELOPMENT AGENCY

October 09, 2019

5:00 PM Closed Session

6:00 PM Regular Meeting

CALL TO ORDER: - 5:00 P.M.

The Regular Meeting of the City Council of the City of Coachella was called to order at 5:00 p.m. in the Council Chamber at City Hall by Mayor Steven Hernandez.

ROLL CALL:

Present: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez, and Mayor Hernandez.

Absent: None.

APPROVAL OF AGENDA:

There were no modifications to the agenda.

PUBLIC COMMENTS (CLOSED SESSION ITEMS):

None.

ADJOURN TO CLOSED SESSION:

City Attorney Carlos Campos asked Council to convene into Closed Session at 5:00 p.m. to discuss the following item:

1. CONFERENCE WITH LEGAL COUNSEL - EXISTING LITIGATION
Pursuant to Government Code Section 54956.9(d)(1)
Ramona Rita Morales v. City of Coachella, et al.
Superior Ct. of California, County of Riverside, Case No. RIC 1803060

RECONVENE REGULAR MEETING: - 6:00 P.M.

The City Council reconvened into open session at 6:00 p.m.

PLEDGE OF ALLEGIANCE:

The Pledge of Allegiance was led by City Treasurer Arturo Aviles.

CLOSED SESSION ANNOUNCEMENTS:

City Attorney Campos stated that direction was given in Closed Session, but no reportable action was taken.

APPROVAL OF THE MINUTES:

2. Special Meeting Minutes of a Coachella City Council Study Session held on September 25, 2019.
3. Regular Meeting Minutes of September 25, 2019, of the City Council, Coachella Fire Protection District, Coachella Sanitary District, Coachella Financing Authority, Coachella Educational and Governmental Access Cable Corporation, Coachella Water Authority, and Successor Agency to the Coachella Redevelopment Agency.

Motion: To approve the minutes as presented.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Beaman Jacinto
Approved: 4-0, by a unanimous voice vote

PROCLAMATIONS/PRESENTATIONS:

None.

WRITTEN COMMUNICATIONS:

None.

CONSENT CALENDAR:

4. Voucher Listing — FY2019/20 Expenditures as of October 9, 2019, \$1,213,182.09.
5. Resolution No. 2019-51 establishing New Selection Criteria to be used during the review of Conditional Use Permits for Cannabis Retail and Retail Microbusinesses (Round #2) within various Subzone #1 (Pueblo Viejo), #3 (Dillon Road), #4 (Wrecking Yard), or #5 (Industrial Park) of the City.
6. Amendment No. 2 to Professional Service Agreement with Southern California Soils and Testing (SCST) to provide Materials testing and Inspection Service for Various City Capital Projects, for an additional amount of \$30,000.00, and extending the term an additional year.
7. Approval of a \$5,400.00 Sponsorship for the Date Harvest Festival Ferris Wheel.

8. Authorization for the City Manager to execute a Library Facilities Use Agreement and Amendment #1 with Desert Community College District.
9. Award of a Professional Services Agreement to GM Business Interiors for furnishing the Senior Center facility in the amount not to exceed \$50,000.00; allowing for non-substantive changes by the City Attorney.
10. Award of a construction contract to Valley Pipeline Services Inc., for Dillon Bridge Pipeline Repair City Project W-40 in the amount of \$133,298.00 including 10% contingency.
11. Approval a Community Based Grant to Angel View in the Amount of \$1,000.00 to support its Outreach Program for Children.
12. Resolution No. 2019-53, a Resolution of the City Council of the City of Coachella, California, approving Final Parcel Map No. 37670.
13. Gas Tax Account Budget Adjustment

Motion: To approve per staff recommendation, Consent Calendar Items 4 through 13.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Beaman Jacinto
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

NEW BUSINESS CALENDAR (LEGISLATIVE AND ADMINISTRATIVE):

14. Resolution No. 2019-52 approving the Ratification of a Two Year Memorandum of Understanding Agreement between the City of Coachella and Teamsters Local 1932, representing the Sanitary and Miscellaneous Employees Bargaining Unit.

Motion: To approve per staff recommendation.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Beaman Jacinto
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

15. Approval of a Professional Services Agreement with KOA Corporations, Inc. for an amount of \$545,360 to provide PS&E and Right-of-Way Services for the Improvements of Avenue 50 from Calhoun Street to Cesar Chavez Street - City Project ST-93.

Motion: To approve per staff recommendation.

Made by: Councilmember Beaman Jacinto
Seconded by: Mayor Pro Tem Martinez
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

16. Approval of a Construction Contract with Onyx Paving Company Inc. for the construction of speed humps for Traffic Calming Phase III, City Project ST-124, in the amount of \$137,000.00, plus 10% contingency.

Motion: To approve per staff recommendation.

Made by: Councilmember Beaman Jacinto
Seconded by: Councilmember Gonzalez
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

17. Award of Construction Contract with Granite Construction for the construction of the Avenue 50 Storm Drain, City Project SD-02, for an amount of \$527,448.00, plus contingency.

Motion: To approve per staff recommendation.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Bautista
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

18. Award of Construction Contract with Matich Corporation in the amount of \$2,390,000.00 and an amount of \$239,000.00 for contingency for the construction of The Active Transportation Program (ATP) Cycle 2, City Project No. ST-100.

Motion: To approve per staff recommendation.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Beaman Jacinto
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

19. Award to Eventscape International, Inc. a Sponsorship in the amount of \$25,000.00 for the 2019 Run With Los Muertos Event.

Motion: To approve a \$25,000.00 sponsorship.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember bautista
Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
NOES: None.
ABSTAIN: None.
ABSENT: None.

20. Authorization for the City Manager to execute a Professional Service Agreement with NV5 to develop final plans, specifications & estimates for the Grapefruit Boulevard Urban Greening and Connectivity Project No. ST-123, in an amount not to exceed \$330,000.00, which includes a 10% contingency.

(Councilmember Bautista stepped away from the dais at 7:23 p.m.)

Motion: To approve per staff recommendation.

Made by: Mayor Pro Tem Martinez
Seconded by: Councilmember Beaman Jacinto
Approved: 4-0, by a unanimous roll call vote:

AYES: Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
 NOES: None.
 ABSTAIN: None.
 ABSENT: Councilmember Bautista.

(Councilmember Bautista returned to the dais at 7:25 p.m.)

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

21. Resolution No. 2019-50 Authorizing the City Manager to Submit and Execute a 2020-2021 Community Development Block Grant (CDBG) Entitlement Funding Application in the Amount of \$395,000.00 and Hold a Public Hearing for the Proposed Activity: Bagdouma Park Basketball Courts Rehabilitation Project.

(Although approval took place at a previous meeting, this item was resubmitted for Council's action due to a public hearing noticing error.)

Mayor Hernandez opened the Public Hearing for Item 21 at 7:25 p.m.

Public Comment: None.

Mayor Hernandez closed the Public Hearing for Item 21 at 7:26 p.m.

Motion: To approve per staff recommendation.

Made by: Mayor Pro Tem Martinez
 Seconded by: Councilmember Beaman Jacinto
 Approved: 5-0, by a unanimous roll call vote:

AYES: Councilmember Bautista, Councilmember Beaman Jacinto, Councilmember Gonzalez, Mayor Pro Tem Martinez and Mayor Hernandez
 NOES: None.
 ABSTAIN: None.
 ABSENT: None.

PUBLIC COMMENTS (NON-AGENDA ITEMS):

- a. Robert Mueller
- b. Christopher Martinez
- c. Sylvia Montenegro
- d. Feliz Núñez
- e. Brad Anderson

REPORTS AND REQUESTS:

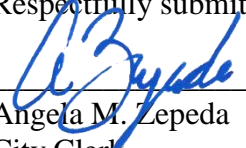
Council Comments/Report of Miscellaneous Committees.

City Manager's Comments.

ADJOURNMENT:

There being no further business to come before the City Council and the Agencies, Mayor Hernandez adjourned the meeting at 8:11 p.m.

Respectfully submitted,



Angela M. Lepeda
City Clerk

CITY OF COACHELLA CALIFORNIA

Proclamation

WHEREAS, the infrastructure that brings 45,839 residents an average of 104 gallons of water per person, per day, and then safely returns water to the environment is essential to the quality of life, environment and economic vitality of the City of Coachella; and

WHEREAS, a day without water would be a public health and safety crisis due to the impacts which would limit the abilities of safety personnel such as firefighters and hospitals staff to do their jobs, as well as businesses and homes to function; and

WHEREAS, Whereas, America's water infrastructure is aging and failing—and there are communities that have never had access to infrastructure in the first place; and

WHEREAS, living without water is an economic crisis, considering a single nationwide day without water service would put \$43.5 billion in economic activity at risk; and

WHEREAS, demographic changes and climate pressures, including natural disasters like droughts, flooding, and wildfires, threaten our infrastructure and increase the possibility of a day without water; and

WHEREAS, our water infrastructure supports every facet of our daily lives, but is facing unprecedented challenges; and

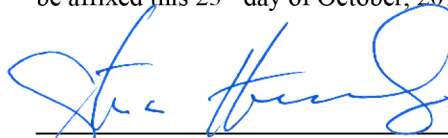
WHEREAS, these challenges look different to different communities and will require local solutions, but it's clear that reinvestment in our water systems must be a national priority; now;

NOW THEREFORE, I, Steven A. Hernandez, Mayor of the City of Coachella, by the power vested in me, do hereby recognize October 23, 2019, as

Imagine a Day Without Water

and the City of Coachella recognizes water is essential to the quality of life and economic competitiveness and acknowledges the importance of educating the public about the value of water through the "Imagine a Day Without Water" campaign.

IN WITNESS HEREOF, I have hereunto set my hand and caused the official seal of the City of Coachella, California to be affixed this 23rd day of October, 2019.



Steven A. Hernandez, Mayor
City of Coachella, California

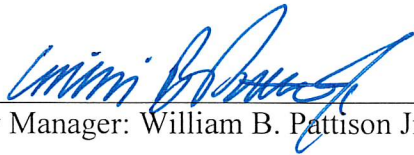
Bank : wfb WELLS FARGO BANK

<u>Check #</u>	<u>Date</u>	<u>Vendor</u>	<u>Invoice</u>	<u>Inv Date</u>	<u>Description</u>	<u>Amount Paid</u>	<u>Check Total</u>
106416	10/7/2019	53454	CENTURY LAW GROUP LLP CSettlement	10/7/2019	SETTLEMENT- AVE 50 EXTEN	7,810.00	7,810.00
106417	10/7/2019	48830	HUITRON CONSTRUCTION C(00228	10/7/2019	INSTLL'D PLASTER ON EXTE	3,640.00	3,640.00
Sub total for WELLS FARGO BANK:							11,450.00

2 checks in this report.

Grand Total All Checks: 11,450.00

Date: October 7, 2019



City Manager: William B. Pattison Jr.



Controller: Javier Estrada

Bank : wfb WELLS FARGO BANK

<u>Check #</u>	<u>Date</u>	<u>Vendor</u>	<u>Invoice</u>	<u>Inv Date</u>	<u>Description</u>	<u>Amount Paid</u>	<u>Check Total</u>	
106418	10/9/2019	53448	ACUNA, VIRGINIA	Ref000207374	10/3/2019	UB Refund Cst #00050985	80.13	80.13
106419	10/9/2019	53438	ALVAREZ, BENJAMIN	Ref000207364	10/3/2019	UB Refund Cst #00042158	71.65	71.65
106420	10/9/2019	53442	BARRIOS, MARIA	Ref000207368	10/3/2019	UB Refund Cst #00047275	28.97	28.97
106421	10/9/2019	53436	CABRERA, SONIA	Ref000207362	10/3/2019	UB Refund Cst #00038767	42.62	42.62
106422	10/9/2019	53439	CAMARA, ALBER	Ref000207365	10/3/2019	UB Refund Cst #00043314	19.25	19.25
106423	10/9/2019	53440	CORONA, JULIO	Ref000207366	10/3/2019	UB Refund Cst #00045065	59.07	59.07
106424	10/9/2019	53445	DR HORTON	Ref000207371	10/3/2019	UB Refund Cst #00050979	59.79	59.79
106425	10/9/2019	53446	DR HORTON	Ref000207372	10/3/2019	UB Refund Cst #00050981	51.16	51.16
106426	10/9/2019	53447	DR HORTON	Ref000207373	10/3/2019	UB Refund Cst #00050984	67.23	67.23
106427	10/9/2019	53449	DR HORTON	Ref000207375	10/3/2019	UB Refund Cst #00051026	81.27	81.27
106428	10/9/2019	53450	DR HORTON	Ref000207376	10/3/2019	UB Refund Cst #00051028	78.91	78.91
106429	10/9/2019	53443	ESPARZA, MIREYA	Ref000207369	10/3/2019	UB Refund Cst #00049375	11.55	11.55
106430	10/9/2019	53451	GARCIA, REYNALDO	Ref000207377	10/3/2019	UB Refund Cst #00051039	98.07	98.07
106431	10/9/2019	53437	GUTIERREZ, JAZMIN	Ref000207363	10/3/2019	UB Refund Cst #00041790	29.76	29.76
106432	10/9/2019	53452	MPSN PROPERTIES II, LP #92	Ref000207378	10/3/2019	UB Refund Cst #00051069	98.55	98.55
106433	10/9/2019	53453	NAVARRO, MARIA	Ref000207379	10/3/2019	UB Refund Cst #00051077	89.87	89.87
106434	10/9/2019	53444	NAVARRO, MIGUEL	Ref000207370	10/3/2019	UB Refund Cst #00050848	56.30	56.30
106435	10/9/2019	53441	WALKER, CASSANDRA	Ref000207367	10/3/2019	UB Refund Cst #00046485	30.12	30.12

Sub total for WELLS FARGO BANK: 1,054.27

18 checks in this report.

Grand Total All Checks: 1,054.27

Date: October 9, 2019



City Manager: William B. Pattison Jr.



Controller: Javier Estrada

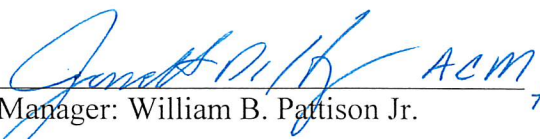
Bank : wfb WELLS FARGO BANK

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106436	10/23/2019	53470	CAPPELLO, MARIE	Ref000207798	10/15/2019 UB Refund Cst #00051018	155.19	155.19
106437	10/23/2019	53469	HAMBURGER, DANIEL	Ref000207797	10/15/2019 UB Refund Cst #00050534	79.71	79.71
106438	10/23/2019	53467	MATHIE, MARLON	Ref000207795	10/15/2019 UB Refund Cst #00049580	32.20	32.20
106439	10/23/2019	53465	ROBLEDO, PERLA	Ref000207793	10/15/2019 UB Refund Cst #00046657	33.49	33.49
106440	10/23/2019	53466	SHELDON, MATHEW	Ref000207794	10/15/2019 UB Refund Cst #00049487	16.39	16.39
106441	10/23/2019	53464	SOTELO, ELIZABETH	Ref000207792	10/15/2019 UB Refund Cst #00044248	64.62	64.62
106442	10/23/2019	53468	USA LAW CENTER, INC	Ref000207796	10/15/2019 UB Refund Cst #00049627	83.08	83.08
Sub total for WELLS FARGO BANK:							464.68

7 checks in this report.

Grand Total All Checks: 464.68

Date: October 23, 2019

 ACM for Bill Pattison.
City Manager: William B. Pattison Jr.


Controller: Javier Estrada

Bank : wfb WELLS FARGO BANK

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106443	10/23/2019	46835	AIR AND HOSE SOURCE, INC. 362426	9/18/2019	4R7-4FNPSM-4MPX37"	31.73	
			362261	9/16/2019	DJ COUPLED ALUM ENDS & T	197.93	229.66
106444	10/23/2019	49813	ALARCON, ALEXANDER Trvl Exp 10/2-4	10/14/2019	TRVL EXP 10/2-4, CACEO COI	20.00	20.00
106445	10/23/2019	51894	ALPHA MEDIA LLC 431127-1	9/30/2019	9/30 AD SPOT: TACOS, TEQU	100.00	100.00
106446	10/23/2019	01436	AMERICAN FORENSIC NURSE 72606	9/30/2019	SEPT2019 BLOOD DRAWS	55.00	55.00
106447	10/23/2019	50161	APWA COACHELLA VALLEY 11/14 Mtg	10/9/2019	11/14 PUBLIC WORKS EXPO:	25.00	25.00
106448	10/23/2019	42837	ARAMARK UNIFORM SERVICE 21845662	9/22/2019	POLY PERF SS POLOS W/ EM	211.86	211.86
106449	10/23/2019	42837	ARAMARK UNIFORM SERVICE SEP2019 GRFT	9/30/2019	PE9/30 UNIFORMS	53.08	53.08
106450	10/23/2019	47955	ARCADIS U.S., INC. 34119301	8/30/2019	PE7/28 SENIOR CENTER EXP	11,281.42	11,281.42
106451	10/23/2019	42251	ARCOS, MARIA Ck 10/23/19	10/2/2019	VOUCHER 69, 8/25-9/20	285.75	285.75
106452	10/23/2019	52968	ARIVITAS PARTNERS, LLC 18-005-08	6/30/2019	MAY-JUNE2019 PLANNING SV	5,535.00	
			18-005-10	8/31/2019	AUG2019 PLANNING SVCS	3,915.00	
			18-005-11	9/30/2019	SEP2019 PLANNING SVCS	6,480.00	15,930.00
106453	10/23/2019	53457	AVENIDA MUSIC LLC 101919	9/30/2019	10/19 PERFORMANCE @ TAC	600.00	600.00

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106454	10/23/2019	45929	BECK OIL, INC.	28356CL	9/15/2019	PE9/15 WATER DEPT FUEL	406.00
				28360CL	9/15/2019	PE9/15 PARKS DEPT FUEL	1,168.83
				28384CL	9/15/2019	PE9/15 VEHICLE MAINT DEPT	46.38
				28385CL	9/15/2019	PE9/15 SENIOR CNTR FUEL	321.22
				28399CL	9/15/2019	PE9/15 CODE ENF DEPT FUE	303.96
				28412CL	9/15/2019	PE9/15 SANITARY DEPT FUEL	598.11
				28421CL	9/15/2019	PE9/15 BLDG MAINT DEPT FL	172.03
				28722CL	9/30/2019	PE9/30 ENG DEPT FUEL	177.18
				28724CL	9/30/2019	PE9/30 LLMD DEPT FUEL	223.40
				28728CL	9/30/2019	PE9/30 STREETS DEPT FUEL	1,396.66
				28730CL	9/30/2019	PE9/30 WATER DEPT FUEL	559.19
				28734CL	9/30/2019	PE9/30 PARKS DEPT FUEL	1,379.79
				28757CL	9/30/2019	PE9/30 VEHICLE MAINT DEPT	195.13
				28461CL	9/15/2019	PE9/15 GRAFFITI DEPT FUEL	294.93
				28833CL	9/30/2019	PE9/30 GRAFFITI DEPT FUEL	343.06
				28347CL	9/15/2019	PE9/15 ENG DEPT FUEL	187.73
				28348CL	9/15/2019	PE9/15 BLDG/PLANNING DEP	44.70
				28350CL	9/15/2019	PE9/15 LLMD DEPT FUEL	191.12
				28354CL	9/15/2019	PE9/15 STREETS DEPT FUEL	755.79
				28758CL	9/30/2019	PE9/30 SENIOR CNTR FUEL	338.63
				28770CL	9/30/2019	PE9/30 CODE ENF DEPT FUE	584.17
				28784CL	9/30/2019	PE9/30 SANITARY DEPT FUEL	753.46
				28793CL	9/30/2019	PE9/30 BLDG MAINT DEPT FL	224.77
				28794CL	9/30/2019	PE9/30 ADMIN DEPT FUEL	125.33
				366640	9/10/2019	SHELL HYDRAULIC S2 68	42.74
				366958	9/16/2019	DYED CARB ULS DIESEL	615.71
				366960	9/23/2019	DYED CARB ULS DIESEL	799.71
							12,249.73

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Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total	
106455	10/23/2019	43462	BEST BEST & KRIEGER, LLP	859352	9/24/2019	PE8/31, #80237, GENERAL RE	33,084.80	
				859353	9/24/2019	PE8/31, #80237.00234, 51937 I	6,286.20	
				859354	9/24/2019	PE8/31, #80237.00441, R. MOF	283.00	
				859355	9/24/2019	PE8/31, #80237.00444, CITY C	1,269.90	
				859356	9/24/2019	PE8/31, #80237.00445, DESEF	2,499.25	
				859357	9/24/2019	PE8/31, #80237.00810, LABOF	84.90	
				859358	9/24/2019	PE8/31, #80237.00820, ENVIR	3,220.00	
				859359	9/24/2019	PE8/31, #80237.00827, LA EN1	4,111.20	
				859360	9/24/2019	PE8/31, #80237.00836, VISTA I	748.80	
				859361	9/24/2019	PE8/31, #80237.00840, CANN/	5,052.40	
				859362	9/24/2019	PE8/31, #80237.00842, COA S.	141.50	
				859363	9/24/2019	PE8/31, #80237.00844, CHROI	1,153.90	
				859364	9/24/2019	PE8/31, #80237.00857, RENEV	877.30	
				859370	9/24/2019	PE8/31, #80237.00858, COA V	2,845.70	
				859371	9/24/2019	PE8/31, #80237.00860, DILLOI	84.90	
				859365	9/24/2019	PE8/31, #80237.00861, ADV CI	13,224.75	
				859366	9/24/2019	PE8/31, #80237.00863, ADV BI	1,490.80	
				859367	9/24/2019	PE8/31, #80237.03001, AV50 P	4,659.98	
				859368	9/24/2019	PE8/31, #80237.03002, AV50 P	2,521.24	
				859369	9/24/2019	PE8/31, #80237.03004, AV50 P	3,576.39	87,216.91
106456	10/23/2019	00836	BIO-TOX LABORATORIES	38473	8/15/2019	LAB SERVICES: 7/12+26	124.00	
				38474	8/15/2019	LAB SERVICE: 7/12	630.00	
				38527	8/15/2019	LAB SERVICE: 7/22	46.00	
				38621	9/18/2019	LAB SERVICES: 8/9+23	213.00	
				38622	9/18/2019	LAB SERVICES: 8/9+23	793.00	
				38674	9/18/2019	LAB SERVICE: 8/26	46.00	1,852.00
106457	10/23/2019	43204	BPS TACTICAL, INC.	19032115	9/17/2019	TACTICAL BALLISTIC VEST C	565.69	565.69
106458	10/23/2019	43634	CACEO	CCEO Rnwl-AA	10/14/2019	2019 CCEO RNWL- A. ALARC	200.00	200.00
106459	10/23/2019	52329	CARRILLO, JUAN	7/9 Reimb	10/1/2019	REIMB FOR WORK BOOTS	39.78	39.78
106460	10/23/2019	53423	CBE OFFICE SOLUTIONS	IN2190838	9/20/2019	ACC CC3502, COLOR COPIEF	866.58	866.58
106461	10/23/2019	53038	CDS OFFICE INTERIORS LLC	750	9/17/2019	CRTDG CLJ 5500/5550 BK, ET	2,468.11	
				763	9/17/2019	GREAT WHITE PAPER	132.72	2,600.83

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Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total	
106462	10/23/2019	02048	CDW GOVERNMENT, INC.	TXR7239	9/17/2019	MS SURFACE PRO TYPE COV	113.98	
				TZN6725	9/19/2019	MAXELL LR6 AA & BROTHER	81.35	
				TZR8643	9/20/2019	MS SURFACE PRO 6 I7 8GB 2	1,452.72	
				TZZ7033	9/23/2019	ENERGIZER MAX 12PK AAA	35.02	1,683.07
106463	10/23/2019	53426	CELL BUSINESS EQUIPMENT	65214942	9/21/2019	SHARP MX5071+MX6071+MXI	611.55	611.55
106464	10/23/2019	07950	CITY OF COACHELLA	Aug 2019	8/31/2019	AUG2019 WATER- ST, PARKS	36,432.91	36,432.91
106465	10/23/2019	52345	COMMERCIAL BANK OF CALIF	Retention	10/15/2019	ESCROW #1605609- PROWE	6,653.41	6,653.41
106466	10/23/2019	44959	COMPUTER CONSULTANTS,	129714	9/21/2019	SEP2019 SVC CALLS	1,584.00	1,584.00
106467	10/23/2019	01924	CONSOLIDATED ELECTRICAL	3298-410273	9/11/2019	PAR20 BULLET, 10W LAMP, E	219.56	
				3298-410344	9/17/2019	26W 4PIN DBL TUBE & VOLTA	55.46	
				3298-410377	9/19/2019	BLK MIGHTY POST & 30W FLI	194.75	
				3298-410378	9/20/2019	MED SOLAR HAT	91.35	
				3298-410402	9/20/2019	30W FLOOD 100WMH EQUAL	275.99	
				3298-410495	9/26/2019	30W FLOOD 100WMH EQUAL	371.34	1,208.45
106468	10/23/2019	51441	CONTRERAS, ABEL	0000012	10/3/2019	10/19 TACOS, TEQUILA & CH/	340.00	340.00
106469	10/23/2019	52375	CORE & MAIN LP	L195550	9/26/2019	12V CAR POWER CABLE	127.08	127.08
106470	10/23/2019	11800	COUNTY OF RIVERSIDE	AN0000001798	10/10/2019	SEP2019 ANML SHLTR+FIELD	20,723.10	20,723.10
106471	10/23/2019	53458	CSI CEJA SECURITY	092119C	9/11/2019	9/21 SECURITY SVCS: EL GRI	3,456.00	3,456.00
106472	10/23/2019	09650	CVAG	Sep2019	10/1/2019	SEP2019 TUMF FEES	8,015.47	8,015.47
106473	10/23/2019	49806	DATA TICKET INC.	105027	9/25/2019	AUG2019 CODE ENF CITATIO	278.33	278.33
106474	10/23/2019	51636	DBX, INC.	R675-RET	9/13/2019	RETENTION- TRAFFIC SIGNA	12,162.95	12,162.95
106475	10/23/2019	52613	DE LEON, MARIBEL	2001	10/3/2019	10/19 PERFORMANCE @ TAC	650.00	650.00
106476	10/23/2019	12870	DEPARTMENT OF JUSTICE	411882	10/4/2019	SEPT2019 BLOOD ALCOHOL,	245.00	
				411936	10/4/2019	AUG2019 BLOOD ALCOHOL A	280.00	525.00
106477	10/23/2019	44836	DESERT CITY GLASS, INC	23103	10/2/2019	INSTLL'D CLEAR DUAL PANE	186.93	186.93
106478	10/23/2019	01089	DESERT ELECTRIC SUPPLY	S2675448.001	9/16/2019	LPV-60-12 60W 90-264V-IN 12	76.13	
				S2671792.001	9/3/2019	EIKO LED15WPT40KMED-G7	43.49	119.62
106479	10/23/2019	52970	DESERT POOL SPECIALISTS,	122797	10/3/2019	FILTER MAINT @ VET/CITY H.	110.00	110.00
106480	10/23/2019	48359	DESERT STEEL SUPPLY	17174	9/24/2019	1X1X3/16"X20' ANGLE	54.38	
				17188	9/25/2019	3/16X1 1/2X20' FLAT BAR, ETC	147.20	201.58

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106481	10/23/2019	49630	DORIS PEREZ INTERPRETING	104607	9/11/2019	9/11 INTERPRETING SVCS: C	350.00	
				104594	9/9/2019	9/9 INTERPRETING SVCS: AG	125.00	
				104606	9/11/2019	9/11 INTERPRETING SVCS: C	350.00	
				104635	9/20/2019	9/20 INTERPRETING SVCS: A	75.00	
				104647	9/25/2019	9/25 INTERPRETING SVCS: C	350.00	
				104648	9/25/2019	9/25 INTERPRETING SVCS: C	350.00	1,600.00
106482	10/23/2019	53462	DURAN, JOSE	PD 10/24	10/15/2019	PD 10/24, CORTECH UNIVER	49.50	49.50
106483	10/23/2019	01969	DV PARTY RENTALS	102619	10/8/2019	10/26 TENT+TBLS+CHAIRS R	1,200.00	1,200.00
106484	10/23/2019	14860	E. K. WOOD LUMBER COMPAN	482774	9/17/2019	HIGH SECURITY/PADLOCK	22.62	
				482815	9/19/2019	GT 4" BRS TWIST NOZZLE, E	38.06	
				482197	8/27/2019	TUBING	0.74	
				482390	9/4/2019	2IN COUPLING, TEFLON TAPI	33.00	
				482228	8/28/2019	100PK SHOAT RINGS	5.38	
				482237	8/28/2019	CLEANOT/PLUG	1.61	
				482631	9/12/2019	MM 24" EPDM RUBB STRAP 8	14.28	115.69
106485	10/23/2019	14700	E. S. BABCOCK & SONS, INC.	BI91027-0076W	9/12/2019	2019 2ND QTR- SLUDGE	1,188.00	1,188.00
106486	10/23/2019	47188	EUROTHERM, INC	903443409	6/13/2019	OBS- MODEL 4103C RECORD	1,903.13	1,903.13
106487	10/23/2019	52416	EVENTSCAPE INTERNATIONAL	ARWLM 2019	10/1/2019	11/2 RUN W/ LOS MUERTOS S	25,000.00	25,000.00
106488	10/23/2019	51141	FENCEWORKS, INC.	A118439	10/7/2019	INSTLL'D TEMP FENCE @ RL	997.76	
				A1118439-1	10/7/2019	TEMP FENCE RELOCATION @	1,824.10	
				118438	8/5/2019	TEMP FENCE RELOCATION @	1,790.52	4,612.38
106489	10/23/2019	48834	FIX-IT COMMERCIAL SERVICE	45036	9/13/2019	RPR'D ICE MACHINE @ CORF	311.00	
				45218	9/24/2019	RPR'D ICE MACHINE @ CORF	709.31	1,020.31
106490	10/23/2019	02272	FRANKLIN TRUCK PARTS, INC	IN302332	9/26/2019	FUEL PUMP & 20PK 3/8" SLEE	183.09	183.09
106491	10/23/2019	51604	FRONTIER	BD 9/16/19	9/16/2019	ACC 209-188-4039-091192-5, 9	172.29	
				3982369-SP19	9/25/2019	760/398-2369, 9/25/19	69.88	
				3983051-OT19	10/1/2019	760/398-3051, 10/1/19	57.15	299.32
106492	10/23/2019	43672	FULTON DISTRIBUTING COM	485652	9/23/2019	GATORADE	262.05	
				485838	9/25/2019	S/O CLNR DISINFECT PINE	748.50	
				485990	9/26/2019	TOILET SEAT COVER & TOILE	572.23	
				485991	9/26/2019	TOWEL MULTIFOLD	101.83	
				486760	10/4/2019	TOILET TISSUE & AIR FRESH	130.06	1,814.67
106493	10/23/2019	51494	GARDA CL WEST, INC.	10519029	10/1/2019	OCT2019 ARMORED TRANSF	596.58	
				10519039	10/1/2019	OCT2019 CASHLINK MAINTEN	726.81	1,323.39
106494	10/23/2019	49100	GOLDMAN, RONALD A.	Sep2019	9/30/2019	SEP2019 SVCS: VISTA DEL AC	3,468.90	3,468.90

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Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106495	10/23/2019	52905	GOVERNMENT TAX SEMINAR Reg 12/12	10/9/2019	REG 12/12, GVRNMNT TAX SI	860.00	860.00
106496	10/23/2019	25500	GRANITE CONSTRUCTION C(1	10/1/2019	PE9/30 VAN BUREN ST IMPRV	112,629.15	112,629.15
106497	10/23/2019	53310	H.E.S. HERRERA ELECTRIC 203	9/24/2019	RPLC'D SING BALLAST & PHC	175.00	
			205	9/26/2019	TRBLSHT PALM TREE LIGHTS	935.00	1,110.00
106498	10/23/2019	19250	HENRY'S GLAZCON 23984	10/14/2019	INSTLL'D BULLET RESISTANT	6,500.00	6,500.00
106499	10/23/2019	51892	HERC RENTALS, INC. 31030682-004	9/25/2019	9/21-22 LIGHT TOWER RNTLS	282.72	
			31030682-005	9/25/2019	9/21-22 LIGHT TOWER RNTLS	471.20	
			31030682-006	9/27/2019	9/21-22 LIGHT TOWER RNTLS	376.96	
			31034391-001	9/25/2019	9/21-22 LIGHT TOWER RNTLS	960.54	
			31034391-002	9/25/2019	9/21-22 LIGHT TOWER RNTLS	188.48	
			31034391-003	9/25/2019	9/21-22 LIGHT TOWER RNTLS	282.72	
			31034391-004	9/27/2019	9/21-22 LIGHT TOWER RNTLS	188.48	
			31034392-001	9/24/2019	9/21-22 LIGHT TOWER RNTL	300.86	
			31034392-002	9/24/2019	9/21-22 LIGHT TOWER RNTL	94.24	
			31034392-003	9/25/2019	9/21-22 LIGHT TOWER RNTLS	188.48	
			31034392-006	9/27/2019	9/21-22 LIGHT TOWER RNTLS	471.20	
			31034405-001	9/25/2019	9/21-22 CART UTV 4 PSNGR F	702.99	
			31030682-001	9/24/2019	9/21-22 LIGHT TOWER RNTL	300.86	
			31030682-002	9/24/2019	9/21-22 LIGHT TOWER RNTL	94.24	
			31030682-003	9/25/2019	9/21-22 LIGHT TOWER RNTLS	188.48	5,092.45
106500	10/23/2019	00996	HOME DEPOT 3010475	9/17/2019	HDX BASE/WALL CABINET LT	214.50	
			3093361	10/7/2019	SAFETY GLASSES, CHP FLAS	149.91	
			5012278	9/5/2019	TOSHIBA 6K BTU W/ REMOTE	260.30	
			6012102	9/4/2019	SCOTT ROLLS, UTILITY GLOV	49.25	
			0011971	9/30/2019	TRASH BAGS, 27GAL TOTE &	409.74	
			0160503	9/30/2019	VINYL TAPE, STAIN OIL & LAT	26.55	
			0172037	9/30/2019	5-SHELF WELDED UNIT	541.58	
			6160443	9/24/2019	2X6-12 GDF, 2X6-14 GDF, ETC	52.29	
			9160524	10/1/2019	8FT PLATINUM CLOSET ROD	214.78	
			2121141	8/29/2019	PUSH BROOM, WTRPROOF E	397.35	2,316.25
106501	10/23/2019	44204	HUERTA, JR, ROBERTO Htl 10/23-24	10/2/2019	HTL 10/23-24, AWWA CONF: E	128.47	
			PD 10/23-24	10/15/2019	PD 10/23-24, AWWA CONF: S/	106.50	
			Reg 10/23-24	10/2/2019	REG 10/23-24, AWWA CONF: :	149.00	383.97
106502	10/23/2019	53459	IGLESIA DE CRISTO YO SOY 9/23 Deposit	10/9/2019	DEPOSIT REIMB- BGDMA PRI	300.00	300.00

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Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106503	10/23/2019	20450	IMPERIAL IRRIGATION DISTRI	50434217-SP19	9/26/2019	AC50434217, 8/27-9/24	47.15
				50459795-SP19	9/26/2019	AC50459795, 8/27-9/24	38.28
				50459796-SP19	9/26/2019	AC50459796, 8/27-9/24	65.83
				50459819-SP19	9/26/2019	AC50459819, 8/27-9/24	38.63
				50522793-SP19	9/26/2019	AC50522793, 8/27-9/25, SCAD	14.31
				50733502-SP19	10/3/2019	AC50733502, 8/30-9/30	27.41
				50734422-SP19	10/3/2019	AC50734422, 8/30-9/30	47.51
				MdAG-MdSP	9/12/2019	MID AUGUST-MID SEPTEMBE	63,931.51
				50035560-SP19	9/30/2019	AC50035560, 8/28-9/26, ST LIC	18,354.44
				50035755-SP19	9/26/2019	AC50035755, 8/27-9/24, PUMP	49.24
				50387122-AG19	9/5/2019	AC50387122, 8/1-30, SWR PL/	25,029.47
				50408460-SP19	9/26/2019	AC50408460, 8/27-9/24, WELL	5,949.59
				50784350-JN/SF	9/19/2019	AC50784350, 6/13-9/10	87.23
				50784352-JN/SF	9/19/2019	AC50784352, 6/13-9/10	76.81
106504	10/23/2019	45757	IMPERIAL IRRIGATION DISTRI	4028015	10/8/2019	PROTECT O/H LINE @ 48263	5,119.75
106505	10/23/2019	42409	IMPERIAL PIPE SERVICES, LL	PL26426-1	9/18/2019	30' LIGHT POLE W/ 15' ARM-C	2,053.13
				PL26428-1	9/18/2019	30' LIGHT POLE W/ 15' ARM-C	2,053.13
				PL26429-1	9/18/2019	30' LIGHT POLE W/ 15' ARM-C	2,053.13
106506	10/23/2019	45108	IMPERIAL SPRINKLER SUPPL	3900286-00	9/18/2019	GLOVES & 5" RED TRENCH S	39.83
				3884434-00	9/5/2019	2" CAP PVC SCH40, BUSHING	6.28
				3884525-00	9/5/2019	2" TEE SCH80 PVC, 11/4" SCH	39.35
				3895457-00	9/13/2019	DRIPRB XERI BUG EMITTER	83.48
				3900110-00	9/18/2019	RAINBIRD ROTOR POP-UP, E	703.59
				3900731-00	9/18/2019	GLOVES & SCH80 TBE NIPPL	50.67
				3900984-00	9/18/2019	10" ROUND VALVE BOX, ETC	108.23
				3905108-00	9/23/2019	10" PRO AL HEDGE SHEAR, C	96.42
				3905146-00	9/23/2019	STAPLE 11 GAUGE 6" JUTE S	10.37
				3908298-00	9/24/2019	HUNTER MP ROTATOR 90-21	100.52
				3909789-00	9/25/2019	DRIPRB SHRUB XERI-BUG EM	111.57
				3894966-00	9/13/2019	GLUE GREY LOW VOC PVC C	33.80
				3892760-00	9/12/2019	GEL KNEE PAD, 3" BLUE TRE	98.92
106507	10/23/2019	53463	JD LANDSCAPE & MAINTENAI	10102	10/8/2019	CMPCT GRND & POUR'D CON	500.00
							113,757.41
							5,119.75
							6,159.39
							1,483.03
							500.00

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Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total	
106508	10/23/2019	47328	KONICA MINOLTA	34043194	9/22/2019	BIZHUB C454+951+C364, SEF	783.00	
				34057139	9/25/2019	BIZHUB C454E, CITY HALL, SI	212.07	
				34080123	9/30/2019	BIZHUB C360, CORP YARD, S	109.84	
				34080124	9/30/2019	BIZHUB 501, WATER DEPT, S	163.44	
				34094338	10/2/2019	ACC 061-0042081-000, SEP20	67.43	1,335.78
106509	10/23/2019	44047	KONICA MINOLTA BUSINESS	9006054486	9/13/2019	BIZHUB C454E, CITY HALL, 8/	241.47	
				9006075472	9/20/2019	BIZHUB C360, CITY HALL, 9/2	37.46	
				9006081926	9/22/2019	BIZHUB 282, FIRE DEPT, 8/23	0.52	
				9006096207	9/27/2019	BIZHUB C364+C454+PRO 951	197.15	
				9006110396	9/30/2019	BIZHUB C360, CORP YARD, S	307.93	784.53
106510	10/23/2019	45051	LAMAR OF PALM SPRINGS	110682677	9/23/2019	9/23 POSTER FLEX	125.00	
				110682679	9/23/2019	9/23-10/20 DIGITAL BULLETIN	7,250.00	
				110682682	9/23/2019	9/23 VINYL W/ EXTENSION AI	1,245.00	8,620.00
106511	10/23/2019	50439	LANTELLIGENCE, INC.	20190639	10/2/2019	OCT2019/20 SHORETEL PART	4,028.32	4,028.32
106512	10/23/2019	08970	LEE ESPINOZA COACHELLA	V1019	10/1/2019	OCT2019 BOXING CLUB SER'	2,500.00	2,500.00
106513	10/23/2019	44160	LEWIS BRISBOIS BISGAARD	2467271	9/30/2019	PE8/31, #41691-2, MORALES'	118.00	118.00
106514	10/23/2019	24600	LOPES HARDWARE	008771	10/7/2019	AIR FILTER, TAPE, CAULK, ET	166.91	
				008828	9/24/2019	PIPE CLAMPS, BUSHING, ZIP	348.72	515.63
106515	10/23/2019	53460	LOUNGE APPEAL, INC.	19041	10/3/2019	12/4-7 HOLIDAY THRONE RN1	772.86	772.86
106516	10/23/2019	49857	MANPOWER US INC.	34342980	9/29/2019	WE 9/29: GALINDO+SOTO	841.05	
				34321936	9/22/2019	WE 9/22: OSUNA+RAMIREZ	1,323.70	
				34342984	9/29/2019	WE 9/29: OSUNA+RAMIREZ	1,388.80	3,553.55
106517	10/23/2019	45343	MCMASTER-CARR SUPPLY C	16842872	9/25/2019	LOCKING KEY-CONTROL CAI	66.47	66.47
106518	10/23/2019	25900	MEREDITH & SIMPSON CONS	190949	9/27/2019	TEST/CONNECT SUMP PUMF	237.84	
				190950	9/27/2019	120V SOLENOID VALVE	1,284.45	
				190927	9/16/2019	RPLC'D MOTOR/CONTROL W	2,360.11	
				190951	9/27/2019	RELOCATE LIGHT FIXTURE 8	338.92	
				190917	9/12/2019	RPR'D ELECTRICAL TO CLAR	1,113.84	5,335.16
106519	10/23/2019	43425	MOWERS PLUS, INC	196209	10/2/2019	BG 86 BLOWER & TRU-FUEL	323.22	323.22
106520	10/23/2019	45197	MSA CONSULTING, INC.	2405.001-11	8/31/2019	PE8/31 CASTRO'S WTR SYST	3,620.00	
				2406.001-08	8/31/2019	PE8/31 SHADY LN WTR SYST	1,620.00	
				2406.002-08	8/31/2019	PE8/31 SHADY LN SEPTIC TC	2,082.00	7,322.00
106521	10/23/2019	42240	MULTI W. SYSTEMS, INC.	31931139	9/25/2019	SUBMERSIBLE PUMP	793.11	793.11
106522	10/23/2019	00101	MUNISERVICES/GRS	INV06-006917	9/16/2019	JULY-SEP2019 SVCS: UTILITY	4,393.72	4,393.72

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106523	10/23/2019	42112 NRO ENGINEERING	10-19-018	9/30/2019	PE9/30 PLNCK, 49751 OATES	471.80	
			10-19-011	9/30/2019	PE9/30 PLNCK, 40850 HRSN	535.50	
			10-19-015	9/30/2019	PE9/30 PLNCK, PRADO:#138.	577.50	
			10-19-012	9/30/2019	PE9/30 PLNCK, LUXTOR:#138	428.40	
			10-19-008	9/30/2019	PE9/30 PLNCK, 45761 DILLON	803.25	
			10-19-013	9/30/2019	PE9/30 PLNCK, COA RSRCH F	5,281.50	
			10-19-009	9/30/2019	PE9/30 PLNCK, 45761 DILLON	1,575.00	
			10-19-010	9/30/2019	PE9/30 PLNCK, RED MOON (\	1,874.25	
			10-19-014	9/30/2019	PE9/30 PLNCK, 48451 HRSN	2,142.00	
			10-19-016	9/30/2019	PE9/30 PLNCK, CERT OF COF	218.75	
			10-19-017	9/30/2019	PE9/30 PLNCK, 32264 VISTA E	787.50	14,695.45
106524	10/23/2019	52757 OLLIN STRATEGIES	73	10/1/2019	SEP2019 CONSULTING SVCS	5,499.00	5,499.00
106525	10/23/2019	47192 O'REILLY AUTO PARTS	2855-150961	10/1/2019	RANGURD BEAM	43.46	
			2855-148933	9/24/2019	STARTER SW	40.50	
			2855-149189	9/25/2019	BATTERY	60.87	
			2855-149274	9/25/2019	BATTERY	213.05	
			2855-149293	9/25/2019	FUEL FILTER	18.37	
			2855-149521	9/26/2019	SPRAY/AROSOL & WIPER BL	30.33	
			2855-150833	10/1/2019	OIL FILTER	18.71	
			2855-150911	10/1/2019	STARTER & CORE	132.42	
			2855-150913	10/1/2019	CORE	-21.75	
			2855-148158	9/21/2019	CONNECTOR	4.34	540.30
106526	10/23/2019	53232 ORQUESTA BONKO	101919	8/7/2019	10/19 PERFORMANCE @ TAC	1,800.00	1,800.00
106527	10/23/2019	50595 PACIFIC LIGHTWAVE INC	19-3382	9/5/2019	OCT-DEC2019 BUSINESS INT	2,397.00	2,397.00
106528	10/23/2019	51847 PARTY TIME RENTALS	27	10/3/2019	10/19 MECHANICAL BULL RN	580.00	580.00
106529	10/23/2019	49989 PAUL ASSOCIATES	84129	10/2/2019	LETTERHEAD W/ REVISED AI	1,938.15	1,938.15
106530	10/23/2019	02028 PETE'S ROAD SERVICE, INC.	356029-00	9/11/2019	SVC CALL: TRACTOR FLAT R	124.11	
			358838-00	9/25/2019	P245/70R17 B HANKOOK DYN	263.87	
			360524-00	9/30/2019	FLAT REPAIR	25.61	413.59
106531	10/23/2019	08050 PETTY CASH	Ck 10/23/19	10/16/2019	VOUCHERS 4178-4187, 8/7-10	884.53	884.53
106532	10/23/2019	53198 PROACTIVE ENGINEERING	16023	10/10/2019	PE9/30 STORMWATER MASTI	2,500.00	2,500.00
106533	10/23/2019	42759 PROPER SOLUTIONS, INC.	10028	9/27/2019	WE 9/27: REYES+SERVIN	1,440.00	
			10029	9/27/2019	WE 9/27: M. LOPEZ	900.00	
			10055	10/4/2019	WE 10/4: L. SERVIN	720.00	
			10056	10/4/2019	WE 10/4: M. LOPEZ	900.00	3,960.00

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106534	10/23/2019	48977	PROTECTION 1/ADT 130731866	9/17/2019	ADDTNL EQUIP @ 87075 AVE	183.71	183.71
106535	10/23/2019	52082	PROWEST PCM, INC. 07-GMP5	9/30/2019	PE9/30 CNSTRCTN- COACHE	126,414.76	126,414.76
106536	10/23/2019	43576	PURE PLANET WATER, INC. 10447	9/23/2019	OCT2019 RNTL, WTR PURIFIK	32.63	
			10443	9/23/2019	OCT2019 RNTL, WTR PURIFIK	32.63	
			10444	9/23/2019	OCT2019 RNTL, WTR PURIFIK	32.63	
			10445	9/23/2019	OCT2019 RNTL, WTR PURIFIK	32.63	
			10446	9/23/2019	OCT2019 RNTL, WTR PURIFIK	32.63	163.15
106537	10/23/2019	52306	QUINN COMPANY WOG00005981	9/24/2019	TRBLSHT CIRC PUMP @ SAN	687.75	687.75
106538	10/23/2019	52365	RESORT PARKING SERVICES 8176	8/19/2019	EVENT PARKING SERVICES (4,253.52	4,253.52
106539	10/23/2019	11080	RIVERSIDE COUNTY RECORA18-231485	7/31/2018	JULY2018- RECORDINGS	189.00	
			18-330746	11/6/2018	NOV2018- LIEN RELEASE FEE	120.00	309.00
106540	10/23/2019	11080	RIVERSIDE COUNTY RECORA18-362411	10/15/2019	DEC2018- CLERK COPY	1.00	1.00
106541	10/23/2019	44161	ROBERT HALF MNGT RESOU154330356	9/23/2019	WE 9/20: A. FELIX	821.60	
			54379157	9/30/2019	WE 9/27: A. FELIX	821.60	1,643.20
106542	10/23/2019	43728	ROSALES, RENE Trvl Exp 10/1-4	10/14/2019	TRVL EXP 10/1-4, CACEO COI	60.00	60.00
106543	10/23/2019	50340	ROYAL GYM SERVICES 5576	10/1/2019	SEPT2019 PREVENTATIVE M/	295.00	295.00
106544	10/23/2019	52203	RUIZ, XITLALY 8	10/8/2019	10/19 TACOS, TEQUILA & CH/	450.00	450.00
106545	10/23/2019	00382	SAFEGUARD BUSINESS SYST33732947	10/4/2019	LASER L3 BLANK CHECKS	406.04	406.04
106546	10/23/2019	32950	SAFETY-KLEEN SYSTEMS, IN80880252	9/12/2019	9/10 SVC	257.12	257.12
106547	10/23/2019	49697	SANCHEZ, ROBERTO 110219	10/2/2019	11/2 VETERANS' DAY CATERI	2,200.00	2,200.00
106548	10/23/2019	51849	SANTA ROSA DEL VALLE 26920	9/10/2019	AUG2019 SVCS: R. HUERTA	25.00	
			26950	9/10/2019	AUG2019 SVCS: R. HUERTA	125.00	150.00
106549	10/23/2019	50827	SDC SOUND COMPANY 1031	10/11/2019	10/19 SOUND/LIGHTING SYS:	6,540.00	6,540.00
106550	10/23/2019	52924	SIEMENS MOBILITY, INC. 5610188661	9/27/2019	AUG2019 TRAFFIC SIGNAL M	1,760.00	
			5620025253	9/27/2019	AUG2019 TRAFFIC SIGNAL C,	1,821.75	3,581.75
106551	10/23/2019	46733	SIMPLOT PARTNERS 208104849	9/20/2019	ADVION FIRE ANT BAIT	357.81	357.81
106552	10/23/2019	35450	SOCALGAS 1377 6th-SP19	9/25/2019	AC 012 623 3701 5, 8/22-9/23	55.78	
			1500 6th-SP19	9/25/2019	AC 020 678 1257 4, 8/22-9/23	83.28	
			1515 6th-SP19	9/25/2019	AC 031 523 3700 6, 8/22-9/23	20.98	
			1540 7th-SP19	9/25/2019	AC 008 423 3900 4, 8/22-9/23	61.99	
			84626Bag-SP19	9/25/2019	AC 153 323 6215 9, 8/22-9/23	15.89	
			87075Av54-SP19	9/25/2019	AC 123 573 5834 5, 8/22-9/23	35.34	
			BagPool-SP19	9/25/2019	AC 069 323 6500 7, 8/22-9/23	15.78	289.04
106553	10/23/2019	35430	SOUTH COAST A.Q.M.D. 3526834	9/17/2019	ID 148143, F82365, >500HP GI	421.02	
			3530207	9/17/2019	ID 148143, FY19/20, EMISSION	136.40	557.42

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106554	10/23/2019	51139	SOUTHERN COMPUTER WARIN-000598630	9/5/2019	HEWLETT PACKARD ENTERF	170.30	
			IN-000583949	6/18/2019	STATE OF CA EWASTE FEE	42.00	
			IN-000598804	9/5/2019	12PK ENERGIZER 9V INDUST	75.79	
			IN-000598859	9/5/2019	STATE OF CA EWASTE FEE	7.00	
			IN-000599448	9/9/2019	12PK ENERGIZER 9V INDUST	75.79	370.88
106555	10/23/2019	47319	SPARKLETTS 9467308 092419	9/24/2019	SEP2019 WATER @ SANITAR	304.71	304.71
106556	10/23/2019	52595	STAPLES BUSINESS CREDIT 7300950080-0-1	10/1/2019	QUICK-STOR-LTR/LGL, STAPI	400.21	
			7300678404-6-1	10/2/2019	FILE LAT 4DRWR	831.71	
			7300756735-0-1	9/20/2019	SUPER SORTER BLK MESH, I	95.74	
			7300756735-0-2	9/24/2019	MESH PAPER TRAY	36.42	
			7300645705-0-1	9/13/2019	COPYPLUS 8.5X11, PEN COU	384.79	
			7300678404-0-1	9/17/2019	NX1-BRIGADE 600 SERIES, E	3,613.25	
			7300678404-0-2	9/23/2019	4PK 14X36 TRANSLUCENT SH	101.10	
			7300678404-2-1	9/20/2019	FILE 4DWR LGL	389.97	
			7300678404-4-1	9/30/2019	NX1-310 SERIES	389.97	
			7300788812-0-1	9/23/2019	XEROX 6510/15 BLK TONER,	440.67	
			7300788812-0-2	9/23/2019	SBG MICROSDHC CLASS 10 (43.26	
			7300665653-0-1	9/16/2019	HP 63XL HYBLK/63 TRI-CLR, E	81.14	
			7300794926-0-1	9/23/2019	SPLS REC COPY CS	81.55	6,889.78
106557	10/23/2019	43858	STAPLES CREDIT PLAN 19532	10/7/2019	QUARTET FABRIC ENCLOSEI	1,420.22	1,420.22
106558	10/23/2019	00102	SUNLINE TRANSIT AGENCY INV03744	10/7/2019	SEP2019 CNG FUEL	1,267.56	1,267.56
106559	10/23/2019	47915	SUPERCIRCUITS, INC. INV25759	10/4/2019	INSTLL'D SEAGATE SKYHAW	284.98	284.98
106560	10/23/2019	52784	THE PUN GROUP LLP 112216	9/27/2019	FY18/19 AUDIT SVCS	20,000.00	20,000.00
106561	10/23/2019	42289	TIME WARNER CABLE 0037022091819	9/18/2019	1515 6TH ST-AH, SEP2019	1,576.30	1,576.30
106562	10/23/2019	51229	TJC PROPERTY SERVICES, IN20190902	9/25/2019	AUG2019 SVCS: 48263 VAN B	1,237.50	1,237.50
106563	10/23/2019	50627	TOP SCALE, INC. 60121	9/10/2019	9/3 THERMOMETER CALIBRA	372.86	372.86
106564	10/23/2019	38250	TOPS N BARRICADES 1077481	9/18/2019	HANDICAP PRKNG, TEMP NC	517.87	
			1077524	9/20/2019	9/18-20 BARRICADE RNTLS	127.60	
			1077554	9/23/2019	9/18-23 BARRICADE RNTLS	122.40	
			1077569	9/24/2019	9/6-23 CHNGBLE MSG SIGN F	1,290.00	
			1077668	9/27/2019	PED BARRICADE GALV	2,300.06	
			1077669	9/27/2019	PED BARRICADE GALV	1,533.38	5,891.31
106565	10/23/2019	50590	TOUCHTONE COMMUNICATIO631650	10/1/2019	AC 1100006871, OCT2019	3.44	3.44
106566	10/23/2019	52204	TPX COMMUNICATIONS 121173784-0	9/16/2019	AC33325, 9/16-10/15	3,352.97	3,352.97
106567	10/23/2019	42187	UNION BANK OF CALIFORNIA1174691	9/20/2019	SP19-AG20, COA LEASE REV	3,280.00	3,280.00

apChkLst
10/16/2019 12:30:42PM

Check List
City of Coachella

Pa Item5.

Bank : wfb WELLS FARGO BANK (Continued)

<u>Check #</u>	<u>Date</u>	<u>Vendor</u>	<u>Invoice</u>	<u>Inv Date</u>	<u>Description</u>	<u>Amount Paid</u>	<u>Check Total</u>
106568	10/23/2019	48436 UNIVAR USA INC.	LA757317	9/27/2019	SODIUM HYPOCHLORITE	5,767.64	5,767.64

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
106570	10/23/2019	50229	URBAN HABITAT ENVIRONME	5144	9/30/2019	DIST 28: RPR'D IRRGTN	48.41
				4419A	3/28/2019	DIST 22: RPR'D IRRGTN	66.99
				4420A	3/28/2019	DIST 24: RPR'D IRRGTN	65.93
				4423A	3/28/2019	DIST 32: RPR'D IRRGTN	91.35
				5077	8/31/2019	DIST 36: RPR'D IRRGTN	88.45
				5078	8/31/2019	DIST 18: RPR'D IRRGTN	195.94
				5087	8/31/2019	DIST 16: RPR'D IRRGTN	33.34
				5093	8/31/2019	DIST 16: RPR'D IRRGTN	171.09
				5094	8/31/2019	DIST 24: RPR'D IRRGTN	206.26
				5095	8/31/2019	DIST 38: RPR'D IRRGTN	67.31
				5145	9/30/2019	DIST 16: RPR'D IRRGTN	405.93
				5146	9/30/2019	DIST 20: RPR'D IRRGTN	66.41
				5156	9/30/2019	DIST 23: RPR'D IRRGTN	78.69
				5160	9/30/2019	SEP2019 LANDSCAPE MAINT	47,504.41
				5096	8/31/2019	DIST 33: RPR'D IRRGTN	64.00
				5097	8/31/2019	DIST 13: RPR'D IRRGTN	159.62
				5098	8/31/2019	DIST 32: RPR'D IRRGTN	68.99
				5104	8/31/2019	AUG2019 LANDSCAPE MAINT	47,504.41
				5114	8/31/2019	DIST 16: RPR'D IRRGTN	64.05
				5115	8/31/2019	DIST 20: RPR'D IRRGTN	141.31
				5116	8/31/2019	DIST 27: RPR'D IRRGTN	139.25
				5088	8/31/2019	DIST 16: RPR'D IRRGTN	141.73
				5089	8/31/2019	DIST 16: RPR'D IRRGTN	290.61
				5090	8/31/2019	DIST 16: RPR'D IRRGTN	250.55
				5091	8/31/2019	DIST 24: RPR'D IRRGTN	65.89
				5092	8/31/2019	DIST 22: RPR'D IRRGTN	140.39
				5117	8/31/2019	DIST 28: RPR'D IRRGTN	141.56
				5118	8/31/2019	DIST 32: RPR'D IRRGTN	117.39
				5123	8/31/2019	DIST 34: RMV'D TREE	450.00
				5124	8/31/2019	DIST 36: RPR'D IRRGTN	210.60
				5125	8/31/2019	DIST 29: RPR'D IRRGTN	88.14
				5135	9/30/2019	DIST 16: RMV'D TREE BRANC	191.00
				5136	9/30/2019	DIST 31: RMV'D TREE BRANC	259.00
				5138	9/30/2019	DIST 25: RPR'D IRRGTN	40.42
				5141	9/30/2019	DIST 22: RPR'D IRRGTN	169.63

Bank : wfb WELLS FARGO BANK (Continued)

Check #	Date	Vendor	Invoice	Inv Date	Description	Amount Paid	Check Total
			5142	9/30/2019	DIST 23: RPR'D IRRGTN	68.52	
			5143	9/30/2019	DIST 16: RPR'D IRRGTN	81.34	
			5177	9/30/2019	DIST 28: RPR'D IRRGTN	33.93	
			5178	9/30/2019	DIST 20: RPR'D IRRGTN	103.21	
			5179	9/30/2019	DIST 11: RPR'D IRRGTN	30.14	
			5180	9/30/2019	DIST 36: RPR'D IRRGTN	67.97	
			5181	9/30/2019	DIST 19: RPR'D IRRGTN	69.21	100,243.37
106571	10/23/2019	43751 USA BLUEBOOK	003503	9/9/2019	CLOCK	33.14	
			995418	8/29/2019	ACETATE BUFFER SOLUTION	630.71	663.85
106572	10/23/2019	39640 VALLEY LOCK & SAFE	155444	7/11/2019	INSTLL'D CYBER KEY SMART	17,498.40	17,498.40
106573	10/23/2019	51665 VALLEY REPROGRAPHICS LL	28649	9/19/2019	BOOKLETS: ATP CYCLE 2	178.60	178.60
106574	10/23/2019	53435 VALLEY WIDE TOWING LLC	00007422	5/26/2019	EVIDENCE TOW #Y191400039	218.00	
			00007423	5/26/2019	EVIDENCE TOW #Y191400039	218.00	436.00
106575	10/23/2019	51820 VARIDESK LLC	IVC-2-1184182	6/3/2019	CUBE CORNER 36-BLACK	484.48	484.48
106576	10/23/2019	53461 VERAZAS, DORA	Refund	10/9/2019	DEPOSIT REFUND- LIBRARY	300.00	300.00
106577	10/23/2019	44966 VERIZON WIRELESS	9837133388	9/1/2019	AC371867190-00001, 8/2-9/1	7,782.19	7,782.19
106578	10/23/2019	50629 VINTAGE ASSOCIATES, INC	213355	8/27/2019	5/21 HYDROSTATIC TEST PUI	112.00	
			213627	9/23/2019	APPLIED TOP DRESSING SOI	12,690.00	12,802.00
106579	10/23/2019	50158 VIRTUAL CONNECT TECHNOI	3012170	10/1/2019	OCT2019/20 CLOUDFILTER &	2,799.36	2,799.36
106580	10/23/2019	44775 VISTA PAINT CORPORATION	2019-097410-00	10/1/2019	GRAFFITI BLOCK FLAT P BAS	64.67	
			2019-106851-00	10/5/2019	ACRIGLO EGGSHELL WHITE-	310.71	375.38
106581	10/23/2019	53455 VORTEX INDUSTRIES, INC.	05-1372412	9/25/2019	INSTLL'D STOREFRONT DOC	4,533.00	4,533.00
106582	10/23/2019	49778 WEST COAST ARBORIST, INC	150413	7/14/2019	PE7/14 TREE MAINT @ LLMD	1,456.00	
			150414	7/15/2019	PE7/15 TREE MAINT @ LLMD	1,768.00	
			152412	9/30/2019	PE9/30 TREE MAINT @ PARK:	378.00	
			152474	8/31/2019	PE8/31 TREE MAINT @ LLMD	450.00	
			150415-A	7/15/2019	PE7/15 TREE MAINT @ STRE	6,760.00	
			151365-A	8/15/2019	PE8/15 TREE MAINT @ LLMD	450.00	
			152318	9/15/2019	PE9/15 TREE MAINT @ STRE	1,640.00	
			152405	9/15/2019	PE9/15 TREE MAINT @ LLMD	1,380.00	14,282.00
106583	10/23/2019	53051 WESTERN DESIGN LLC	2089	10/3/2019	50L LED WIDE ANGLE WARM	1,720.81	
			2090	10/3/2019	NATURAL MOUNTAIN PINE W	586.96	2,307.77
106584	10/23/2019	51697 WESTERN WATER WORKS SI	57019-00	9/17/2019	EXT GATE VLV KEY	158.24	
			57074-00	9/27/2019	PE IPS ID PIPE SIDR-7	130.50	288.74
106585	10/23/2019	00384 WILLDAN FINANCIAL SERVICI	010-42532	9/27/2019	OCT-DEC2019 ADMIN SVCS,	2,585.53	2,585.53

Sub total for WELLS FARGO BANK: 973,493.98

142 checks in this report.

Grand Total All Checks: 973,493.98

Date: October 23, 2019


City Manager: William B. Pattison Jr.


Controller: Javier Estrada

City of Coachella Building Activity Report
JULY 2019

Item 6.

Permits	Current Month	Past Month	This Month Last Year	This Year to Date	Last Year to Date
Res. Solar Panels	2	0	1	15	5
Misc. Building Permit:	28	28	9	186	102
Residential Additions	1	2	0	14	6
Single Family Dwelling	0	16	0	36	1
Commercial Bldgs.	0	0	0	4	14
Commercial Ten. Impr	5	2	0	3	10
Multi- Family Units	0	0	0	0	0
Plan Check	26	25	18	157	147
TOTALS	62	73	28	415	285
Revenue Fees					
Building Fees	\$13,687.00	\$45,162.50	\$3,415.50	\$158,382.51	\$81,067.95
Dev. Impact Fees	\$0.00	\$2,157,847.31	\$0.00	\$2,489,329.17	\$965,469.92
Sewer & Water Fees	\$895.41	\$109,501.41	\$0.00	\$568,417.71	\$1,071,060.73
Misc. (TUMF, MSF, FIF	\$2,120.18	\$60,900.45	\$0.00	\$106,436.16	\$278,828.80
Plan Check	\$6,950.50	\$7,170.50	\$34,735.50	\$41,621.50	\$100,454.30
Cert of Occupancy	\$0.00	\$4,096.00	\$0.00	\$10,240.00	\$2,304.00
SB1473	\$56.00	\$195.00	\$21.00	\$775.00	\$1,425.50
TOTALS	\$23,709.09	\$2,384,873.17	\$38,172.00	\$3,375,202.05	\$2,500,611.20
1% Construction Tax	\$714.96	\$36,148.20	\$0.00	\$92,844.60	\$245,062.17
Strong Motion Instr.	\$208.37	\$500.20	\$6.89	\$1,616.79	\$6,797.59
TOTALS	\$923.33	\$36,648.40	\$6.89	\$94,461.39	\$251,859.76
Valuations	\$775,374.00	\$4,539,710.82	\$16,048,521.56	\$9,745,115.85	\$43,584,639.79
Inspections					
General	154	123	130	831	1079
Final Single Family	8	0	0	8	14
Final Multi Family	0	0	0	0	50
Final Commercial	1	0	0	4	0
Final Miscellaneous	37	0	34	127	205
Final Solar	11	2	0	15	5
Code Enforcement Ins:	5	2	2	18	26
TOTALS	216	127	166	1003	1379

TOTAL DWELLING UNITS IN THE CITY OF COACHELLA

8871

Approved by:

Brian Gumpert
Deputy Building Official

SFD Finalled in July 2019

50414 via prado 84450 Raphael 84457 Raphael 84445 Raphael
84444 Raphael 84456 Raphael 84441 Raphael 50415 Ribera

City of Coachella Building Activity Report
AUGUST 2019

Item 6.

Permits	Current Month	Past Month	This Month Last Year	This Year to Date	Last Year to Date
Res. Solar Panels	1	2	1	16	6
Misc. Building Permits	5	28	35	191	137
Residential Additions	0	1	2	14	8
Single Family Dwelling	0	0	0	36	1
Commercial Bldgs.	0	0	0	4	14
Commercial Ten. Impr	4	5	3	7	13
Multi- Family Units	0	0	0	0	0
Plan Check	16	26	29	173	176
TOTALS	26	62	70	441	355
Revenue Fees					
Building Fees	\$8,344.50	\$13,687.00	\$10,332.46	\$166,727.01	\$91,400.41
Dev. Impact Fees	\$0.00	\$0.00	\$0.00	\$2,489,329.17	\$965,469.92
Sewer & Water Fees	\$0.00	\$895.41	\$7,106.71	\$568,417.71	\$1,078,167.44
Misc. (TUMF, MSF, FIF)	\$826.20	\$2,120.18	\$0.00	\$107,262.36	\$278,828.80
Plan Check	\$4,212.50	\$6,950.50	\$6,014.00	\$45,834.00	\$106,468.30
Cert of Occupancy	\$0.00	\$0.00	\$0.00	\$10,240.00	\$2,304.00
SB1473	\$25.00	\$56.00	\$37.00	\$831.00	\$1,462.50
TOTALS	\$13,408.20	\$23,709.09	\$23,490.17	\$3,388,641.25	\$2,524,101.37
1% Construction Tax	\$1,664.96	\$714.96	\$2,033.41	\$94,509.56	\$247,095.58
Strong Motion Instr.	\$57.02	\$208.37	\$49.48	\$1,673.81	\$6,840.18
TOTALS	\$1,721.98	\$923.33	\$2,082.89	\$96,183.37	\$253,935.76
Valuations	\$251,058.00	\$775,374.00	\$340,860.40	\$9,996,173.85	\$43,925,500.19
Inspections					
General	136	154	75	967	1154
Final Single Family	0	8	21	8	35
Final Multi Family	0	0	0	0	50
Final Commercial	1	1	0	4	0
Final Miscellaneous	23	37	46	150	251
Final Solar	1	11	2	16	7
Code Enforcement Ins	3	5	2	21	28
TOTALS	164	216	146	1166	1525

TOTAL DWELLING UNITS IN THE CITY OF COACHELLA

8871

Approved by:

Brian Gumpert
Deputy Building Official

SFD Finalled in August 2019

0

City of Coachella Building Activity Report
 SEPTEMBER 2019

Item 6.

Permits	Current Month	Past Month	This Month Last Year	This Year to Date	Last Year to Date
Res. Solar Panels	11	1	1	27	7
Misc. Building Permits	42	5	24	233	161
Residential Additions	3	0	2	17	10
Single Family Dwelling	0	0	0	36	1
Commercial Bldgs.	0	0	0	4	14
Commercial Ten. Impr	5	4	2	12	15
Multi- Family Units	0	0	0	0	0
Plan Check	35	16	12	208	188
TOTALS	96	26	41	537	396
Revenue Fees					
Building Fees	\$14,741.50	\$8,344.50	\$6,257.50	\$181,468.51	\$97,657.91
Dev. Impact Fees	\$0.00	\$0.00	\$0.00	\$2,489,329.17	\$965,469.92
Sewer & Water Fees	\$80,897.30	\$0.00	\$1,838.85	\$649,315.01	\$1,080,006.29
Misc. (TUMF, MSF, FIF	\$8,015.47	\$826.20	\$0.00	\$115,277.83	\$278,828.80
Plan Check	\$9,146.00	\$4,212.50	\$4,218.50	\$54,980.00	\$110,686.80
Cert of Occupancy	\$256.00	\$0.00	\$0.00	\$10,496.00	\$2,304.00
SB1473	\$72.00	\$25.00	\$29.00	\$856.00	\$1,491.50
TOTALS	\$113,128.27	\$13,408.20	\$12,343.85	\$3,501,722.52	\$2,536,445.22
1% Construction Tax	\$600.00	\$1,664.96	\$895.00	\$95,109.56	\$169,993.68
Strong Motion Instr.	\$169.40	\$57.02	\$21.73	\$1,843.21	\$2,947.36
TOTALS	\$769.40	\$1,721.98	\$916.73	\$96,952.77	\$172,941.04
Valuations	\$975,989.30	\$251,058.00	\$238,600.00	\$10,972,163.15	\$44,164,100.19
Inspections					
General	143	136	87	1110	1241
Final Single Family	5	0	0	13	35
Final Multi Family	0	0	0	0	50
Final Commercial	1	1	0	5	0
Final Miscellaneous	26	23	32	176	283
Final Solar	5	1	1	21	8
Code Enforcement Ins	3	3	7	24	35
TOTALS	183	164	127	1349	1652

TOTAL DWELLING UNITS IN THE CITY OF COACHELLA

8876

Approved by:

 Brian Gumpert
 Deputy Building Official

Business License Report

Name Cesar Lucrecio
Department Business License
Period 7/1/2019 - 9/30/19

Date Submitted 10/14/2019

Revenue Account	Description of Revenue	2020 Total Budget	1Q 2020 Revenue	Last Quarter Totals	Up-to-date 2020 Revenues	Balance
101-11-110-10-315	Annual Registration Tax	\$ 36,000.00	\$1,230.00		\$1,230.00	\$ 34,770.00
101-11-110-10-316	Business Licensee Fee	\$ 550,000.00	\$7,128.07		\$7,128.07	\$ 542,871.93
101-11-110-10-319	Penalties Revenue	\$ 5,000.00				\$ 5,000.00
240-12-110-40-342	Uniform Fire Code Fee	\$ 35,000.00	\$1,870.00		\$1,870.00	\$ 33,130.00
101-11-110-10-325	SB-1186- State Tax	\$ 4,000.00				\$ 4,000.00
101-11-110-10-322	Transient Occupancy Tax	\$ 150,000.00	\$24,863.25		\$24,863.25	\$ 125,136.75
	Total Budget	\$780,000.00	\$35,091.32	\$0.00	\$35,091.32	\$744,908.68

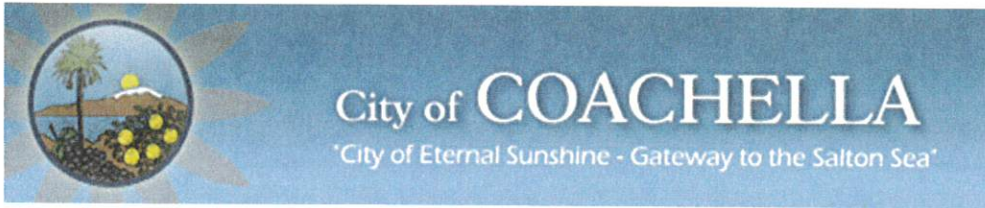


**CITY OF COACHELLA
53-990 ENTERPRISE WAY
COACHELLA CA 92236**

***CODE ENFORCEMENT, GRAFFITI ABATEMENT AND
ANIMAL CONTROL QUARTERLY REPORTS***

July 1, 2019 to Sept. 30, 2019

**Prepared by:
Rene Rosales**



Item6.

Code Enforcement Summary Report

Report Criteria:

<i>Status</i>	<i>Assigned To</i>	<i>Census Tract</i>	<i>Violation</i>	<i>Initiation</i>	<i>Open Date Range</i>	<i>Follow up Date Range</i>	<i>Close Date Range</i>
All	All		All	All	From 07/01/2019 To 09/30/2019	From To	From To

AVA Cases by Violation

<i>Violation</i>	<i>Total Violations</i>	<i>Closed Violations</i>	<i>Open Violations</i>
Abandoned Vehicle	52	50	2
Totals	52	50	

CE Totals

	<i>Total</i>	<i>Closed Cases</i>	<i>Open Cases</i>
Totals	123	85	38

CE Cases by Violation

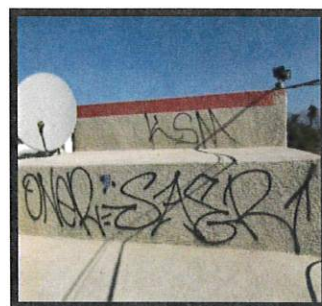
<i>Violation</i>	<i>Total Violations</i>	<i>Closed Violations</i>	<i>Open Violations</i>
	6	6	0
10.20.010(18)Abandoned/Inoperable/Dismantled vehicle(s).	8	4	4
12.12.050 - Duty to maintain sidewalks.	1	1	0
12.16.030 - Permit required/CURB CUTS AND DRIVEWAYS	0	0	0
12.42.030 - Unlawful camping.	1	1	0
12.42.040 - Storage of personal property on public property.	2	2	0
13.03.044 Wasting water prohibited.	0	0	0
13.03.208 - Access to water meter.	0	0	0
13.03.306 Expiration or Extension of Permit	0	0	0
15.04.020 (A) Concrete Flat Work, Shades & Storage Containers	5	1	4
15.44.010 Building Numbering/Required	1	1	0
15.60.010 Building Permit Required	19	7	12
15.60.050 - Substandard buildings and housing.	1	1	0
15.66.010 Seismic Hazard Mitigation	0	0	0
17.16.020 - Permitted uses.	0	0	0
17.16.030(C)(5)-Property development standards. Walls/Fences	0	0	0
17.48.020 Display of merchandised out of doors.	0	0	0

3.10.010(D)(11)-Sewage.	0	0	0
3.10.010(D)(15)-Hazardous Substances and Waste.	0	0	0
3.10.010(D)(16)-Visibility Hazard.	0	0	0
3.10.010(D)(19)-Visual Blight	14	7	7
3.10.010(D)(20)-Swmng Pools, Ponds and Other Bodies of Wtr.	0	0	0
3.10.010(D)(21)-Public Toilets.	0	0	0
3.10.010(D)(22)-Privies.	0	0	0
3.10.010(D)(25)-Land Use Entitlements.	0	0	0
3.10.010(D)(27)-Public Burning.	0	0	0
3.10.010(D)(28)-Air Pollution.	0	0	0
3.10.010(D)(29) Mosquito Breeding Places.	1	1	0
3.10.010(D)(30)Discharge of Sewage	0	0	0
3.10.010(D)(31)-Animal Manure	0	0	0
3.10.010(D)(32)-Hauling of Offensive Substances	0	0	0
3.10.010(D)(33)-Clothes Lines.	0	0	0
3.10.010(D)(34)Stormwater Drainage	1	1	0
3.10.010(D)(35)-Water Disposal.	0	0	0
3.10.010(D)(36)-Signs.	0	0	0
3.10.010(D)(37)-Encroachment.	0	0	0
3.10.010(D)(38)-Municipal Code Violations.	0	0	0
3.10.010(D)(39)-California Civil Code.	0	0	0
3.10.010(D)(40)-California Red Light Abatement Act.	0	0	0
3.10.010(D)(41)-California Drug Abatement Act.	0	0	0
3.10.010(D)(42)-State Housing Law.	0	0	0
3.10.010(D)(43)-Weed Abatement.	2	2	0
3.10.010(D)(6)Parking on Yard Off Driveway	2	1	1
3.10.010(D)(7) Occupied Vehicles	1	0	1
3.16.090 - Failure to comply with notice of violation.	0	0	0
8.20.040(C)Dust Control requirements	0	0	0
8.44.070 Commercial Garbage Collection and Disposal	0	0	0
8.48.030 (B)Graffiti Prohibited	3	3	0
8.60 - REGISTRATION AND MAINTENANCE OF ABANDONED PROPERTIES	0	0	0
8.60.040 Registration of Abandoned Property	1	0	1
8.60.050 - Abandoned Property Maintenance requirements.	0	0	0
8.64.050 - Vacant Property Maintenance Requirements	0	0	0
Abandoned/Inoperable/Dismantled vehicle(s)	1	0	1
Business License Required	1	1	0
Business Subject to Miscellaneous Tax Rates/Transportation, Trucking and Towing	0	0	0
Display of merchandise out of Door	0	0	0

UFC 10.206(a)-(b) Obstruction	0	0	0
UFC 10.206(b)-Hydrant,(c) Red curb-15' each side	0	0	0
UFC 10.207(1)-Fire Apparatus Access Roads,(a) Red curb on signs	0	0	0
UFC 10.207-Fire Apparatus Access Roads.(b) Obstruction	0	0	0
UFC 10.208(a)-Premises Identification,	0	0	0
UFC 10.301(a)-Fire Extinguishers,(a) Minimum 2A10BC	0	0	0
UFC 10.301(a)-Fire Extinguishers,(e) Commercial kitchen 40BC.	0	0	0
UFC 10.302(a)-Fire Extinguishers,(b) Servicing due	0	0	0
UFC 10.313(b)-Hood and Duct Systems,(b) Proper nozzle position, caps, coverage	0	0	0
UFC 10.313(c)-Hood and Duct Systems,(c) Automatic gas/electric shut off	0	0	0
UFC 10.313(e)	0	0	0
UFC 10.313(e)-Hood and Duct Systems,(d) Fuseable links, cables, conduits	0	0	0
UFC 10.401	0	0	0
UFC 10.401-Walls and Ceilings,(a) Holes sealed	0	0	0
UFC 10.402(a)-Exits,(e) Maintain fire rated doors, windows, dampers, and hardware	0	0	0
UFC 11.201(b)-General Housekeeping,(a) Rubbish/trash buildup	6	4	2
UFC 11.203(b)-General Housekeeping,(d) Storage not within 24" of ceiling	0	0	0
UFC 11.203(c)-General Housekeeping,(g) Storage in boiler, mech., elect. panel rooms prohibited	0	0	0
UFC 11.205(a)-General Housekeeping,(h) Candles used in approved manner	0	0	0
UFC 12.103(a)-Exits,(d) Exit obstruction	0	0	0
UFC 12.106(c)-General Housekeeping,(e) Storage under stairs prohibited	0	0	0
UFC 12.203(a)-Occupant Limit/Sign,(a) Occupant load sign(s) posted	0	0	0
UFC 12.207-General Housekeeping,Storage in attic space prohibited	0	0	0
UFC 13.103-Occupant Limit/Sign,(b) Required NO SMOKING signs	0	0	0
UFC 74.107(a)	0	0	0
UFC 79-201(e)-Flammable Liquids -(a) Approved safety cans under 10 gal.	0	0	0
UFC 79.107-Flammable Liquids,(c) Legible labeling	0	0	0
UFC 79.201(e)-Flammable Liquids,(b) Approved cabinet exceeding 10 gal.	0	0	0
UFC 85.104 (c)-Electrical,(c) Abatement of electrical hazards	1	0	1
UFC 85.104 (f)-Electrical,(f) Cover open space in circuit breaker panel	0	0	0
UFC 85.106-Electrical,(d) Exten. cords not allowed as permanent wiring	2	1	1
UFC 85.107-Electrical,(a) Zip cords/cube adapters not allowed	0	0	0
UFC 85.108-Electrical,(b) Min. 30" clear access to circuit breaker	0	0	0
UFC Stand. No. 10-1-6.9-Fire Extinguishers,(c) Mounted 3-5 feet from floor	0	0	0
UFC Standard No. 10-1-6.5-Fire Extinguishers,(d) Obstruction	0	0	0
UMC 504(a)-General Housekeeping,(c) 36" clearance around water heater	0	0	0
Weed Abatement	14	7	7
Totals	244	147	97

Code Enforcement:
Graffiti Department Quarterly Report
July 1, 2019 to September 30, 2019

	Totals
Gallons Used	238 Gal.
5 Gallon buckets Purchased	3
Cover up/ stops	364
Pressure Washing Sq. ft.	460 sq ft
These Graffiti artists started in February and are continuing to vandalize City & private property.	"SAER" "SAVOR" "KSM"





County of Riverside
 Department of Animal Services
 Coachella City Quarterly Report for September 2019 Field Service

Field Services Impound Statistics	Sep-19	QTR 7/1/2019 - 9/30/2019)
Total Canines Impounded	37	114
Owner Turn-Ins	1	4
Stray	2	70
Dead on Arrival	34	40
Total Felines Impounded	32	72
Owner Turn-Ins	0	0
Stray	17	49
Dead on Arrival	15	23
Total Other Animals Impounded	1	5
Owner Turn-Ins	0	0
Stray	0	4
Dead on Arrival	1	1

Field Services Activities	Sep-19	QTR 7/1/2019 - 9/30/2019)
Number of Requests for Service	90	314
Number of Animal Bite Calls	5	13
Number of sick/injured animal calls	11	28
Vicious Animal (imminent danger)	7	18
Cruelty/Neglect Calls	6	18
Total Citations	5	20
Total Licenses	47	200
Number of Nuisance animal Complaints	0	0

City of Coachella

Development Status Report

September, 2019



Prepared By:
Development Services Department
Luis Lopez, Development Services Director
Juan Carrillo, Associate Planner
Rosa Montoya, Planning Technician
Yesenia Becerril, Planning Commission Secretary

Coachella Permit Center
53-990 Enterprise Way
Coachella, CA 92236
(760) 398-3102 Fax (760) 398-5421
www.coachella.org

	<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
	AR EA	14-02 14-03	<u>Double Date Packing Facility</u> Construct new 35,500 sq. ft. date packing facility	86301 Industrial Way APN 763-131-061	Steven Gilfenbain 9777 Wilshire Blvd., #900 Beverly Hills, CA. 90212 (310) 651-2591	PC approved 12/17/14 1 st Phase Complete
	AR	16-04	<u>Truck Storage</u> To develop a vehicle storage yard in the M-S Zone on 3.10 acres.	49-640 Oates Lane	Julia Molinar P.O. Box 3091 Indio, CA. 92201 (760) 578-2108	PC Approved 4/20/16 1 st Phase Complete (Off-site improvements pending)
	AR	16-16	<u>County Dept. Public Social Services Bldg.</u> Construction of new 30,000 sq. ft. office building with parking lot.	51258 Harrison St.	Capital Partners Development Co. LLC 2890 Kilgore Road, Suite 175 Rancho Cordova, CA. 95670 (916) 834-4774	PC Approved 2/15/17 Under Construction
	AR TPM CZ EA CUP DA	16-19 37209 16-03 16-06 290	<u>Date Palm Business Park</u> To subdivide 80 acres into a multi-tenant industrial park.	East of Harrison Street, North and South of Avenue 49	Wesley Ahlgren Sundate/Anthony Vineyards P O Box 9578 Bakersfield CA 93389-9578 (760)406-4060	PC Approved 10/18/17 CC Approved 12/13/17 PC Approved (DA) 12/20/18 CC Approved (DA) 1/17/18
	AR DA	17-07	<u>Glenroy Resort - Project A</u> To construct a 76-unit Bungalow-Style Hotel with 160,656 sq. ft. on 24.43 acres in the CG zone.	SE Corner of Avenue 48 and Van Buren Street	Glenroy Coachella, LLC 1801 S. La Cienega Blvd. Los Angeles CA 90035 (310) 207-6990	PC Approved 6/7/17 CC Approved (DA) 10/11/17 Under Construction
	AR CZ VAR	17-12 17-02 17-06	<u>Coachella Village</u> To construct a new 242-unit multifamily residential project on 9.69 acres	South side of Ave 48, west of Van Buren St	David Schuman Coachella Village Partners, LLC 1014 S. Westlake Bl Ste 14 Westlake Village CA 91361 (213)321-9689	PC Approved 11/15/17 CC Approved 1/17/18
	AR	18-05	<u>Coachella Village - Phase #2</u> To construct a new 110-unit multifamily residential gated community on 4.51 acres of vacant land.	South side of Ave 48, west of Van Buren St	David Schuman Coachella Village Partners, LLC 1014 S. Westlake Bl Ste 14 Westlake Village CA 91361 (213)321-9689	PC Approved 10/17/18

City of Coachella Development Status Report

September, 2019

Item 6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
AR	18-06	<u>"Prado" by D.R. Horton</u> Construct 60 new single family homes inside the Prado Gated Community	South Side of Ave. 50 at Via Prado	D.R. Horton 2280 Wardlow Cir #100 Corona CA 92880 (951)739-5469	PC Approved 11/28/19 CC (Appeal) Approved 1/23/19 (In Litigation)
AR	18-10 (Admin)	<u>Golden State Energy Services</u> Construct new electrical substation	NE Corner Polk St. and Industrial Way	Golden State Energy Services 3421 Gato Ct. Riverside CA 92507 (951) 906-9865	Pending
AR	18-11	<u>Pueblo Viejo Plaza</u> Construct new 6,900 sq. ft. commercial center with two restaurants and retail suite.	Northwest corner of 9 th St. and Grapefruit Blvd.	Pedro Padilla 49305 Grapefruit Blvd Coachella, CA. 92236 (760) 972-2441	PC Approved 4/17/19
AR	19-01	<u>Oraway Engineering</u> To construct a new 825 sq. ft. office and contractor yard on 2.89 acres.	54-101 Enterprise Way	Armando Bravo 83-850 Corte Solis Coachella, CA. 92236 (760) 791-4383	PC Approved 4/17/19
AR	19-02	<u>Guardado Commercial Center</u> To construct a new 22,300 sq. ft. multi-tenant retail/office complex on 1 acre	Southeast Corner of Valley Rd. and Cesar Chavez Street	Jual Carlos Guardado 82204 Hwy 111 Ste A Indio, CA. 92201 (760) 578-3669	PC Approved 8/7/19
AR	19-03 (Admin)	<u>Vista Escondida – Phase 3</u> Review of 3 production home models ranging in size from 1,378 sq. ft. to 1,874 sq. ft.	North side of Avenue 54 east of Cesar Chavez Street (Tract 32264)	Mario Alberto Ornelas 2280 Wardlow Circle Ste 100 Corona CA 92880	Approved 8/27/19

City of Coachella

Development Status Report

September, 2019

Item6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
AR	19-04 (Admin)	<u>Club Leon</u> New trellis patio, pavilion w/ bbq island, restrooms and 2-car garage for existing residential estate	85601 Ave. 50	Eutemio Guerrero 50-782 Avenida Adobe Coachella CA 92236	Director Decision Pending
AR	19-05 (Admin)	<u>Desert Valley Date – Canopy</u> Construct two shade structures @ 15,000 sq. ft.	86-740 Industrial Way	George Kirkjan c/o Veju Lakhani 1173 E. Merlon St. Ontario CA 91761	Director Decision Pending
AR CZ	19-06 19-01	<u>Villa Verde Apartments</u> To construct 152 multifamily apartments on 9.25 acres of vacant land.	84-824 Calle Verde (SW Corner of Calle Techa & Calle Verde)	Villa Verde I., LP 1149 S. Hill St Suite 700 Los Angeles CA 90015 (213) 255-2815	Incomplete
CUP AR	254 (Mod) 12-07	<u>Gateway AM/PM Project</u> Construct carwash, drive-thru restaurant modification	Southwest corner of Ave. 48 and Grapefruit Blvd.	The Chandi Group USA P.O. Box 2817 Indio, CA. 92202 (760) 396-9260	PC approved 10/21/15 Under Construction
CUP EA DA	265 15-07	<u>Cultivation Technologies</u> To develop a 111,000 sq. ft. multi-tenant medical cannabis cultivation facility.	84-811 Avenue 48	Richard O'Connor Cultivation Technologies, Inc. 3 Park Plaza, Suite 490 Irvine, CA. 92614 (888) 851-9802	PC Approved 7/6/16 CC Approved (DA) 7-27-16 PC Approved Mod.-12/19/19 <u>Project Re-Entitled as:</u> Desert Research Park #4

City of Coachella

Development Status Report

September, 2019

Item6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP AR TTM TPM EA	266 16-03 37040 37083 16-01	<u>Coachella Vineyards Luxury RV Park</u> To develop a 185-Lot RV Park and subdivision on 29.41 acres of vacant land	NE Corner of Tyler Street and Vista Del Norte	Patty Nugent Coachella Vineyards Luxury RV Park, LLC 45-920 Meritage Lane Coachella, CA. 92236 (760) 289-5279	PC Approved 7/18/18 CC Approved 9/26/18 Under Construction
CUP CUP AR TTM EA	267 268 16-05 37088 16-02	<u>Ravella - To develop 20 acres</u> Planned Development with a service station on 5 acres of Neighborhood Commercial center, and 115 residences on 15 acres.	NW Corner of Avenue 50 & Calhoun Street	Tower Energy Group 1983 W. 190 th St., Ste. 100 Torrance, CA. 90504 (310) 535-8000	P.C. Approved 6/15/16 CC Approved 7/13/16 1 st Phase completed.
CUP	268 (Mod.)	<u>Borrego Health</u> To modify the Ravella/Tower Energy Planned Development to allow a new 40,919 sq. ft., 2-story, medical clinic with 294 parking spaces on approx. 4 acres in Tract 37088-1	NW Corner of Avenue 50 & Calhoun Street	Tower Energy Group 1983 W. 190 th St., Ste. 100 Torrance, CA. 90504 (310) 535-8000	P.C. Approved 4/17/19
CZ CUP AR VAR	16-02 271 15-14 16-01	<u>Coachella Grow Assoc.</u> To build a 29,180 sq. ft. medical cannabis cultivation and fabrication facility, with reduced street frontage.	84-775 Avenue 48	Jesus Gonzalez Coachella Grow Association P O Box 1144 Coachella, CA 92236 (760) 766-6233	PC Approved 9/21/16 CC Approved 10/12/16 Time Extension – 10/12/19
CUP	273	<u>Our Lady Of Soledad</u> To construct a new 21,029 sq. ft. church and future 11,870 sq. ft. social hall in the C-G and R-M zone.	52-980 Harrison Street	Diocese of San Bernardino 1201 E. Highland Ave. San Bernardino CA 92404 (909) 475-5052	PC Approved 11/16/16 Under Construction

City of Coachella Development Status Report

September, 2019

Item6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP AR	275 16-14	<u>Mosque & Assembly Hall</u> To construct a new 20,260 square foot mosque to be built in two phases.	84-650 Avenue 49	<u>Shakil Patel</u> 25982 Hinkle St Loma Linda CA 92354 (909)796-0300	PC Approved 12-21-16 In Plan Check
CUP AR EA	276 16-18 16-05	<u>Coachella Warehouses</u> To construct a 255,800 sq. ft. multi-tenant medical cannabis cultivation facility	84-851 Avenue 48	<u>Kevin Stumm</u> PCC Industrial/Commercial 760 Garden View Court, Suite 200 Encinitas, CA 92024 (760)452-8075	PC Approved 12/21/16 CC Approved 02/08/17 Under Construction
CUP AR VAR	278 17-02 17-03	<u>Kismet Organic</u> To construct a phased 77,400 sq. ft. medical cannabis cultivation facility.	48-050 Harrison Street	Ed Sapigao 5151 California Ave Ste 100 Irvine CA 92617 (949)280-4782	PC Approved 7/19/17 CC (Appeal) Approved 9/27/17 Under Construction
CUP CUP CUP	284 285 286	<u>Mobilitie Mono-Pole</u> To install 3 new wireless communications poles in the street right-of-way	SEC of Ave 52 & Tyler SWC Ave 50 & Harrison NWC Westerfield & Harrison	Robert Lewis Mobilitie, LLC 2955 Red Hill Ave Ste 200 Costa Mesa CA 92626 (951) 212-5825	PC Approved 12/21/17 CUP 284 Withdrawn
CZ CUP AR EA	17-01 279 17-03 17-01	<u>Coachella Green LLC</u> To construct a phased 833,829 sq. ft. medical cannabis industrial park.	86-601 Industrial Way	<u>Barry Walker</u> 1933 S. Broadway No. 806 Los Angeles CA 90007 (323) 997-9914	PC Approved 11/15/17 CC Approved 11/29/17
CUP AR	289 17-11	<u>Coachella Brands</u> To construct a new 91,948 sf cannabis cultivation facility in the MW zone.	84-805 Ave. 48	<u>Richard O'Connor</u> 2 Curie Ct Rancho Mirage CA 92270 (760)409-6464	AR Approved 8/30/17 CUP Approved 11/1/17 Interim Use In Operation

City of Coachella

Development Status Report

September, 2019

Item 6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP AR TPM EA	280 17-04 37266 17-02	<u>Coachella Research Park #2</u> To develop an 833,829 sq. ft. multi-tenant medical cannabis cultivation facility, with industrial condo lots, on 20 acres in the MW zone.	48-451 Harrison Street	<u>Michael Meade</u> Desert Rock Development 72-100 Magnesia Falls Dr. Ste 2 Rancho Mirage CA 92270 (760)837-1880	PC Approved 10/4/17 CC Approved 10/25/17
CUP AR DA VAR	280 (Mod) 17-04 (Mod) 18-05	<u>Coachella Research Park #2</u> To allow two 3-story industrial buildings (98,520 sf) and four greenhouses (404,308 sf) for cannabis cultivation uses.	48-451 Harrison Street	<u>Michael Meade</u> Desert Rock Development 72-100 Magnesia Falls Dr. Ste 2 Rancho Mirage CA 92270 (760)837-1880	PC Approved 11/28/18 CC Approved 2/13/19
CUP AR TPM EA	281 17-05 37265 17-03	<u>Coachella Research Park #1</u> To develop an 311,250 sq. ft. multi-tenant medical cannabis cultivation facility, with industrial condo lots on 11.28 acres in the MW zone.	NE Corner of Ave. 48 and Harrison Street	<u>Michael Meade</u> Desert Rock Development 72-100 Magnesia Falls Dr. Ste 2 Rancho Mirage CA 92270 (760)837-1880	PC Approved 10/4/17 CC Approved 10/25/17 Under Construction
CUP TPM EA	282 37333 17-04	<u>REI / Ponte Hotel Project</u> To develop a 5.45 acre site with 2 hotels, drive-thru pad, and restaurant.	NE Corner of Vista Del Norte and Dillon Road	Claudio Ponte 726 W. Ventura Blvd Ste F Camarillo CA 93010 (805)383-0367	PC Approved 7/19/17 CC Approved 8/9/17
CUP	291	<u>Mobilitie</u> To install a wireless communications pole in the street right-of-way	East side of Jackson St, south of Ave 48	Robert Lewis Mobilitie 2955 Red Hill Ave Ste 200 Costa Mesa CA 92626-1205 (951)212-5825	PC Approved 12/21/17

City of Coachella Development Status Report

September, 2019

Item6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP CUP CZ	299 300 18-03	<u>HOTN/Sinsemilla Dispensary & Pub Project</u> Proposed 1,839 sq. ft. retail cannabis dispensary and 1,432 sq. ft. bar/pub	1694 6 th Street	Pedro Ordoñez 6 th Street Tap Room 45631 Capistrano St Indio CA 92201 (760)409-6169	PC Approved 3/20/19 CC Approved 5/8/19
CUP	180 (Mod #2)	<u>Love's Country Store</u> To allow a new truck parking lot north of Love's Country Store	West side of Dillon Rd, north of Vista Del Norte	Aaron Oliver 10601 N. Pennsylvania Ave Oklahoma City OK 73120 (559)688-5263	PC Approved 12/21/17 In Plan Check
CUP	180 (Mod #3)	<u>Love's Country Store</u> To allow a 900 sf addition to existing auto repair facility	NS Corner of Dillon Rd and Vista Del Norte	Lonnie Spicer HFA 1705 S. Walton Bl Ste 3 Bentonville AR 72712 (479)273-7780 ext. 299	PC Approved 12/21/17 In Plan Check
CUP	292	<u>Verizon Wireless</u> To install a new wireless communications pole at Bagdouma Park	Northeast corner of Frederick St and Ave 52	Bryce Novak 7711 Normal Ave La Mesa CA 91941	PC Approved 1/31/18
CUP AR	293 17-14	<u>Coachella Herb Plantation</u> To construct a new 67,240 sf medical cannabis cultivation facility on 3.2 acres in the MW zone.	84-801 Ave 48	Marcelo Morales 84-801 Ave. 48 Coachella CA 92236 (760)699-1332	PC Approved 11/28/17 Time Extention to 11/28/18 Interim Use Under Construction

City of Coachella

Development Status Report

September, 2019

Item 6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP	294	<u>Chelsea Mixed Use Project</u> To establish "PD" land use regulations and development standards for TOD project (105 Apartments w/ 3,000 sf commercial and Transit Hub)	East side of Harrison Street, north of 6 th St	Walter Heiberg Chelsea Investment Corp 6339 Paseo Del Lago Carlsbad CA 92011 (760)456-6000	PC Approved 12/21/17 CC Approved 11/29/18 Modification Approved 1/30/19
CUP AR VAR	297 18-01 18-01	<u>CoachellaGro Corp.</u> To construct a new 257,051 sf cannabis cultivation facility on 10.99 acres of vacant land in the M-W zone.	East side of Harrison Street, South of Ave. 48	CoachellaGro Corp. 3060 Saturn St #250 Brea CA 92821 (760)940-0100	PC Approved 6/6/18
CZ CUP AR VAR	18-09 307 18-07 18-04	<u>Polk Cannabis Redevelopment Project</u> To construct a new 174,500 sf cannabis cultivation facility on 10 acres of land.	53-800 Polk St.	Peter Solomon 53-800 Polk St Coachella CA 92236 (760)895-2621	PC Approved 11/7/18 CC Approved 12/12/18 (In Litigation)
CUP	301	<u>AT & T Wireless Antenna</u> To install a new wireless communications mono-palm at 50 ft high	Jackson Square SE Corner of Jackson St. and Ave. 48	AT & T c/o Melissa Francisco 19519 Jacob Ave. Cerritos CA 90703 (562)972-5161	PC Approved 9/19/18
CUP	302	<u>AT & T Wireless Antenna</u> To install a new wireless communications mono-palm at 50 ft high	Bagdouma Park 51-711 Douma St.	AT & T c/o Melissa Francisco 19519 Jacob Ave. Cerritos CA 90703 (562)972-5161	PC Approved 9/19/18

City of Coachella Development Status Report

September, 2019

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<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP CZ	298 18-02	<u>Coachella Smoke Co.</u> To allow a new 1,296 sf retail cannabis micro-business	85-995 Grapefruit Blvd Suite #1	Nick Meza 50-580 Calle Mendoza Coachella CA 92236 (760)541-9034	Pending
CUP AR EA TPM	313 18-13 18-06 37670	<u>Luxor Luxury RV Storage</u> To develop an indoor 123,940 sq. ft. indoor RV storage facility with repair and washing service, and caretaker unit	49-751 Oates Lane	Luxtor Limited, LLC 450 Kansas Street #104 Redlands CA 92373 (909)556-5848	PC Approved 3/6/19 CC Approved 7/10/19 In Plan Check
CUP AR EA	308 18-08 18-03	<u>Red Moon RV Park</u> To develop a 78.3-acre RV Resort with 46 long-term rental spaces and 390 short-term rental spaces	44-790 Dillon Rd.	Jim Komick Red Moon Dev. & Construction 4320 Priceless View Dr. Gold Canyon AZ 85118 (480)947-9253	PC Approved 3/06/19 In Plan Check
TTM EA CUP CZ GPA	32263 05-11 207 05-04 07-04	322 SF Units Subdivision 64.64 acres See AR 05-21 Nickel creek	Ave 44, West of Dillon	Coachella-Nickel Creek LLC 5744 Canyon Road Amarillo, TX. 79109	PC approved 9/5/07 CC approved 9/12/07 SB1185 Ext to 9/12/10 AB333 Ext to 9/12/12 AB208 Ext to 9/12/14 AB116 Ext. to 9/12/16 1 Yr. Ext. to 9/12/17 1 Yr. Ext. to 9/12/18 PC Extended to 9/12/19

City of Coachella Development Status Report

September, 2019

Item6.

<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
CUP	314	<u>C'Los Restaurant Liquor</u> To allow on-sale beer, wine and spirits alcohol license	51-335 Cesar Chavez Street Suite #117	Juan Carlos Alba 45-676 Panorama Dr. Palm Desert CA 92260	PC Approved 8/7/19
CUP	316	<u>JC Burger – Alcohol Sales</u> To allow on-sale beer, wine and spirits w/ eating establishment	49-405 Grapefruit Blvd	Jesus Corraler 84-463 Da Vinci Dr Coachella CA 92236 (760)238-1010	Pending
TTM AR EA CUP CZ GPA Annex	35523 07-13 07-16 231 07-08 07-05 60	<u>Villa Palmeras</u> 111 Single Family attached & detached residential 11.58 acres	South side of Ave 50 between Jackson St & Calhoun St	DSE Turco, L.P. Attn: Scott Carino 8910 University Center Ln Suite 690 San Diego CA 92122 (858) 458-3432 (858) 458-3412 FAX	PC approved 1/16/08 CC approved 5/28/08 <i>SB1185 Ext to 5/28/11</i> <i>AB 333 Ext to 5/28/13</i> <i>AB 208 Ext to 5/28/15</i> <i>AB116 Ext to 5/28/17</i> <i>1 yr. Ext. to 5/28/18</i> <i>2nd yr. Ext to 5/28/19</i>
TTM	36555	<u>La Obra (Prado Ph. 2)</u> 107 lot SFR lots on 27 acres.	Northside of Ave. 51 between Van Buren & Chiapas Dr.	Sunwood Prado LLC 9740 Appaloosa Road, Suite 130 San Diego, CA. 92131 (619) 573-1805	PC scheduled 7/16/14 PC approved 9/03/2014 CC approved 9/24/2014 <i>1 Yr. Ext. to 9/24/17</i> <i>1 Yr. Ext. to 9/24/18</i> <i>1 Yr. Ext to 9/24/19</i>
TTM	36757	<u>Rancho Las Flores Ph. 2</u> 124 lot SFR lots on 31 acres	Northeast corner of Ave 49 and Van Buren St.	Glenroy Coachella, LLC 9601 Wilshire Blvd. #260 Beverly Hills, CA. 90210 (310) 461-3232	PC denied 07/16/2014 CC approved 10/08/2014 <i>1 Yr. Ext. to 10/08/17</i> <i>2nd Yr. Ext. to 10/08/18</i> <i>3rd Yr. Ext. to 10/08/19</i>

City of Coachella Development Status Report

September, 2019

Item 6.

	<i>Case</i>	<i>No.</i>	<i>Name /Description</i>	<i>Location</i>	<i>Contact Person</i>	<i>Approval Date & Status</i>
	SP GPA CZ EA TTM	14-01 14-01 14-01 14-04	<u>Vista del Agua Specific Plan</u> 280-acre subdivision with single-family residential, multi-family residential and commercial development.	S of Vista del Sur N of Ave. 48, E of Tyler St., W of Polk Street	James Kozak Strategic Land Partners, LP 12671 High Bluff Dr., Suite 150 San Diego, Ca. 92130 (858) 699-7440	PC Workshop 3/20/19 PC Approved 6/19/19
	VAR	18-02	<u>Desert Research Park #1</u> To exceed height limit for 3 new industrial buildings.	NE Corner of Ave. 48 and Harrison St.	Michael Meade Desert Rock Development 72-100 Magnesia Falls Rancho Mirage CA 92270 (760)837-1880	PC Approved 4/18/18

City of Coachella
Development Status Report
September, 2019

Item 6.

PC = Planning Commission

CC = City Council

Types = RS Single Family Residential, RM Multifamily Residential, CG Commercial General, NC Neighborhood Commercial

MH Manufacturing Heavy, MS Manufacturing Service, MW Wrecking Yard, RE Residential Estate

Status of Projects = Under Construction, Approved, Pending Approval

Status of Maps = Constructed, Under Construction, Recorded, Approved Tentative, Pending Approval

AR Architectural Review

CUP Conditional Use Permit

CZ Change of Zone

DA Development Agreement

EIS (EA) Environmental Initial Study (Environmental Assessment)

GPA General Plan Amendment

PD Planned Development

TTM Tentative Tract Map or Tentative Subdivision Map

TPM Tentative Parcel Map

VAR Variance

Tentative Maps (Tract and Parcel) are approved for 2 years and may be extended an additional year three times upon approval of their request and payment of application fees.

Senate Bill (SB 1185) automatically grants a 12 month extension to any Tentative Tract or Tentative Parcel Map that was valid as of July 15, 2008 and will normally expire before January 1, 2011.

Assembly Bill 333 (AB333) automatically grants a 24 month extension to any Tentative Tract or Tentative Parcel Map that was valid as of July 15, 2009 and will normally expire before January 2, 2012.

Assembly Bill 208 (AB208) automatically grants a 24 month extension to any Tentative Tract or Tentative Parcel Map that was valid as of July 15, 2011 and will normally expire before January 1, 2014.

Assembly Bill 116 (AB116) automatically grants a 24-month extension to any Tentative Tract or Tentative Parcel Map that was valid as of July 11, 2013.

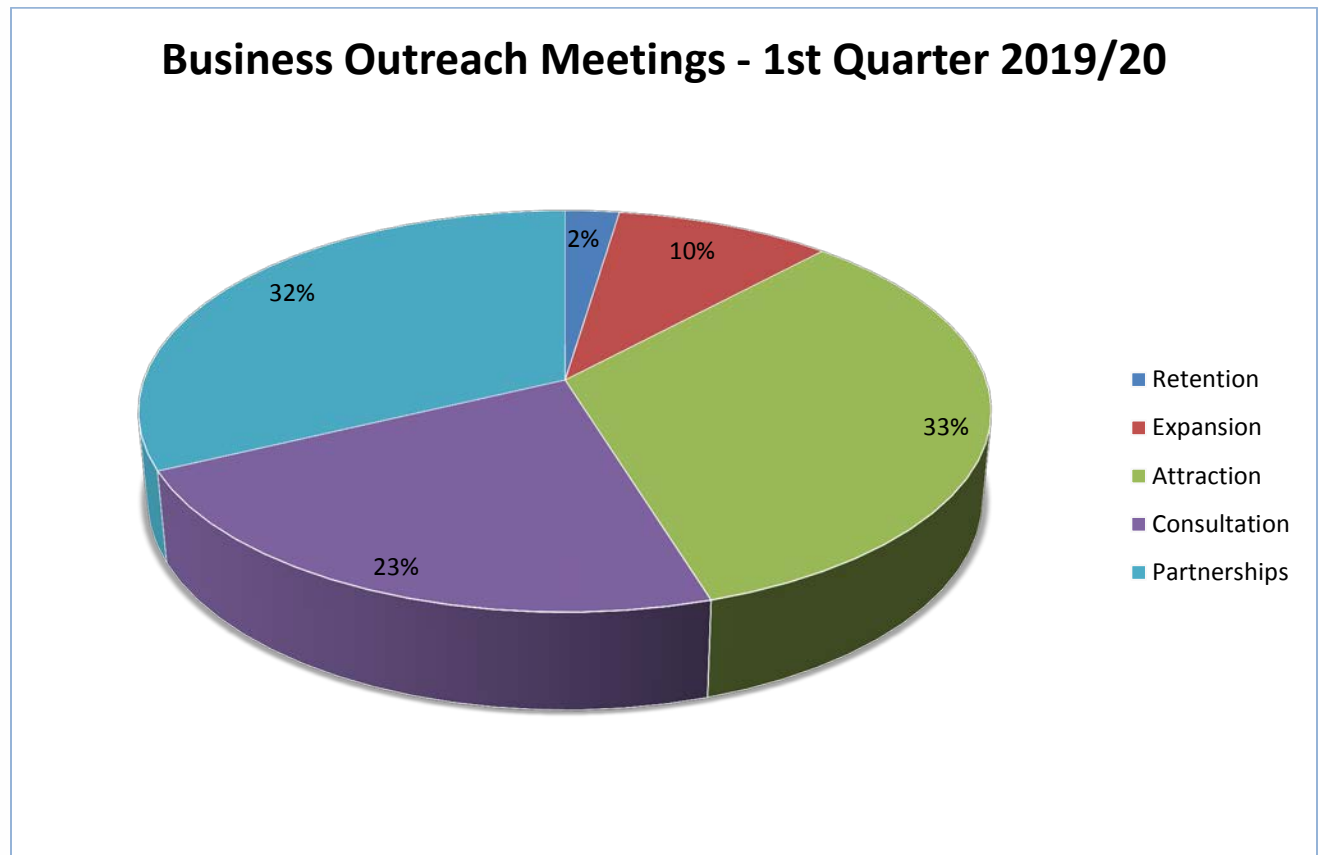


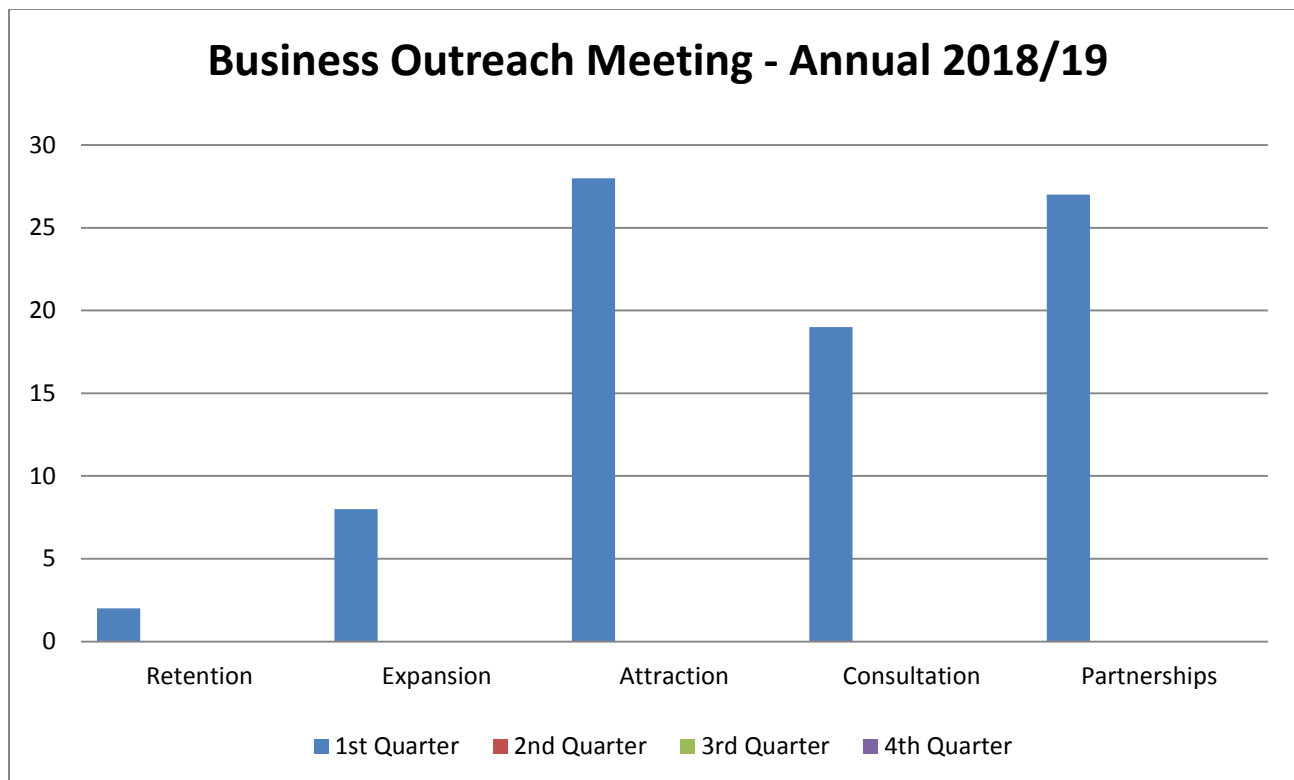
ECONOMIC DEVELOPMENT DEPARTMENT

Introduction:

The City of Coachella is a progressive, service-oriented and responsive partner to the business community. The Economic Development Department's staff facilitates and fosters the retention, attraction, and expansion of businesses by communicating and providing business-friendly programs and services that are aimed toward a balanced economy, an increase in tax revenues and a positive City image. The City of Coachella, known as the City of Eternal Sunshine, is a business-friendly community with good proximity to local, regional and international markets.

Economic Development Activity:





Economic Development Events:

Event:	Organization:	Date:	Location:
Spark Program Workshop	Accelerator for America	July 25, 2019	Mercado La Paloma, Los Angeles, CA
Funding Fair for Infrastructure Project	Coachella Valley Water District	August, 14, 2019	CVWD Palm Desert, CA
Regions Rise Together	Go-Biz	September 13, 2019	CSU San Bernardino, San Bernardino, CA
ICSC Western Conference	ICSC	September 15 – 18, 2019	LA Convention Center, Los Angeles, CA
All Valley Mayor & Tribal Chair Lunch	Greater Coachella Valley Chamber of Commerce	September 26, 2019	The Riviera Palm Springs, CA



City of Coachella
2019/2020 Economic Development Report
1st Quarter



Contents

Economy Overview	1
Historic & Projected Trends	3
Population Characteristics	5
Industry Characteristics	7
Business Characteristics	13
Workforce Characteristics	14
Educational Pipeline	20

Economy Overview

43,990

Population (2018)

Population grew by **3,605** over the last 5 years and is projected to grow by **4,044** over the next 5 years.

10,144

Jobs (2018)

Jobs grew by **580** over the last 5 years and are projected to grow by **99** over the next 5 years.

\$44.7K

Avg. Earnings Per Job (2018)

Regional average earnings per job are **\$21.0K below** the national average earnings of \$65.7K per job.

Takeaways

- As of 2018 the region's population **increased by 8.9%** since 2013, growing by 3,605. Population is expected to **increase by 9.2%** between 2018 and 2023, adding 4,044.
- From 2013 to 2018, jobs **increased by 6.1%** in Coachella Zip Code from 9,565 to **10,144**. This change **fell short of the national growth rate of 8.2% by 2.1%**. Labor force data is not available for Coachella Zip Code.
- There is no education attainment data for Coachella Zip Code.
- The top three industries in 2018 are Support Activities for Crop Production, Crop Production, and Restaurants and Other Eating Places.

	Population (2018)	Labor Force (2018)	Jobs (2018)	Cost of Living	GRP	Imports	Exports
Region	43,990	N/A	10,144	126.8	\$938.79M	\$2.34B	\$1.44B
California	39,853,042	19,658,856	19,646,487	133.8	\$2.88T	\$1.40T	\$2.01T
United States	328,038,851	162,488,483	162,744,861	100.0	\$20.15T	\$0	\$8.59T

Economy Overview - Cont.

2018 Labor Force Breakdown

Population breakdown data is not available at the ZIP level. Please choose a different region to see this data.

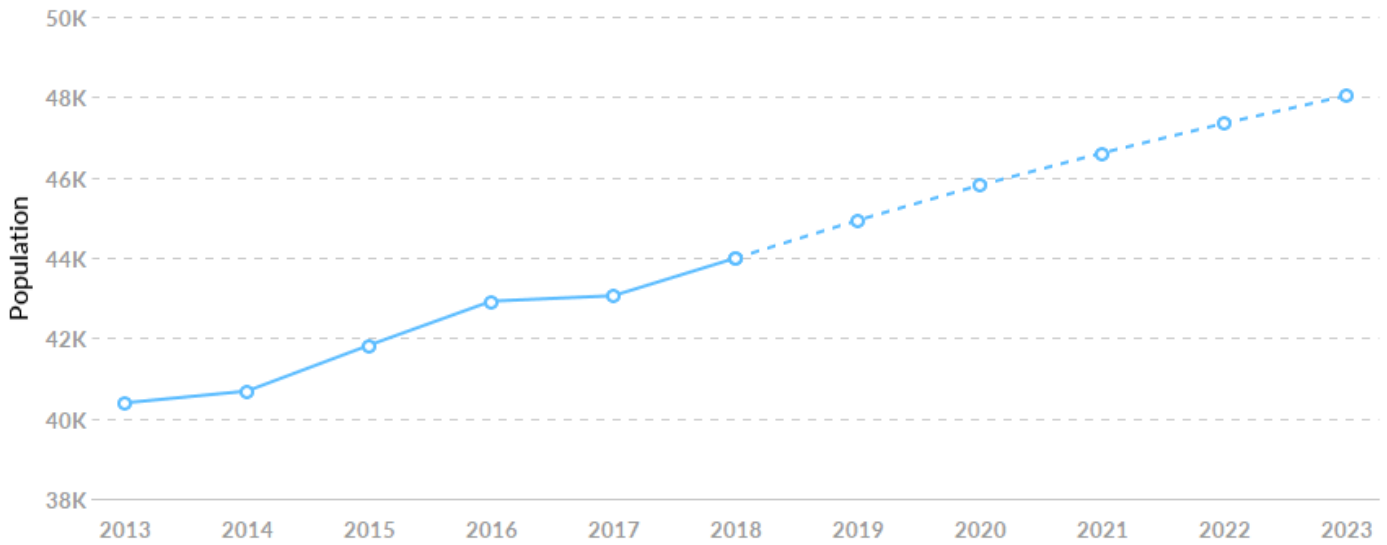
Educational Attainment

Educational attainment data is not available at the ZIP level. Please choose a different region to see this data.

Historic & Projected Trends

Population Trends

As of 2018 the region's population increased by 8.9% since 2013, growing by 3,605. Population is expected to increase by 9.2% between 2018 and 2023, adding 4,044.

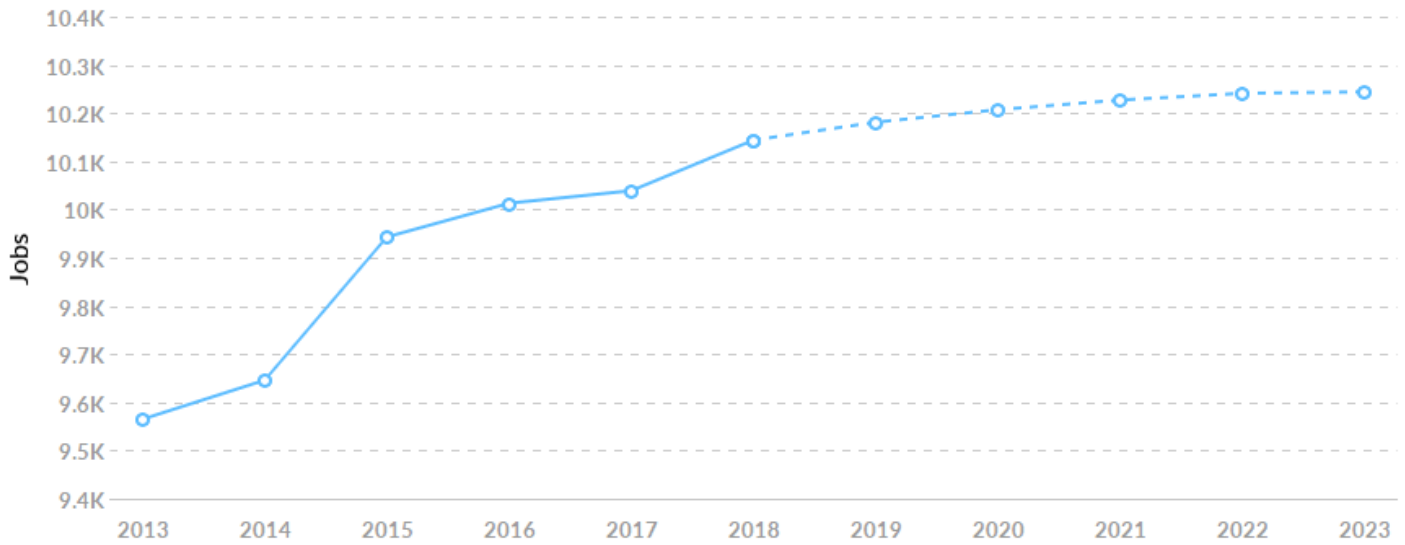


Timeframe	Population
2013	40,385
2014	40,679
2015	41,821
2016	42,917
2017	43,054
2018	43,990
2019	44,932
2020	45,805
2021	46,606
2022	47,345
2023	48,034

Historic & Projected Trends - Cont.

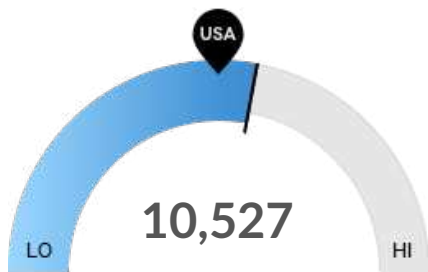
Job Trends

From 2013 to 2018, jobs increased by 6.1% in Coachella Zip Code from 9,565 to 10,144. This change fell short of the national growth rate of 8.2% by 2.1%.



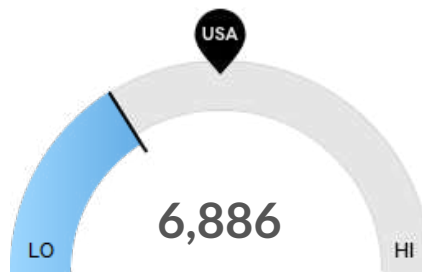
Timeframe	Jobs
2013	9,565
2014	9,646
2015	9,943
2016	10,013
2017	10,039
2018	10,144
2019	10,181
2020	10,207
2021	10,227
2022	10,241
2023	10,244

Population Characteristics



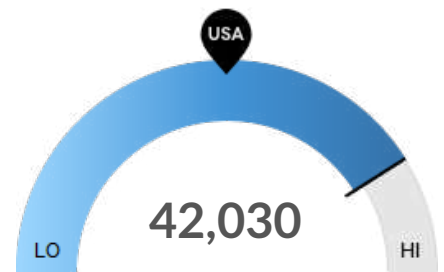
Millennials

Coachella Zip Code has 10,527 millennials (ages 20-34). The national average for an area this size is 8,917.



Retiring Soon

Retirement risk is low in Coachella Zip Code. The national average for an area this size is 12,274 people 55 or older, while there are 6,886 here.



Racial Diversity

Racial diversity is high in Coachella Zip Code. The national average for an area this size is 16,908 racially diverse people, while there are 42,030 here.



Veterans

Veterans data is not available at the ZIP level. Please choose a different region level to see this data.



Violent Crime

Crime data is not available at the ZIP level. Please choose a different region level to see this data.



Property Crime

Crime data is not available at the ZIP level. Please choose a different region level to see this data.

Population Characteristics - Cont.

Place of Work vs Place of Residence

Understanding where talent in Coachella, CA (in Riverside county) currently works compared to where talent lives can help you optimize site decisions.



Where Talent Works

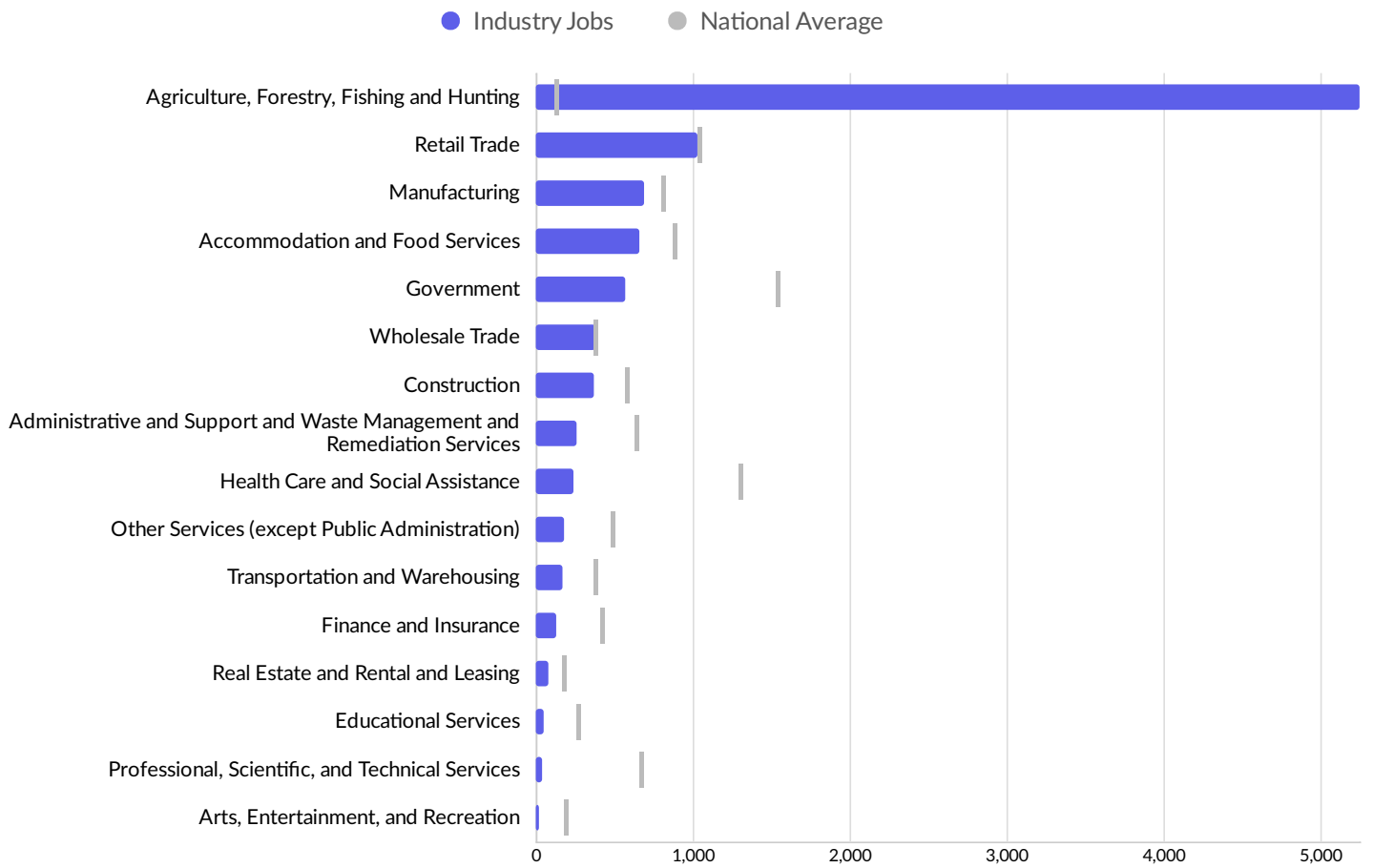
Where Talent Lives

ZIP	Name	2018 Employment
92236	Coachella, CA (in Rivers...	10,144

ZIP	Name	2018 Workers
92236	Coachella, CA (in Rivers...	18,017

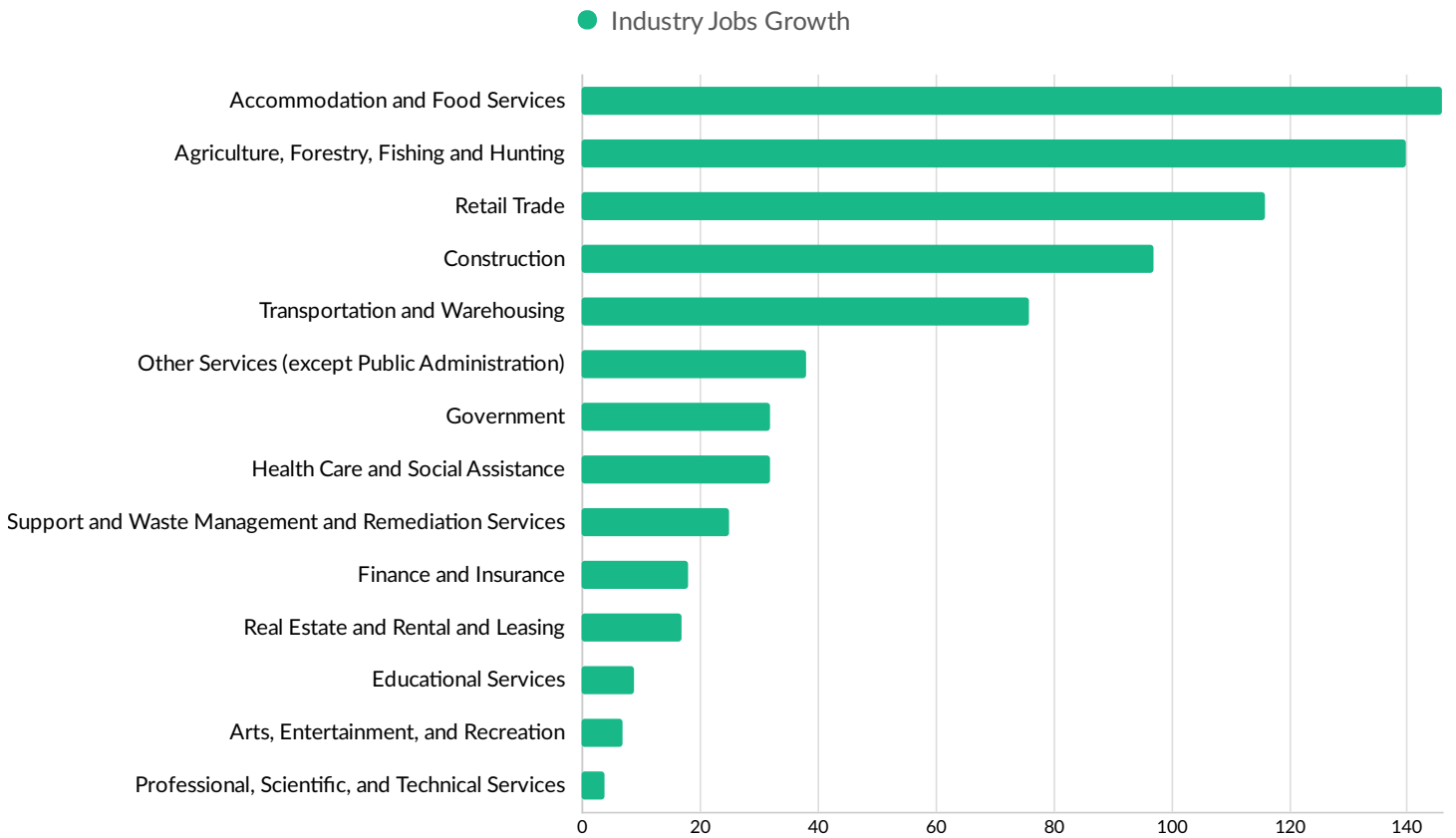
Industry Characteristics

Largest Industries



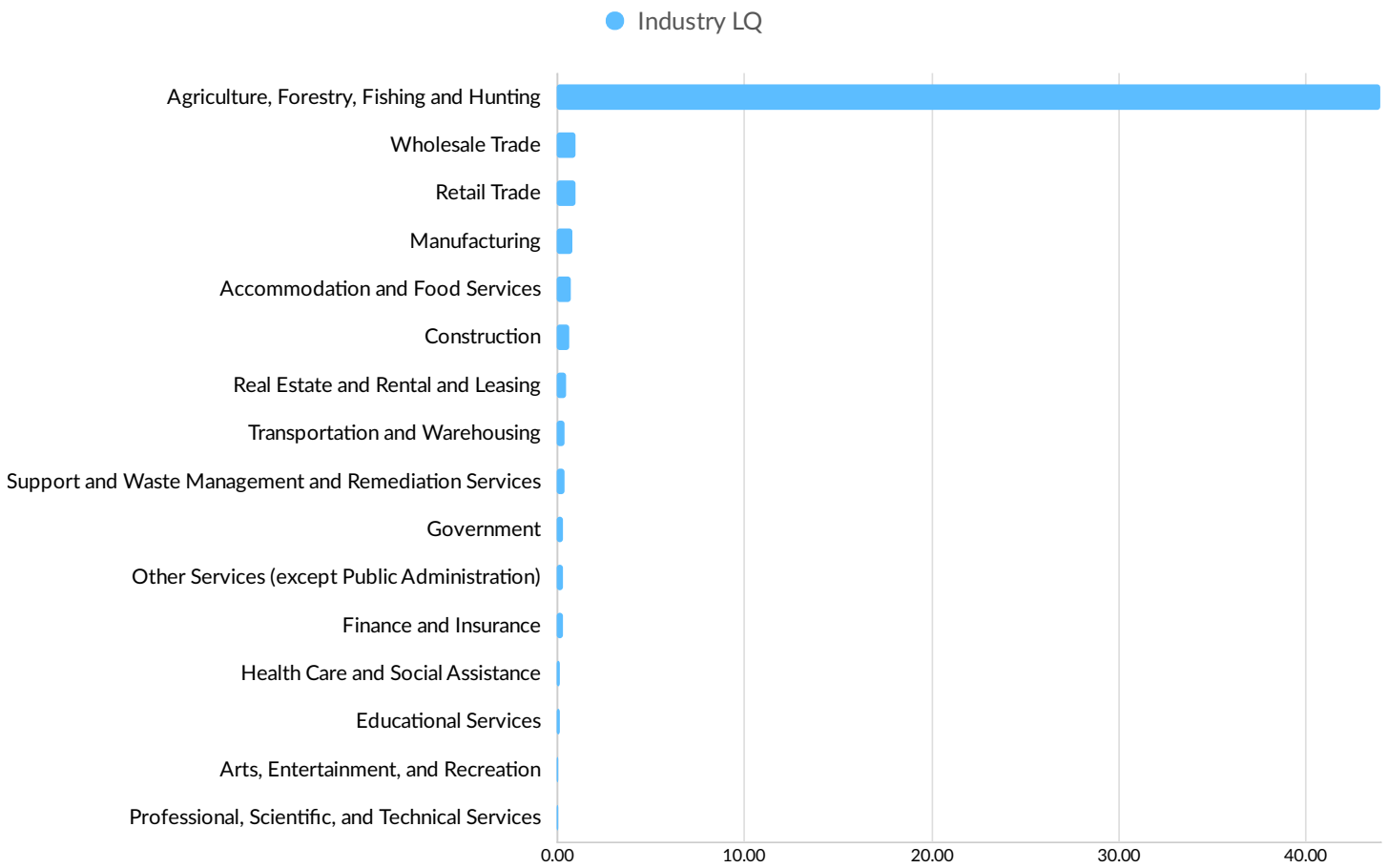
Industry Characteristics - Cont.

Top Growing Industries



Industry Characteristics - Cont.

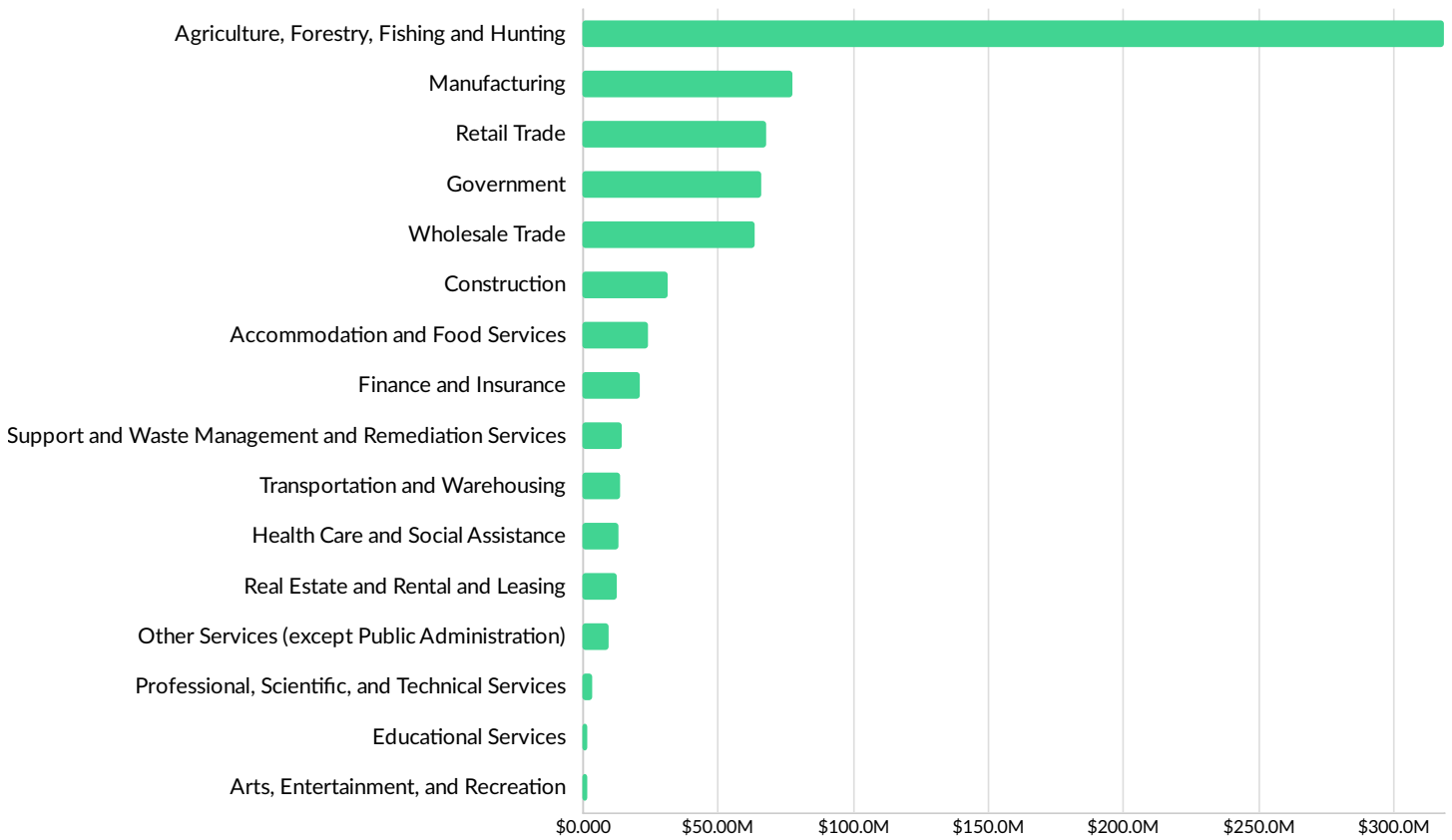
Top Industry LQ



Industry Characteristics - Cont.

Top Industry GRP

● 2018 Gross Regional Product



Industry Characteristics - Cont.

Top Industry Earnings

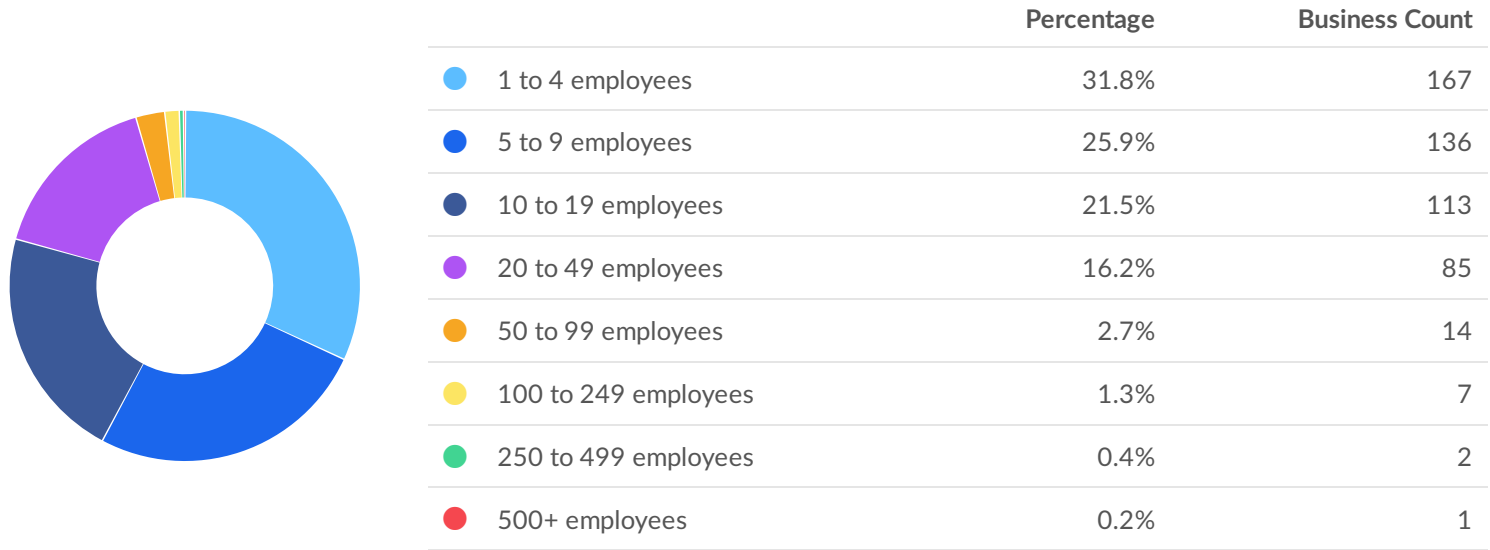


Industry Characteristics - Cont.

Industry	2013 Jobs	2018 Jobs	Change in Jobs	% Change in Jobs	2018 LQ	2018 Earnings Per Worker	2018 GRP
Agriculture, Forestry, Fishing and Hunting	5,107	5,247	140	+3%	44.00	\$38,567	\$318.72M
Retail Trade	914	1,030	116	+13%	1.00	\$35,042	\$68.02M
Manufacturing	771	689	-82	-11%	0.86	\$61,949	\$77.72M
Accommodation and Food Services	511	657	146	+29%	0.75	\$22,013	\$24.39M
Government	538	570	32	+6%	0.37	\$99,318	\$66.18M
Wholesale Trade	488	377	-111	-23%	1.01	\$62,339	\$63.55M
Construction	273	370	97	+36%	0.65	\$61,095	\$31.66M
Administrative and Support and Waste Management and Remediation Services	239	264	25	+10%	0.42	\$38,334	\$14.76M
Health Care and Social Assistance	211	243	32	+15%	0.19	\$42,166	\$13.48M
Other Services (except Public Administration)	142	180	38	+27%	0.37	\$41,522	\$10.02M
Transportation and Warehousing	89	165	76	+85%	0.45	\$50,721	\$13.70M
Finance and Insurance	109	127	18	+17%	0.31	\$63,750	\$21.03M
Real Estate and Rental and Leasing	65	82	17	+26%	0.48	\$49,039	\$12.63M
Educational Services	39	48	9	+23%	0.18	\$33,644	\$1.70M
Professional, Scientific, and Technical Services	35	39	4	+11%	0.06	\$53,090	\$3.36M
Arts, Entertainment, and Recreation	13	20	7	+54%	0.11	\$27,265	\$1.58M

Business Characteristics

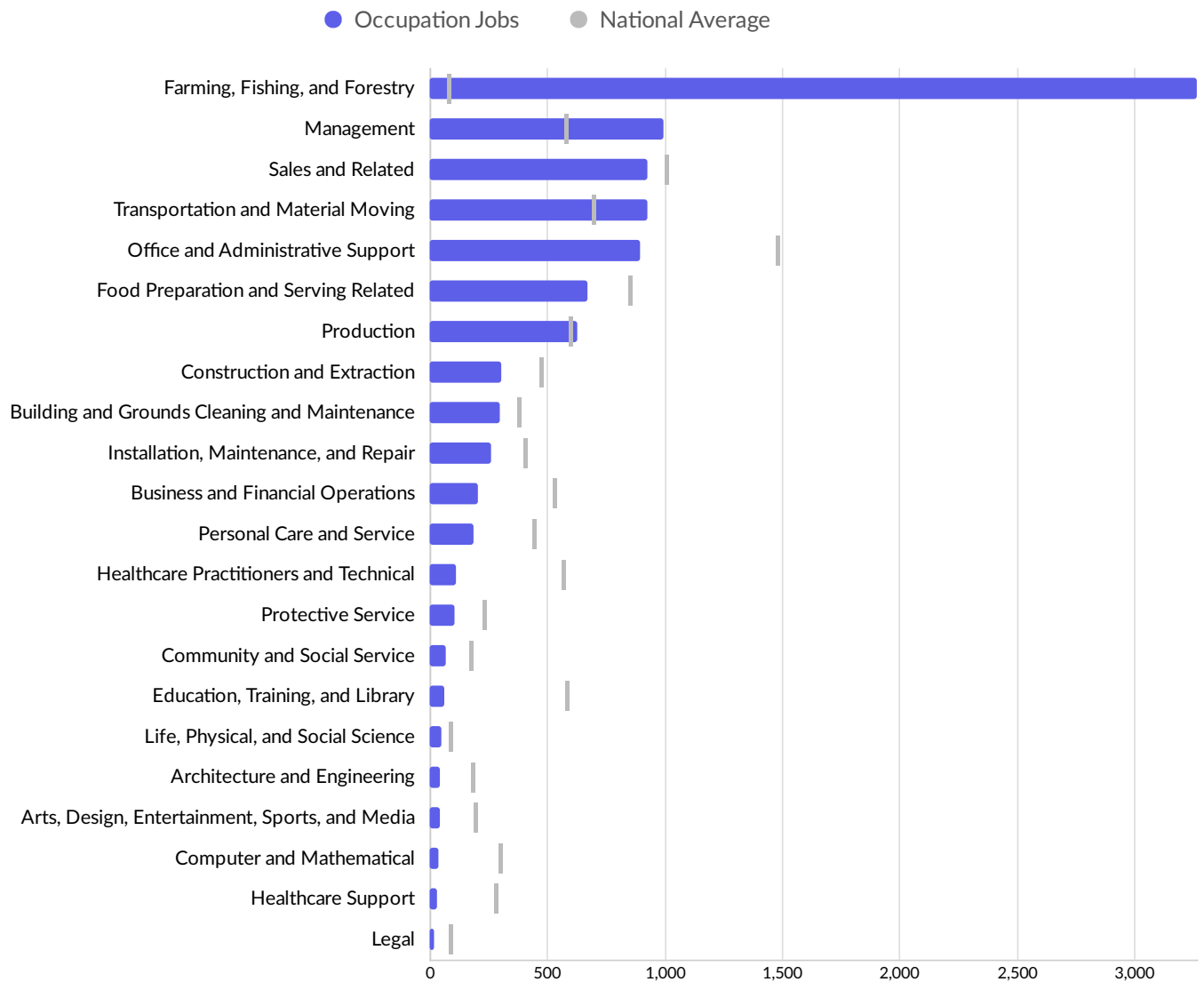
Business Size



*Business Data by DatabaseUSA.com is third-party data provided by Emsi to its customers as a convenience, and Emsi does not endorse or warrant its accuracy or consistency with other published Emsi data. In most cases, the Business Count will not match total companies with profiles on the summary tab.

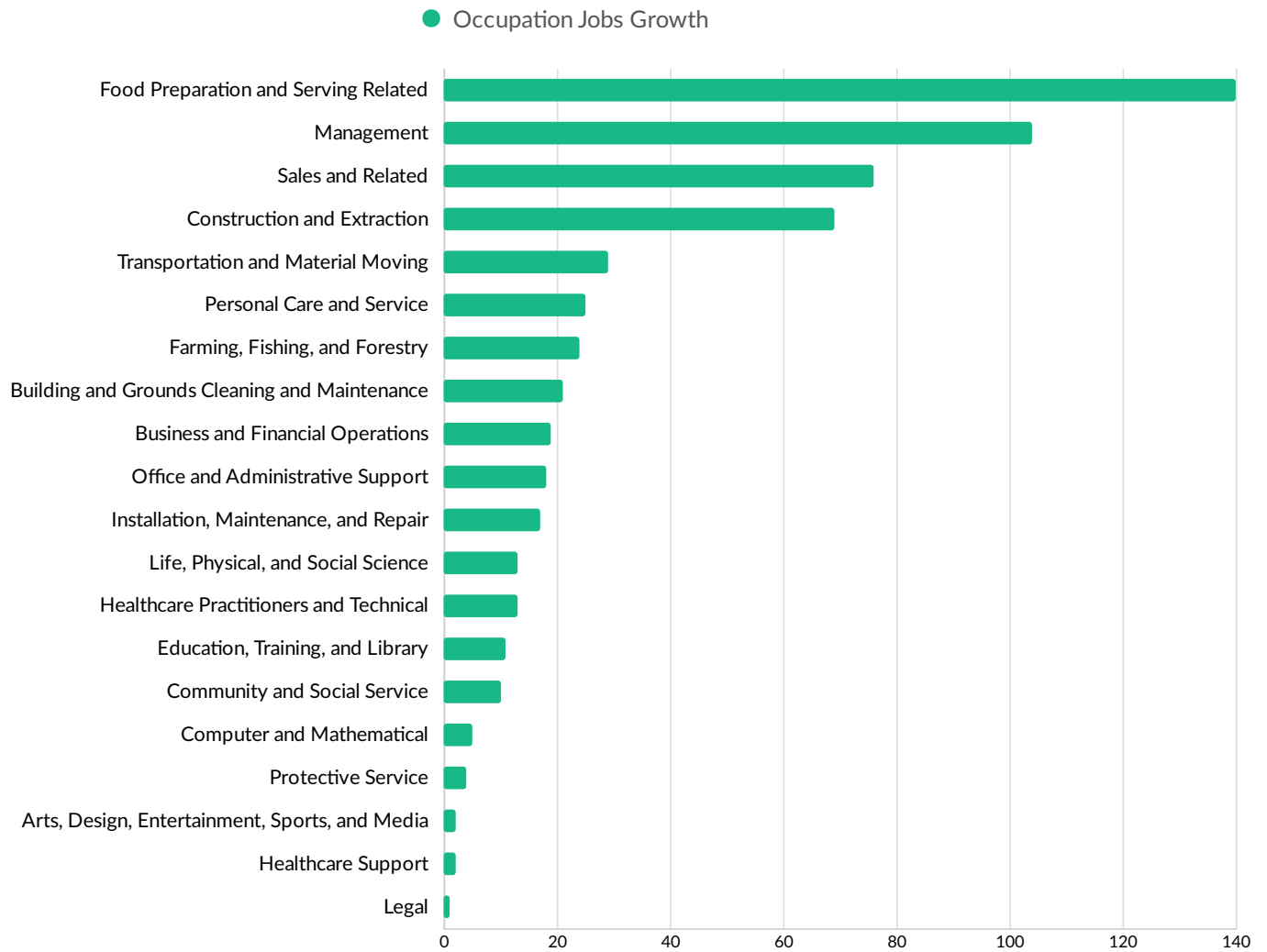
Workforce Characteristics

Largest Occupations



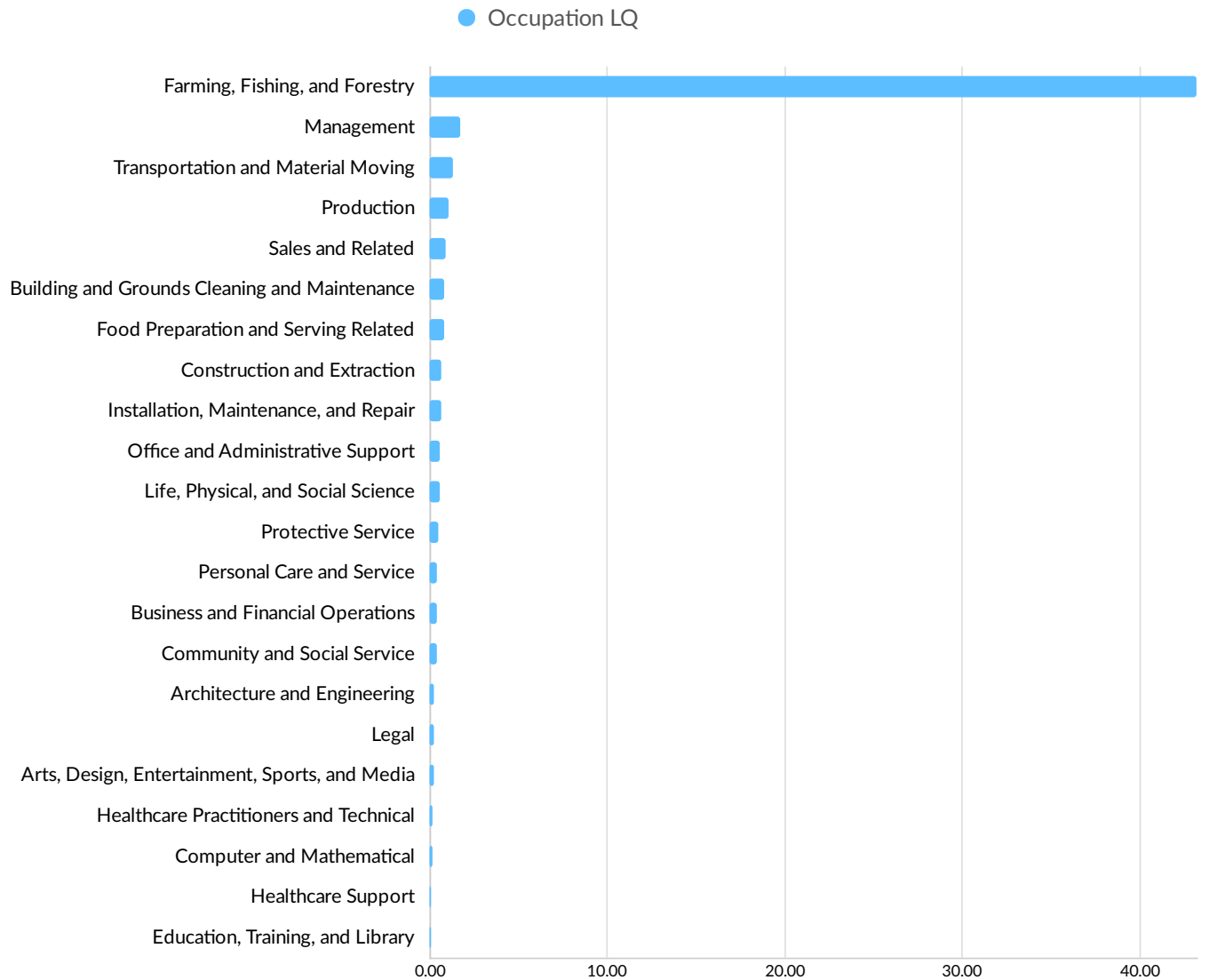
Workforce Characteristics - Cont.

Top Growing Occupations



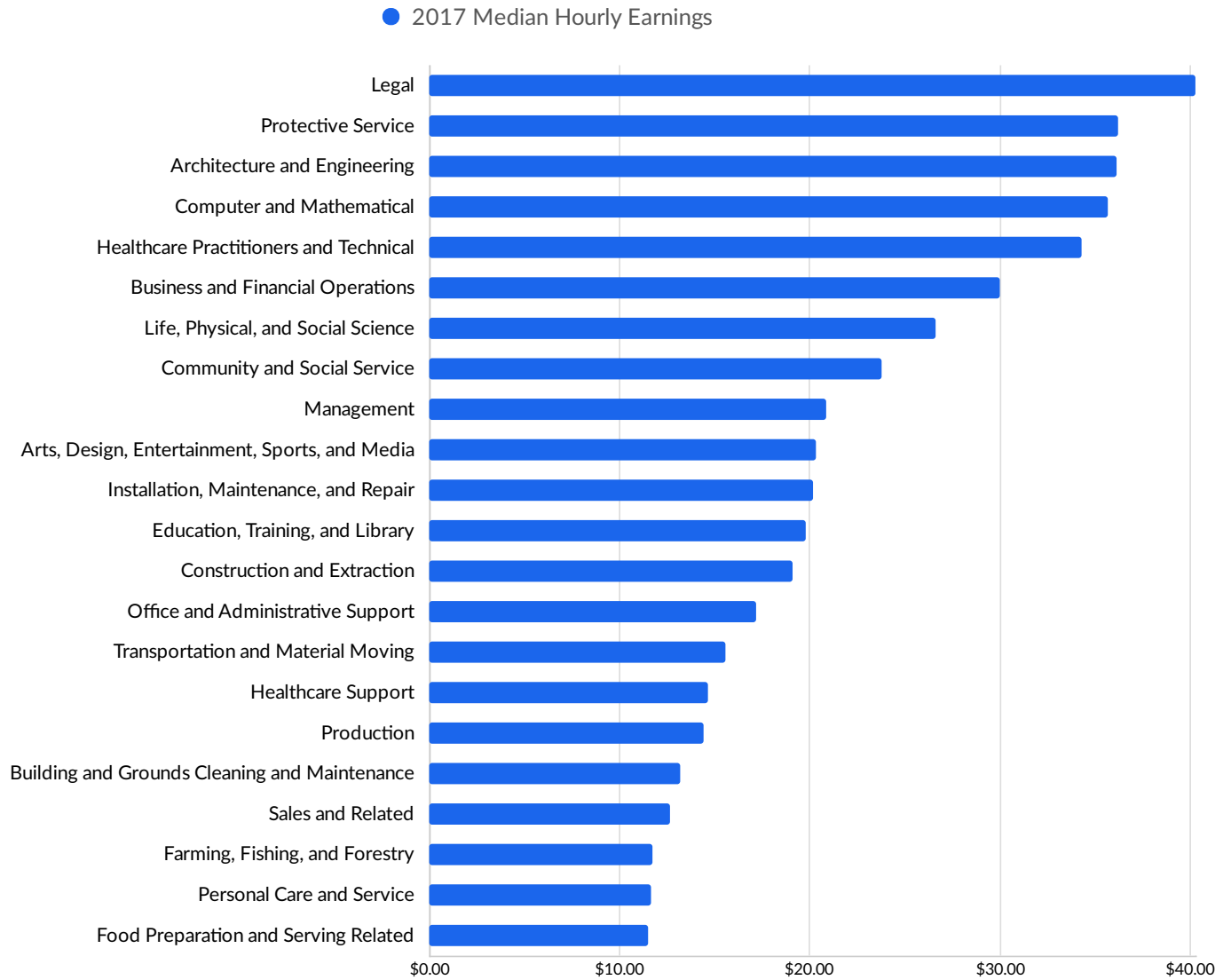
Workforce Characteristics - Cont.

Top Occupation LQ



Workforce Characteristics - Cont.

Top Occupation Earnings



Postings data is not available for the currently selected region.

Underemployment data is not available for the currently selected region.

Workforce Characteristics - Cont.

Occupation	2013 Jobs	2018 Jobs	Change in Jobs	% Change in Jobs	2018 LQ	2017 Median Hourly Earnings	May 2018 - Apr 2019 Unique Average Monthly Postings
Farming, Fishing, and Forestry	3,243	3,267	24	+1%	43.22	\$11.74	0
Management	894	998	104	+12%	1.75	\$20.90	0
Sales and Related	853	929	76	+9%	0.93	\$12.67	0
Transportation and Material Moving	899	928	29	+3%	1.34	\$15.58	0
Office and Administrative Support	877	895	18	+2%	0.61	\$17.22	0
Food Preparation and Serving Related	535	675	140	+26%	0.80	\$11.49	0
Production	656	628	-28	-4%	1.06	\$14.43	0
Construction and Extraction	238	307	69	+29%	0.66	\$19.11	0
Building and Grounds Cleaning and Maintenance	278	299	21	+8%	0.80	\$13.23	0
Installation, Maintenance, and Repair	243	260	17	+7%	0.66	\$20.20	0
Business and Financial Operations	188	207	19	+10%	0.40	\$30.05	0
Personal Care and Service	159	184	25	+16%	0.42	\$11.70	0
Healthcare Practitioners and Technical	99	112	13	+13%	0.20	\$34.30	0
Protective Service	101	105	4	+4%	0.46	\$36.28	0
Community and Social Service	56	66	10	+18%	0.39	\$23.83	0
Education, Training, and Library	48	59	11	+23%	0.10	\$19.85	0
Life, Physical, and Social Science	36	49	13	+36%	0.59	\$26.63	0

Workforce Characteristics - Cont.

Occupation	2013 Jobs	2018 Jobs	Change in Jobs	% Change in Jobs	2018 LQ	2017 Median Hourly Earnings	May 2018 - Apr 2019 Unique Average Monthly Postings
Architecture and Engineering	45	45	0	0%	0.26	\$36.20	0
Arts, Design, Entertainment, Sports, and Media	39	41	2	+5%	0.22	\$20.34	0
Computer and Mathematical	32	37	5	+16%	0.12	\$35.73	0
Healthcare Support	30	32	2	+7%	0.12	\$14.66	0
Legal	18	19	1	+6%	0.23	\$40.33	0

Educational Pipeline

In 2017, there were 126 graduates in Coachella Zip Code. This pipeline has grown by 50% over the last 5 years. The highest share of these graduates come from "Heating, Air Conditioning, Ventilation and Refrigeration Maintenance Technology/Technician", Building Construction Technology, and Business/Office Automation/Technology/Data Entry.

School	Total Graduates (2017)	Graduate Trend (2013 - 2017)
CET-Coachella	126 	

- Certificate
- Associate's
- Bachelor's
- Master's or Higher

ENGINEERING:

Capital Improvement Projects
1st Quarterly Report
October 23rd, 2019

ST-67 Ave 50/I-10 Interchange

The plans are 100% complete and submitted to Caltrans for final approval. Caltrans provided responses and MBI working on final comments. Plan Approval expected by end of 2019. Offer letters have been provided to property owners on the north and south side of Interstate 10. Appraiser working with owners to revise appraisals. Build Grant submittal in July, award announcements expected in November 2019. Project meetings continue with CalTrans Management to review the Interchange Grading and finalize plans for construction.



ST-69 Ave 50 Bridge over Whitewater Channel / ST-81 New Interchange @ Avenue 50 and 86 South Expressway

Environmental document were approved and finalized in February 2019. Staff is requesting final authorization for design phase of the project. Project meeting with Caltrans to review technical studies on the project continues. Request for Authorization has been submitted to Caltrans HBP.



ST-78 Avenue 48 Widening Project (Jackson—Van Buren)

Granite Installed pavement on Avenue 48 and completed concrete cross gutters. Project is 90% complete.



ST-84 Class 2 Bike Lanes along Harrison (Green Lanes)

Construction is complete. Notice of Completion was approved by council September 11, 2019.



ST-93 Avenue 50 Widening Project (Calhoun to Harrison)

City staff received environmental study and drainage study, the Planning Department will submit letters to the tribes notifying them in regards to the project. City Council approved a PSA with KOA Corporations to provide PS&E and Right of Way Services on October 9, 2019.



ST-98 Avenue 50 Extension PS&E (All American Canal to I-10 Interchange)

Engineering Staff continues to wait for responses to the offers of acquisitions; Right-of-way acquisitions under negotiations with five property owners located west of the All American Canal. City Council approved two purchase agreements for 2 of the 5 parcels. Bureau of Reclamation (BOR) has approved the environmental document for the work within the canal area.

ST-100 ATP Cycle 2

Project bid advertisement published for construction on Sept 12, 2019. Council approved Matich Corporation for contractor at October 9, 2019 Council Meeting. Contract is being routed for signatures.



ST-106 Coral Mountain School Street Signal

Project was completed in August 2019. Notice of Completion to be approved in October 2019.



ST-109 Dillon Road Bridge Interstate I-10 Interchange & SR 86 Interchange

JPA meeting on Sept 12, 2019. Project Design Team met on Sept. 12, 2019, at 1pm to review Traffic, Environmental, Structure Analysis and Stakeholder coordination of NEPA document lead.

ST-110 Sidewalk Araby

Construction is complete. Notice of Completion was approved by Council on September 11, 2019.



ST-115 SB1 Road Repair

City Staff is working on Specifications and Plans.

ST-116 Avenue 52 & Avenue 54 Road Reconstruction

County Board of Supervisors awarded project on May 21, 2019, County is lead on project. Construction started September 25th. Construction continues on Avenue 54 and Grapefruit Blvd.



ST-117 Sidewalk Installation on Araby Avenue Phase 2

City Staff is working on Grant for CDBG with Specifications and Plans. Plan is to start project fiscal year 2019/2020.

ST-119 La Ponderosa

City Staff is working on Specifications and Plans.

ST-120 Phase II Slurry REAS

City Staff is working on Specifications and Plans.

ST-122 Van Buren

Construction began September 23rd. Project is 60% complete.



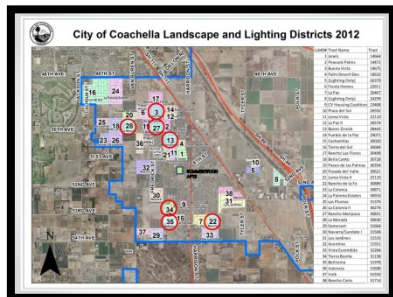
ST-123 Urban Greening & Connectivity

Project RFP published in August 2019. Proposals were received from six engineering firms and Staff conducted interviews with the top three firms. City Council approved a PSA with NV5 to provide Plans, Specifications and Estimate Services.



ST-124 Traffic Calming Phase III

Project bid advertisement published for construction on Sept 19, 2019. Council approved Onyx Paving Inc. for contractor at October 9, 2019 Council Meeting. Contract is being routed for signatures.



SD-02 Avenue 50 Storm Drain

Project bid advertisement published for construction on Sept 19, 2019. Council approved Granite Construction for contractor at October 9, 2019 Council Meeting. Contract is being routed for signatures.



Commercial and Residential Projects:

Senior Center Expansion:

Construction continues as contractor prepares grades and places widened sidewalk.



51711 Douma St. AT&T wireless tower at Bagdouma Park:

Contractor has completed a 19' footing to install rebar cage and place concrete at the monopole tower facility.



49751 Oates Ln. Luxtor RV Parking & Storage

Contractor continues to complete the storm drain and underground chamber installation and backfills to start preparing for the parking lot.



Red Moon Development at the Villas at the Vineyards:

Contractor continues to mass grade and build perimeter block wall on Vista Del Norte.



48975 Grapefruit Blvd.: Plaza mi Pueblo. 90% Civil completion achieved.

Owner has completed placing the exterior colored walk. Activity or inspections have not been reported.



Tower Market Indio:

Contractor was granted a Coachella permit to complete a small portion of improvements on Avenue 48 fronting the Tower market project.



Palm View Elementary School Rehabilitation:

Demolition of school site continues.



Our Lady of Soledad Church: Projected completion October 2019.

Contractor continues to complete the parking lot signing, striping and paving improvements. Both main driveway ingress/egress points are complete.



Prado:

21 houses have been sold out of the 63 total. Construction continues as planned.



31508 Navarra 7 remaining Homes:

Contractor continues to frame the 7 remaining houses as per the structural plans.



Vista Escondida D.R. Horton

3rd and final grading and model home civil plan check in progress.



Frontier Emergency Repairs: Van Buren north of Ave. 51

Frontier will provide a schedule to permanent patch this repair window as of 9-24-19.



Kismet:

The Ave. 48th offsite water main line installation including a new fire hydrant and two hot tap connections permit has been issued and plans approved. Offsite Sewer plans approved as of 9-12-19.

84650 Avenue 49 Mosque

Engineering held a site preconstruction meeting. Contractor has not submitted the requirements to engineering in order to start. Project continues on Hold by the Owner.

The Villas at the Vineyards lots 71 & 72

Offsite and onsite street plans have been submitted and plan check in progress. Project remains on hold by Owner.

Glenroy Project:

Project has not requested Engineering inspections since April 2019. Owner has not contacted engineering with any requests.

Desert Rock Dev. Phase 1 48700 Harrison St.

Project remains on hold by Owner. Owner has not contacted engineering with any requests.

83641 Ave. 51 Juanita Hernandez

Project remains on hold by Owner. Owner has not contacted engineering with any requests.

84851 Avenue 48 Del Grow

Project has been granted various certificates of occupancies on multiple suites. Master project construction continues.

Engineering Permits	Development Service Reviews	Building Permit Reviews
30	12	26



RIVERSIDE COUNTY SHERIFF'S DEPARTMENT
STANLEY SNIFF, SHERIFF / CORONER
Thermal Station

TO: Sgt. Davis

DATE: October 17, 2019

FROM: Deputy James #4399

SUBJECT: CVVCGTF Monthly Activity Report – July, August, September 2019

The Coachella Valley Violent Crime Gang Task Force (CVVCGTF) is a multi-agency task force comprised of members from the Coachella Police Department, La Quinta Police Department, Palm Desert Police Department, Desert Hot Springs Police Department, Cathedral City Police Department, California Highway Patrol, Riverside County Probation and Riverside County Sheriff's Department.

The Mission of the CVVCGTF is to allocate resources from all of the Coachella Valley cities and unincorporated communities, the District Attorney, Riverside County Probation and other allied agencies for promoting safe, secure neighborhoods free of violent crime and gang violence. Their responsibilities include, but are not limited to the following:

1. To prevent and deter violent crime and gang activity through education and the promotion of positive commitments, beliefs, activities, and attachments.
2. To intervene and hold accountable, youths exhibiting delinquent behaviors leading to the formation of gangs and gang membership.
3. To suppress and deter gang activity and violent crime through enforcement, intensive supervision, vertical prosecution, and the collection and dissemination of criminal intelligence.
4. Work a flex schedule that allows for adjustment of schedules based on significant gang related crimes and/or high profile events.

COACHELLA VALLEY VIOLENT CRIME GANG TASK FORCE STATISTICS

Current

Felony Arrests:	6
Misdemeanor Arrests:	6
F.I. Gang Cards:	5
F.I. Cards:	0

Gang Intervention Contact:	0
Criminal Cases Filed:	3
Gang Enhancements:	1
Probation Searches:	15
Probation Violations:	10
Parole Searches:	6
Parole Violations:	1
Firearms Recovered:	1
Search Warrants Written:	1
Search Warrants Served:	4
Pedestrian Checks:	10
Narcotics Seized (Grams):	0
Asset Forfeiture:	0
Warrant Arrests (Felony):	2
Warrant Arrests (Misdemeanor):	1
Overtime Hours:	144
Gang Presentations:	0
Training Classes (Presented):	0
Training Hours (Presented):	0
Training Classes (Attended):	1
Training Hours (Attended):	32
Court Testimony:	0
Assist Other Department:	4
Follow up	2
Youth Outreach	0

Summary of Activities:

Incident: Search Warrant

Case #: GE192140002

Locations: 52408 Calle Camacho, Coachella

Date/Time: 08/02/2019 – 0800hrs

Total Team hours: 10

CVVCGTF members served a search warrant at the above listed addresses. The warrants stemmed from information that controlled substances are being sold from the location. Warrant was served and three suspects were arrested. Large amounts of marijuana for sale, THC cubes, and currency from the proceeds from sales were taken as evidence.

Incident: 11377 HS AWS Arrest

Case #: GE192140001

Locations: Calle Camacho x Avenue 53, Coachella

Date/Time: 08/02/2019 – 0700 hrs

Total Team hours: 1

TFO James and Heidt conducted a bicycle stop at the above-mentioned intersection and arrested Rene Garcia for an outstanding arrest warrant.

Incident: 11377 HS and 3056 PC Arrest
Case #: GE192190001 and 0003
Locations: 31525 Avenida Juarez, Cathedral City
Date/Time: 08/07/2019 – 1200 hrs
Total Team hours: 2

CVVCGTF members responded to the above listed address to conduct a parole compliance check of Barrio Cathedral City gang member Raul “Banana” Gutierrez. During the search methamphetamine was located inside his dresser drawer. Gutierrez was placed under arrested and booked at the Indio Jail.

Incident: 470(a) PC AWS Arrest and 273.6 PC Arrest
Case #: GE192210001 and 0002
Locations: 72865 La Canada, Thousand Palms
Date/Time: 08/09/2019 – 1031 hrs
Total Team hours: 4

CVVCGTF members responded to the above listed address to conduct a probation compliance check of Nicole Pech. During the search, Lisa Pomel was found at the location and had an outstanding arrest warrant and was placed under arrest. Additionally, documented North Side Indio gang member Francisco Leyva was found to be in violation of a Domestic Violence Court Order with Pech as the protected person. Leyva was also placed under arrest. Both Pomel and Leyva were booked at the Indio Jail.

Incident: Search Warrant
Case #: GE192240001
Locations: 65111 Van Buren St, Thermal
Date/Time: 08/12/2019 – 0500hrs
Total Team hours: 10

CVVCGTF members served a search warrant at the above listed addresses. The warrants stemmed from information that suspect who is a convicted felon, has more weapons and ammunitions on his house. Warrant was served and Ezequiel Ortega Nava was arrested for possession of ammunition and possession of controlled substances for sale.

Incident: 11364 HS
Case #: GE192320001
Locations: 82346 Oleander, Indio
Date/Time: 08/20/2019 – 1537 hrs
Total Team hours: 2

CVVCGTF members responded to the above listed address to conduct a parole compliance check of North Side Indio gang member “Hikey” Carmona. While at the residence Ray Leon was

contacted and found to be in possession of a glass pipe and placed under arrest for 11364 HS.

Incident: 3056 PC Arrest
Case #: GE192350006
Locations: 53747 Sapphire Lane, Coachella
Date/Time: 08/23/2019 – 1046 hrs
Total Team hours: 3

CVVCGTF members responded to the above listed address to conduct a parole compliance check on 290 PC Registrant Enrique Hernandez. Hernandez was found to be in violation of his parole terms and placed under arrest and booked at the Indio Jail.

Incident: 3056 PC and 11364 HS Arrest
Case #: GE192420002 and 0003
Locations: 30650 Avenida Alvera, Cathedral City
Date/Time: 08/30/2019 – 1200 hrs
Total Team hours: 3

CVVCGTF members responded to the above listed address to conduct a parole compliance check of Barrio Dream Homes gang member Nathan “Pacman” Tapia. During the search drug paraphernalia was located in his bedroom. Tapia was placed under arrest and booked at the Indio Jail.

Incident: 3056 PC Arrest
Case #: GE19 260 0001
Locations: 66636 Estrella Ave, DHS
Date/Time: 09/17/19 – 1111 Hrs
Total Team hours: 2

TFO’s James, Kirk and Davidson responded to the above listed address to conduct a probation compliance check of Marc Daltry. Upon arriving TFO’s contacted and arrested Travis Vargas for an outstanding parole violation warrant. Vargas was booked into the Banning Jail.

Incident: AWS Arrest and AOD to Cathedral City Police
Case #: GE19 260 0005
Locations: 68120 Calle Las Tiendas #A, DHS
Date/Time: 09/17/19 – 1449 Hrs
Total Team hours: 2

TFO’s James and Kirk responded to the above listed address to assist Cathedral City Police with the apprehension of Isaac Randolph, a witness and victim to a prior assault. Randolph was placed under arrest and booked at the Indio Jail for an outstanding arrest warrant.

Incident: 30305 PC AWS Arrest
Case #: GE19 262 0005
Locations: 53011 Calle Avila, Coachella
Date/Time: 09/19/19 – 1453 Hrs

Total Team hours: 2

TFO's responded to the above listed address and arrested documented VCR criminal street gang member Augustine Lopez, for an outstanding 30305 PC arrest warrant. Lopez was booked into the Indio Jail.



Public Works Department 2019 Third Quarter Report

TO: Honorable Mayor and Council

FROM: Maritza Martinez, Public Works Director

DATE: October 17, 2019

SUBJECT: Quarterly Report

Departmental Mission:

In a balanced effort to maintain cost effective operations and provide maintenance and emergency response services in a reasonable and efficient manner, it is the mission of the Public Works Department to provide quality maintenance and repair of facilities and infrastructure for the City of Coachella community and its city departments.

Executive Summary:

The Public Works Department is responsible for the operations of our: facilities, fleet, landscape, lighting and maintenance districts, parks, and streets. The Department is also the home to our Emergency Services Division and Recreation Division. As reflected below the Public Works Department staff responded to a myriad of requests as reflected below between July 2019 and September 2019.

	Acres	Feet/Sq Feet	Miles/Sq Miles	Quantity
<u>Streets Division</u>				
Illegal Dumping				8 removals 14 homeless camps removed
Weed/brush Abatement				22 jobs
Annual Disaster Council and Quarterly OAPC Meetings				19 meetings
Potholes/Street Repairs		27 tons of hot mix		2500 Potholes
Sidewalk Repairs				12

Special Events				2 events
Street Sign Main.				Replaced 52; Repaired 25
Street Light Repair				58 repaired
Street Striping				10 parking 129 legends
Storm Drain Main				2 storm drains
Traffic Control				21 assignments
	Acres	Feet/Sq Feet	Miles/Sq Miles	Quantity
<u>Parks/LLMD</u>				
Citations Issued				1,437
Facility Use Permits				402 permits
Overseeding /Fertilization	30 acres			
Park Lighting				59 Fixture Repairs
Special Events				2 events
Sport Field Preparation	22 acres			
Sports Field and Walking Tract				25 Tons of DG for walking tracks
Sprinkler/Irrigation Repairs				121 Heads; 17 valves; 2 Main Lines; 55 drip line;
Tree/Plant Replacement				56 plants
<u>Building Maintenance</u>				
HVAC Systems Serviced / Repaired				3 repairs
<u>Building Repairs</u>				
Meeting Room Setups				62
Floor Maintenance	7,500 sq ft			



Utilities Department Memorandum

TO: William B. Pattison, Jr., City Manager

FROM: Cástulo R. Estrada, Utilities Manager

DATE: October 16, 2019

SUBJECT: Quarterly Activity Report (July 2019 to September 2019)

The City of Coachella Utilities Department is submitting the Quarterly Activity report for July through September 2019.

The following report summarizes activities and significant achievements for the Coachella Sanitary District and Coachella Water Authority divisions of the Utilities department.

Coachella Sanitary District:

The Utilities Department, Sanitary Division, provides for all the collection, treatment, and disposal of residential, commercial, and industrial wastewater for the more than 8,000 service connections within the City and more than 400,000 lineal feet of sewer collection system, four (4) sewage pump stations, and over 1,580 manholes.

For the Third Quarter of July, August, & September 2019, almost 241,178 MG of wastewater was received at the wastewater plant or average daily flow of 2.622 MGD. The plant treated a total of 180.694 MG. A total of 220 samples were collected at the plant for monitoring.

Water Reclamation Facility – 87-075 Enterprise Way	
<u>Description</u>	<u>Value</u>
Total Influent per month (MG)	80.392
Influent Ave. Daily Flow (MGD)	2.622
Total Effluent per month (MG)	60.231
Effluent Ave. Daily Flow (MGD)	1.974
Plant Permitted Capacity (MGD)	4.5
Operational Upsets	None
Permit Violations	None
Hypochlorite consumption (Gallon)	15,000
Sodium Bisulfite consumption (Gallon)	5,000
Odor complaints	1
Samples collected this quarter (Plant)-	220
State Report completed/submitted	Yes

Collection System & Pump Stations Highlights	
<u>Description</u>	<u>Value</u>
Underground Service Alert	56
Collection System Cleaning (feet)	23,293
Manhole Inspection/Cleaning	31
Collection System Spills	Page 98
Collection System Blockages	
Stand-By Calls	5

Collection System CCTV Inspection (feet)	200
Samples Collected (Field)	None
Pump Station Alarms	1
Wet Well Cleaning	None
Wet Well Inspections	45

Coachella Water Authority:

The Utilities Department Water Authority Division provides water pumping, treatment, and distribution services to more than 8,058 service connections citywide that include residential, commercial, and industrial users. The City has three (3) Storage Reservoirs of more than 10 million gallons of capacity with six (6) operating wells that provides more than 8,000 acre-feet of annual water production. This division is responsible for a variety of tasks including meter reading, repairs, pumping, production, treatment, main and distribution repairs, water quality complaints, service connections/disconnections, and capital improvements along with other assigned duties.

Coachella Water Authority Production and Distribution Highlights	
<i>Description</i>	<i>Value</i>
Total -Water Production (July to Sept.) (Million Gallons)	700.7
Chlorine Consumption (Gallons)	2100
Water Quality complaints	1
Monthly Samples Collected	180
Valves Exercised	46
Valves Repaired/Replaced	2
Underground Service Alerts	11
Hydrants Flushed/Serviced	6
Standby Service Calls	31
Water Service Leaks	26
Main Leaks	1
Customer Service Calls	
Office work orders	358
Non-Reads	2298
Meter Replacements	42
Meter Repairs	10
Turn-Off	N/A
Turn-On	N/A
Delinquency Turn Off	483
State DDW Monthly Report Submitted	Yes
Well Depths Monitored	
Well No. 10 Depth (feet,)	-
Well No. 11 Depth (feet,)	-
Well No. 12 Depth (feet,)	-
Well No. 16 Depth (feet,)	-
Well No. 17 Depth (feet,)	-
Well No. 18 Depth (feet, bgs)	-
Well No. 19 Depth (feet, bgs)	-
Annual Production	
2017 Annual (MG)	2221.26
2018 Annual (MG)	2327.23

Environmental/Regulatory Division:

The Environmental/Regulatory Division of the Utilities Department is responsible for areas relating directly to water, wastewater, stormwater and air quality protection. Initial focus is aimed at the National Pollutant Discharge Elimination System (NPDES) Industrial Wastewater Pretreatment program – known as Source Control. This program provides for the auditing and or permitting, monitoring, and enforcement of our local City Ordinance. The following quarterly report will detail activities from the previous quarter. The following list of activities and or tasks assigned:

Coachella Water Tasks

- Turf program- Closed the Prop 84 Round 3 grant program. CWA submitted a Prop 1 grant in the CVRWGMG application - program will be relaunched upon award of grant or other funding outlets.
- Continued review of CV Water Counts 2019 campaign; online and print ads for both English and Spanish language outlets;
- Assisted Senior Water Worker with submittal of 3rd Quarter 2019 DBPR; monthly total coliform and TCR reports;
- Attended the SWRCB's Webinar on Urban Water Supplier Monthly Reports – reports will be streamlined
- Distributed at city offices the 2018 Consumer Confidence Report/Water Quality Report
- Coordinated transition of analytical services to BSK Laboratory;
- Attended the 4th of July event Bagdouma Park – featured water tank/water bottles
- Coordinated setup for El Grito outreach booth – staff provided reusable mood cups, flyer for payment assistance program and promotional materials for TT&C event;

Item6.

Coachella Sanitary Tasks

- Closed out the Prop 84 Round 2 grant Recycled Water Feasibility Study project – awaiting release of retention;
- Continued scheduling for facilities of concern based on MS4 and Source Control program using Remote Inspector software;
- Review and comments for various RAC from Planning Dept.;
- Submitted the Annual Industrial General Permit report for Sanitary Plant;
- Submitted approval letters for new restaurants starting operations in the service area; waivers were approved for facilities that met the criteria.
- Attended webinar training for Municode;- transition from Sire to new agenda management system

Public Works Tasks

- Attended monthly Desert Task MS4 NPDES meetings in Palm Desert – discussions included requirement to notify businesses to enroll in IGP program and verification of SWPPP
- Packaged and shipped battery collection from residential program;
- Submitted the Technical Report for the Statewide Trash TMDL for the City's MS4 program;
- Conducting stormwater inspection at various locations throughout the City;
- Reviewed/commented various WQMP for construction projects
- Conducted training to staff for municipal facilities pollution prevention plan for MS4 permit requirements – conducted onsite inspection at the Corporate Yard and provided corrections needed for compliance with NPDES permit.

Intern Program:

The Utilities Department Intern Program was established on December 2012. The program assigns responsibility to provide expertise and guidance to the incumbent and technical assistance to the various divisions within the Department and the public. Incumbent is assigned tasks accordingly to promote career growth and is under the general supervision of the Utilities General Manager.

- Program is currently suspended



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Jonathan Hoy, P.E., Assistant City Manager/City Engineer

SUBJECT: Notice of Completion for City Project Number ST-106, Coral Mountain Academy Traffic Signal Project

STAFF RECOMMENDATION:

Accept the ST-106, Coral Mountain Academy Traffic Signal Project as complete and direct the City Clerk to file a Notice of Completion attached hereto with the County of Riverside.

BACKGROUND:

In December 2016, the City received a petition which included over 800 parent signatures requesting a traffic signal at the school entrance to Van Buren Street. The Engineering department sent the petition to the City's traffic Engineer and requested that a traffic analysis be completed to determine whether traffic signal warrants were met.

In February 2017, Traffic Safety Engineers completed their analysis and determined that the City may consider the installation of a traffic signal at this driveway intersection to mitigate this undesired traffic circulation dilemma. This option is permitted by Section 4c.01 on page 829 of the MUTCD, California Supplement.

On January 23, 2019 the City Council in accordance with City standard policy awarded a contract to DBX, Inc. for the construction of a traffic signal on Van Buren Street at the intersection of Coral Mountain Academy.

City Staff has inspected the Project and found the improvements to be in accordance with the plans, specifications and City Standards. Staff recommends the acceptance of this project and that City Council authorize the filing of a Notice of Completion. Upon acceptance by City Council, the Project will enter a one year warranty period as prescribed by the project contract agreement.

FISCAL IMPACT:

No fiscal impact to the General Fund. The original contract amount for the project was \$255,363.00 plus contingency equaling \$280,900.00 and the final contract amount is \$243,259.00. The project was constructed within contract budget.

<p>To be recorded with County Recorder within 10 days after completion and Acceptance. No recording fee.</p> <p>When Recorded, return to:</p> <p>Angela Zepeda, City Clerk City of Coachella 1515 Sixth Street Coachella, CA 92236</p>	<div style="text-align: right; border: 1px solid black; padding: 2px;">Item 7.</div> <div style="text-align: right; margin-top: 100px;">(For Recorders Use)</div>
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Notice of Completion

(California Civil Code Section 3093 - Public Works)

Notice is hereby given by the undersigned owner, a political subdivision of the State of California that a public work improvement described as Coral Mountain Academy Traffic Signal, Project ST-106 has been completed and was accepted by the undersigned awarding authority on the date hereof. Sidewalk improvements are located within City right-of-way on Van Buren Street at the intersection of Coral Mountain Academy.

The contractor on such work was DBX, Inc. and the surety on his bond is GMGS Risk Management & Insurance Services, 6201 Oak Canyon, Suite 100 Irvine, CA 92618.

The real property upon which said work was performed is in the City of Coachella, County of Riverside, and State of California.

The nature of the interest of the owner is in fee.

Date: August 8, 2019
(Date of Acceptance)

City of Coachella
(Name of Political Subdivision)

By: _____
Steven A. Hernandez

Title: Mayor

State of California)
) ss

County of Riverside)

I hereby certify that I am the City Clerk of the governing board of the City of Coachella, the political subdivision which executed the foregoing notice and on whose behalf I make this verification; that I have read said notice, know its contents, and that the same is true. I certify under penalty of perjury that the foregoing is true and correct.

Executed at Coachella, California on _____ (Date)
(City Where Signed)

Angela Zepeda, City Clerk, City of Coachella

County Counsel Form 1 (Rev. 5-64)



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Maritza Martinez, Public Works Department

SUBJECT: Authorize execution of Amendment No. 1 to West Coast Arborist Maintenance Agreement extending their term for one year.

STAFF RECOMMENDATION:

Authorize execution of Amendment No. 1 to the West Coast Arborist Maintenance Agreement extending their term for one year.

EXECUTIVE SUMMARY:

The City published a Request for Proposals (RFP) for the Tree Trimming Maintenance Project for city parks, street medians and street parkways on June 8, 2017. The RFP would provide services for areas not within the city's landscape lighting maintenance district areas. This RFP closed on July 12, 2017 and the City received two responses. The Tree Trimming Maintenance Project RFP identified that the term of the project would be for two years.

Council awarded the correspondent maintenance agreement to West Coast Arborist on July 26, 2017. The RFP allows for the agreement term to be extended for one additional year. Staff is recommending execution of the attached Amendment No. 1 to the awarded maintenance agreement with West Coast Arborist, extending the term through July 14, 2020, and augmenting the compensation to allow for services to be continued through the extended term. The augmented compensation will reflect an additional \$40,000.00 above the originally awarded amount bringing the new not to exceed amount to \$115,000.00.

FISCAL IMPACT:

The recommended action will not have a negative impact on the budget. The award amount is allocated in the adopted Fiscal Year 2019/2020 Street and Parks Divisional Budgets.

Attachment:

Proposed Agreement

**Amendment No. 1 to the Maintenance Services Agreement
Between the City of Coachella
And
West Coast Arborist Inc.
Project No 071217**

1. Parties And Date.

This Amendment No. 1 to the Maintenance Services Agreement ("Amendment No.1") is made and entered into this 23rd day of October, 2019 by and between the City of Coachella ("City") and West Coast Arborist Inc., ("Contractor"). City and Contractor are sometimes, individually referred to as "Party" and collectively as "Parties" in this Amendment No.1.

2. Recitals.

2.1 Agreement. City and Contractor entered into that certain Agreement for City Parks and Streets Tree Trimming project ("Agreement"), entered into on July 26, 2017.

2.2 Amendment. City and Contractor desire to amend the Agreement for the first time to amend the term and compensation of the Agreement as set forth in this Amendment No. 1.

3. Amendments.

3.1 Term of Agreement. Section 3.1.2 of the Agreement is hereby deleted in its entirety and replaced with the following:

3.1.3 Term. The term of this Agreement shall be from July 15, 2017 – July 14, 2020, unless earlier terminated as provided herein. Contractor shall complete the Services within the term of this Agreement, and shall meet any other established schedules and deadlines.

3.2 Compensation. Section 3.3.1 of the Agreement is hereby deleted in its entirety and replaced with the following:

3.3.1. Compensation. Contractor shall receive compensation, including authorized reimbursements, for all Services rendered under this Agreement at the rates set forth in Exhibit "C" attached hereto and incorporated herein by reference. The total compensation shall not exceed **one hundred fifteen dollars (\$115,000)** without written approval of City's City Council. Extra Work may be authorized, as described below, and if authorized, will be compensated at the rates and manner set forth in this Agreement.

3.3 Continuing Effect of Agreement. Except as amended by this Amendment No.1, all provisions of the Agreement shall remain unchanged and in full force and effect From and after the date of this Amendment No.1, whenever the term "Agreement" appears in the Agreement, it shall mean the Agreement as amended by this Amendment No.1.

3.4 Adequate Consideration. The Parties hereto irrevocably stipulate and agree that they have each received adequate and independent consideration for the performance of the obligations they have undertaken pursuant to this Amendment No.1.

3.5 Counterparts. This Amendment No.1 may be executed in duplicate originals, each of which is deemed to be an original, but when taken together shall constitute but one and the same instrument.

IN WITNESS WHEREOF, the parties have executed this Amendment, No.1 as of the date first written above.

THE CITY OF COACHELLA

WEST COAST ARBORIST INC.

By: _____
William B. Pattison, Jr., City Manager

By: _____

Attest:

City Clerk

Approved as to Form:

Carlos Campos, City Attorney



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Jacob I. Alvarez, Assistant to the City Manager

SUBJECT: Fee Waiver Request for Veterans Park Stage and Special Event Fee for the Confronting Climate Change Forum

STAFF RECOMMENDATION:

That the City Council approve to waive the \$187.00 Flat Fee and the corresponding \$25 Special Event fee for the use of the Veterans Park Stage for the Confronting Climate Change Forum that will take place on November 9, 2019.

BACKGROUND:

As you may recall, City Council on October 9, 2019 during the Public Comment portion of the agenda heard from Robert Mueller regarding a Free Confronting Climate Change Forum that is proposed to be held on Saturday, November 9th from 1:00pm-3:00pm at the City of Coachella's Veterans Park.

The event organizer would like to use the stage and grass area of the park for the event and has requested that the Special Event and Veterans Park Stage fees be waived. This would allow the City become a co-host of the event.

FISCAL IMPACT:

There will be no fiscal impact on the General Fund. However, the City will forgo the collection of \$212.00 in fee revenues.



**STAFF REPORT
10/23/2019**

TO: Honorable Mayor and City Council Members
FROM: Maritza Martinez, Public Works Director
SUBJECT: Authorize Beer Garden to be operated at Dateland Park for the Synergy Festival, on November 9, 2019 from 11am to 8pm.

STAFF RECOMMENDATION:

Authorize beer garden to be operated at Dateland Park for Synergy Festival, on November 9, 2019 from 11am to 8pm.

EXECUTIVE SUMMARY:

Staff is requesting Council authorize operation of a beer garden at the 2019 Synergy Festival. This event has been a city sponsored event for eight years. Event will occur on Saturday, November 9, 2019 at Dateland Park. The proposed beer garden will operate from 11am-8pm on November 9th. The event organizer is required to pull the appropriate County Health Permit and ABC License for all beer garden operations.

FISCAL IMPACT:

Approval of the recommended action would not negatively impact the approved FY 19/20 budget.





STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members
FROM: William Pattison, City Manager
SUBJECT: Investment Report – August 2019

STAFF RECOMMENDATION:

Staff recommends that the City Council receive and file the investment report for August of 2019

EXECUTIVE SUMMARY:

On April 10, 2019, the City of Coachella along with its component units (Sanitary District, Educational & Governmental Access Cable Corporation, Fire Protection District and Water Authority) approved and adopted the current “Statement of Investment Policy”.

Pursuant to Section 16 of that policy, the City Treasurer shall provide to the City Council a monthly investment report which provides a clear picture of the status of the current investment portfolio. This report shall include, at a minimum, the following information for each type of investment held in the City’s investment portfolio: the issuer; amount of investment; current market value; yield on investment; income generated from investments; dollar amount invested on all securities, investments and moneys held by the local agency; and shall additionally include a description of any of the local agency’s funds, investments, or programs; and a description of unusual investment activity or developments during the month for which the report is prepared. This information shall be provided for all City and component unit pooled investments, as well as for bond accounts, which are managed by outside Fiscal Agents.

The interest rates presented are the most current rates available as of the date of these reports. The market values presented for pooled City investments are based on closing prices for the related investments as of the date of these reports. This information was obtained from the Wall Street Journal or other reliable sources of market prices.

The Market values presented for investments managed by contracted parties are based on amounts reported by the Fiscal Agent on the most recent bank statement to be market value as of the date of said bank statement. The purchase date and type of investment is not included for funds held by the fiscal agent.

Attached is the Treasurer's Report of Investments which includes an overview on investments which provides information on investment activity, withdrawals and deposits, interest earned, payment of interest and payment of principal as of the periods ending August 31, 2019 . In addition, this report includes detailed information and current activity on individual investments.

All City investments are in compliance with the guidelines established for Authorized Investments as specified in the Investment Policy, Section 8.

There was no unusual investment activity to report.

The City and Districts have sufficient moneys to meet their expenditure requirements for the next six months.

FISCAL IMPACT:

None, this report is receive and file only.

CITY OF COACHELLA
TREASURER'S REPORT - INVESTMENT REPORT
 As of August 31, 2019
 Fiscal Year 2019-2020

Item 11.

DESCRIPTION	CURRENT YIELD	BALANCE AS OF 7/31/2019	NET: DEPOSITS AND/OR (WITHDRAWALS)	INTEREST EARNED / CHANGE IN VALUE	PAYMENT OF INTEREST	PAYMENT OF PRINCIPAL	BALANCE AS OF 8/31/2019	
CASH ON HAND								
Wells Fargo-General Checking	N/A	9,107,552.32	(3,582,004.45)	-	-	-	5,525,547.87	①
Wells Fargo-Road Maintenance SB1	N/A	755,766.09	70,899.56	-	-	-	826,665.65	②
Rabobank-Payroll Account	0.0%	8,372.45	65.25	2.02	-	-	8,439.72	③
Rabobank-AG Summit Account	0.0%	13,562.96	-	-	-	-	13,562.96	④
Rabobank-Special Gas Tax Account	0.0%	152,881.41	85,933.50	-	-	-	238,814.91	⑤
Petty Cash	N/A	3,075.00	-	-	-	-	3,075.00	
Total Cash on Hand		10,041,210.23	(3,425,106.14)	2.02	-	-	6,616,106.11	
CASH WITH FISCAL AGENT								
Union Bank of California	varies	1,551,785.92	1,371,691.21	1,882.13	(157,287.50)	(445,000.00)	2,323,071.76	⑥
Wells Fargo Bank, N.A.	4.6%	462,241.69	-	21.71	-	-	462,263.40	⑥
County of Riverside	N/A	161,736.86	-	161.95	-	-	161,898.81	⑦
Total Cash with Fiscal Agent		2,175,764.47	1,371,691.21	2,065.79	(157,287.50)	(445,000.00)	2,947,233.97	
INVESTMENTS								
State of California - LAIF	0.2%	4,671,234.04	-	-	-	-	4,671,234.04	⑧
Investment Management Acct	3.3%	19,372,633.24	304,541.01	-	-	-	19,677,174.25	⑨
Saving Account - Wells Fargo Bank		5,080.09	-	-	-	-	5,080.09	⑩
Total Investments		24,048,947.37	304,541.01	-	-	-	24,353,488.38	
Grand Total		36,265,922.07	(1,748,873.92)	2,067.81	(157,287.50)	(445,000.00)	33,916,828.46	⑫

Prepared by: **Ruben Ramirez-Accounting Manager**

Reviewed by: **Javier Estrada - Controller**

Date: **8/31/2019**

CITY OF COACHELLA
TREASURER'S REPORT - INVESTMENT REPORT
 As of August 31, 2019
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INVESTMENTS							
STATE OF CALIFORNIA LOCAL AGENCY INVESTMENT FUND (LAIF)							
REDEVELOPMENT AGENCY(#004)	0.3%	2,433.89	-	-	-	-	2,433.89 ⑧
CITY GENERAL ACCOUNT(#171)	0.3%	2,819,847.71	-	-	-	-	2,819,847.71 ⑧
COACHELLA SANITARY DISTRICT	0.3%	1,848,830.78	-	-	-	-	1,848,830.78 ⑧
REDEVELOPMENT BONDS	0.3%	121.66	-	-	-	-	121.66 ⑧
TOTAL LAIF ACCOUNTS		4,671,234.04	-	-	-	-	4,671,234.04
INVESTMENT MANAGEMENT ACC							
MORGAN STANLEY	3.5%	-	-	-	-	-	-
PFM Funds	3.5%	19,372,633.24	304,541.01	-	-	-	19,677,174.25 ⑨
TOTAL INVESTMENT MANAGEMENT ACCT		19,372,633.24	304,541.01	-	-	-	19,677,174.25
SAVINGS & MONEY MARKET ACCOUNTS							
POLICE EVIDENCE ACCT - B of A #04	0.1%	5,080.09	-	-	-	-	5,080.09 ⑩
TOTAL SAVINGS ACCOUNT		5,080.09	-	-	-	-	5,080.09
TOTAL INVESTMENTS		24,048,947.37	304,541.01	-	-	-	24,353,488.38

CITY OF COACHELLA
TREASURER'S REPORT - INVESTMENT REPORT
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UNION BANK OF CALIFORNIA (Cost Basis reported)

COACHELLA WATER AUTHORITY

CITY OF COACHELLA WATER: WATER REFUNDING BONDS 2012 SERIES

A/C #: 6712016201 Bond Fund	0.0%	602,288.50	(602,287.50)	254.68	-	-	255.68
A/C #: 6712016202 Interest Account	0.0%	-	157,287.50	-	(157,287.50)	-	-
A/C #: 6712016203 Principal Account	0.0%	-	445,000.00	-	-	(445,000.00)	-
A/C #: 6712016204 Reserve Fund	0.0%	1.00	-	-	-	-	1.00
A/C #: 6712016206 Escrow Fund 2003	0.0%	-	-	-	-	-	-

COACHELLA FINANCING AUTHORITY

COACHELLA SANITARY DISTRICT: WASTEWATER REV 2005-A COPS

A/C #: 6711755701 Bond Fund	0.0%	-	-	-	-	-	-
A/C #: 6711755702 Interest Account	0.0%	-	-	-	-	-	-
A/C #: 6711755703 Principal Account	0.0%	-	-	-	-	-	-
A/C #: 6711755704 Reserve Account	2.5%	-	-	-	-	-	-

RDA PA #4 2004 B SERIES: TAX ALLOCATION BONDS

A/C #: 6711745801 Revenue Fund	0.0%	-	-	-	-	-	-
A/C #: 6711745802 Interest Account	0.0%	-	-	-	-	-	-
A/C #: 6711745803 Principal Account	0.0%	-	-	-	-	-	-
A/C #: 6711745804 Reserve Fund	2.5%	-	-	-	-	-	-

Successor Agency to the Coachella Redevelopments Agency 2014 Series

A/C #: 6712104701	0.0%	96.77	(96.08)	0.31	-	-	1.00
A/C #: 6712104702	0.0%	-	165,906.25	-	-	-	165,906.25
A/C #: 6712104703	0.0%	-	235,000.00	-	-	-	235,000.00
A/C #: 6712104704	0.0%	1.00	-	-	-	-	1.00
A/C #: 6712104705	0.0%	-	-	-	-	-	-
A/C #: 6712104706	0.0%	-	-	-	-	-	-
A/C #: 6712104707 Escrow Funds	0.0%	-	-	-	-	-	-
A/C #: 6712104708 Escrow Funds	0.0%	-	-	-	-	-	-

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COACHELLA SANITARY DISTRICT							
<u>WASTEWATER SERIES 2015A</u>							
A/C #: 6712148600	0.0%	-	-	-	-	-	-
A/C #: 6712148601	0.0%	108.71	-	0.31	-	-	109.02
A/C #: 6712148602	0.0%	-	-	-	-	-	-
A/C #: 6712148603	0.0%	-	-	-	-	-	-
A/C #: 6712148604	0.0%	1.00	-	-	-	-	1.00
A/C #: 6712148605	0.0%	-	-	-	-	-	-
A/C #: 6712148606	0.0%	-	-	-	-	-	-
A/C #: 6712148607	0.0%	-	-	-	-	-	-
A/C #: 6712148608	0.0%	-	-	-	-	-	-
<u>COACHELLA SANITARY DISTRICT: PROJECT FUND 2011</u>							
A/C #: 6711963500 Project Fund 2011	0.0%	24,943.68	-	47.49	-	-	24,991.17
COACHELLA REDEVELOPMENT AGENCY							
<u>MERGED PROJECT AREAS BONDS 98 & 99: BONDS 2013</u>							
A/C #: 6712071401 Interest Account	0.0%	38.17	(37.17)	-	-	-	1.00
A/C #: 6712071402 Interest Acc	0.0%	-	65,043.75	-	-	-	65,043.75
A/C #: 6712071403 Principal Acc	0.0%	-	390,000.00	-	-	-	390,000.00
A/C #: 6712071404 Reserve Account	0.0%	1.00	-	-	-	-	1.00
<u>SA TO COACHELLA RDA REFUNDING BONDS SERIES 2016A & 2016B</u>							
A/C #: 6712160601	0.0%	167.07	(166.38)	0.31	-	-	1.00
A/C #: 6712160602	0.0%	-	171,646.88	-	-	-	171,646.88
A/C #: 6712160604	0.0%	-	1,140,000.00	-	-	-	1,140,000.00
A/C #: 6712160604	0.0%	1.00	-	-	-	-	1.00
A/C #: 6712160605	0.0%	-	-	-	-	-	-
A/C #: 6712160606	0.0%	-	-	-	-	-	-
A/C #: 6712160607	0.0%	-	-	-	-	-	-
A/C #: 6712160608	0.0%	-	-	-	-	-	-
A/C #: 6712160609	0.0%	-	-	-	-	-	-

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A/C #: 6712179800 Special Fund	0.0%	-	-	-	-	-	-
A/C #: 6712179801 Interest Account	0.0%	417.59	-	0.62	-	-	418.21
A/C #: 6712179802 Interest Account	0.0%	-	-	-	-	-	-
A/C #: 6712179803 Principal Account	0.0%	-	-	-	-	-	-
A/C #: 6712179804 Reserve Account	3.4%	1.00	-	-	-	-	1.00
A/C #: 6712179805 Project Fund	0.0%	923,719.43	(795,606.04)	1,578.41	-	-	129,691.80
A/C #: 6712179806 Project Fund	0.0%	-	-	-	-	-	-
A/C #: 6712179807 Project Fund	0.0%	-	-	-	-	-	-
TOTAL UNION BANK OF CALIFORNIA		1,551,785.92	1,371,691.21	1,882.13	(157,287.50)	(445,000.00)	2,323,071.76
WELLS FARGO BANK, N.A.							
GAS TAX BONDS SERIES 2008-A							
A/C #: 22863900 Revenue Fund	0.0%	(0.00)	-	5.16	-	-	5.16
A/C #: 22863902 Interest Account	0.0%	441.74	-	0.66	-	-	442.40
A/C #: 22863903 Interest Account	0.0%	927.94	-	1.40	-	-	929.34
A/C #: 22863904 Reserve Fund	4.6%	451,237.50	-	-	-	-	451,237.50
A/C #: 22863906 Administration Fund	0.0%	9,634.51	-	14.49	-	-	9,649.00
A/C #: 22863909 Acquisition Fund	0.0%	-	-	-	-	-	-
TOTAL WELLS FARGO BANK		462,241.69	-	21.71	-	-	462,263.40
COUNTY OF RIVERSIDE							
COUNTY OF RIVERSIDE - FIRE		161,729.54	-	161.94	-	-	161,891.48
COUNTY OF RIVERSIDE - SANITARY		7.32	-	0.01	-	-	7.33
TOTAL COUNTY OF RIVERSIDE		161,736.86	-	161.95	-	-	161,898.81
GRAND TOTAL FISCAL AGENT CASH		2,175,764.47	1,371,691.21	2,065.79	(157,287.50)	(445,000.00)	2,947,233.97



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Jonathan Hoy, P.E., Assistant City Manager/City Engineer

SUBJECT: Reimbursement Agreement with Cabazon Band of Mission Indians for the Vista Del Norte Pavement Rehabilitation Project No. ST-121

STAFF RECOMMENDATION:

Approve Reimbursement Agreement in an amount not to exceed \$300,000.00 with Cabazon Band of Mission Indians for the Vista Del Norte Pavement Rehabilitation Project No. ST-121, pending approval as to form by City Attorney.

BACKGROUND:

Vista Del Norte is a City street located on the north side of Interstate 10 and serves as a frontage road between the City limit on the west and Dillon Road on the east (approximately ½ mile in length). The City's 2016 Pavement Management Plan rated this segment of roadway with a Pavement Condition Index (PCI) of 20, and required full depth reconstruction.

On April 10, 2019 the City of Coachella approved Resolution 2019-22, adopting a list of road rehabilitation projects to be funded by SB1 (Gas Tax). This resolution allocated up to \$600,000.00 for the Vista Del Norte Rehabilitation Project No. ST-121.

DISCUSSION/ANALYSIS:

The Cabazon Band of Mission Indians is currently working on a Tribal road rehabilitation project. This project includes the full depth reconstruction of Vista Del Norte within Tribal Boundaries, west of the City limit.

The Tribe has reached out to the City and offered to extend the project limits to include the City's portion of Vista Del Norte, and provided plans and estimates for the City to evaluate. City staff has completed its review and found that the Tribes' project meets City standards by rehabilitating Vista Del Norte and, with proper maintenance, extends its useful life twenty years.

Additionally, the 'in-place' full depth reclamation process will save the City 30-40% of its original estimated replacement cost identified in the City's approved 2019/20 Capital Improvement Program.

FISCAL IMPACT:

The project cost in an amount not to exceed \$300,000 will be funded with SB1, as designated in Resolution 2019-22. The City will reimburse the Tribe once construction is complete and has been accepted by the City.

REIMBURSEMENT AGREEMENT

**VISTA DEL NORTE PAVEMENT REHABILITATION
PROJECT NO. ST-121**

THIS AGREEMENT is made and entered into on _____ by and between the CITY OF COACHELLA, a Municipal Corporation (hereafter called "CITY") and CABAZON BAND OF MISSION INDIANS (hereafter called "TRIBE"), together referred to as "Parties" and shall terminate upon the parties' performance hereunder.

WITNESSETH:

WHEREAS, Vista Del Norte is a City street located on the north side of Interstate 10 and serves as a frontage road between the City limit on the west and Dillon Road on the east (approximately 1/2 mile in length);

WHEREAS, the City's 2016 Pavement Management Plan rated this segment of roadway with a Pavement Condition Index (PCI) of 20, and required full depth reconstruction;

WHEREAS, the Cabazon Band of Mission Indians is working on a Tribal road rehabilitation project that includes the full depth reconstruction of Vista Del Norte within Tribal Boundaries, west of the City limit;

WHEREAS, the TRIBE has reached out to the City and offered to extend the project limits to include the reconstruction of the City's portion of Vista Del Norte, in accordance with City Standards and extending the streets useful life twenty years; and

WHEREAS, the TRIBE at their own cost and expense has agreed to furnish all equipment and material necessary to perform and complete, in a good and workmanlike manner, the construction of Vista Del Norte Pavement Rehabilitation Project No. ST-121 in accordance with approved drawings on file in the office of the City Engineer provided that City reimburse TRIBE for the cost of the construction of the road segment within the City's right-of-way.

NOW THEREFORE, THE PARTIES HERETO AGREE AS FOLLOWS:

1. CITY hereby agrees to reimburse TRIBE, solely as provided herein, for the cost of constructing the Vista Del Norte Pavement Rehabilitation Project No. ST-121, located on the north side of Interstate 10 between the City limit on the west and Dillon Road on the east (approximately ½ mile in length), upon completion and acceptance of said improvements by the City Engineer. The Reimbursement shall not exceed **Three Hundred Thousand Dollars** (\$300,000.00).

2. Upon completion of the improvements stated in paragraph 1 above, TRIBE agrees to notify the City Engineer and submit an itemized statement outlining TRIBE'S cost in making said improvements. Including verification that the TRIBE shall comply with all applicable Labor Code provisions, which include, but are not limited to the payment of not less than the required prevailing rates to all workers employed by them in the execution of the Contract, the employment of apprentices, the hours of labor and the debarment of contractors and subcontractors.

3. TRIBE shall defend, hold harmless, and indemnify CITY as to any and all claims, judgments, liabilities or damages for personal injuries and property damage directly arising out of TRIBE'S performance of the Agreement.

4. This agreement shall be binding upon and for the benefit of the Parties and their respective agents, assigns, directors, employees, executors, officers, representatives, and successors.

5. The Parties expressly state that they have consulted with their respective attorneys of record concerning all portions of this Agreement and they have been fully advised by their attorneys with respect to their rights and obligations hereunder.

6. This Agreement constitutes the entire agreement between the Parties, and it is expressly understood and agreed that this Agreement may not be altered, amended, modified or otherwise changed in any respect or particular whatsoever except by a writing duly executed by authorized representatives of the Parties hereto. The Parties hereby agree and acknowledge that they will make no claim at any time or place that this agreement has been orally altered or modified or otherwise changed by oral communication of any kind or character.

7. Each of the undersigned signing on behalf of a Party which is not an individual person warrants that he or she is authorized to sign it for and binds such party.

8. This Agreement shall be governed by, construed in accordance with and interpreted under the laws of the State of California and shall be deemed to have been entered into in Coachella, California, for purposes of venue no matter where actually executed.

9. If any provision of this Agreement shall be determined to be contrary to law or unenforceable, the remaining provisions shall, at the option of the party who would have been benefited by such unenforceable provision, be severable and enforceable in accordance with their terms.

10. This Agreement may be executed in multiple identical counterparts, each of which shall be deemed an original, but all of which together will constitute one and the same instrument but the parties agree that the agreement on file in the Office of the Coachella City Clerk is the version of the agreement that shall take precedent should any differences exist among counterparts of the document.

11. The Parties each agree that, in the event of any breach of this Agreement, the party aggrieved shall be entitled to recover from the party who breaches, in addition to any other relief provided by law, such costs and expenses as may be incurred by said party, including court costs, attorney's fees, and other costs and expenses, taxable or otherwise, reasonably necessary in preparing the defense of, defending against, or seeking or obtaining an abatement of, or an injunction against, such action or proceeding, or in enforcing this Agreement, or in establishing or maintaining the applicability of, or the validity of, this Agreement, or any provision thereof, and in the prosecuting any counterclaim or cross-complaint based thereon.

IN WITNESS WHEREOF, said authorized representatives of said parties have affixed their signatures on the date set forth above.

CITY OF COACHELLA,
a Municipal Corporation

CABAZON BAND OF MISSION
INDIANS

By: _____
Steven Hernandez
Mayor

By: _____
Name:
Title:

ATTEST: CITY CLERK

By: _____
Angela M. Zepeda

APPROVED AS TO FORM:

By: _____
Carlos Campos
City Attorney



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Luis Lopez, Development Services Director

SUBJECT: Resolution No. 2019-58, Appeal of Planning Commission's denial of Time Extension Request for Tentative Tract Map No. 31978 (Bellissima) for the subdivision of approximately 38 acres into 158 single-family residential lots located at the south east corner of Avenue 53 and Frederick Street. Coachella Investments, LLC, Appellant.

STAFF RECOMMENDATION:

Staff recommends that the City Council overturn the decision of the Planning Commission and approve the third time extension for the remaining phases of Tentative Tract Map No. 31978 (Bellissima), by adopting the attached draft Resolution No. 2019-58 approving a third and final 12-Month Time Extension for Tentative Tract Map No. 31978 for the subdivision of approximately 38 acres into 158 single-family residential lots located at the southeast corner of Avenue 53 and Frederick Street.

BACKGROUND:

Pursuant to the Subdivision Map Act, and Title 16 of the Coachella Municipal Code, a tentative tract map expires after 24 months unless the final map is recorded. The Planning Commission may grant up to three 12-month time extensions. On October 13, 2004 the City Council approved Resolution No. 2006-79 which approved the original project including a Mitigated Negative Declaration pursuant to CEQA guidelines (Environmental Assessment No. 04-13) and Tentative Tract Map No. 31978. On June 20, 2006 the first phase of Tentative Tract Map 31978 was recorded which included 49 single family lots which were built and sold. Between 2008 and 2014, Tentative Tract Map 31978 was eligible for several automatic time extensions granted by the State of California.

In 2016 the City Council was reviewing the first 12-month time extension request for this project. Because the original subdivision was approved in the early 2000's, staff was directed to schedule a study session to discuss all of the City's expiring tentative subdivision maps and develop policies for these expiring tentative maps in light of the 2015 General Plan update. After the City Council denied the first time extension, the owners filed a lawsuit against the City, wherein the parties ultimately reached a settlement that allowed the subdivider to obtain the time extension and the City agreed to not impose new conditions of approval upon the project. In 2018 the Planning Commission granted the 2nd time extension request.

DISCUSSION/ANALYSIS:

The current landowners of the Bellissima subdivision property are requested their 3rd and final time extension on the tentative subdivision map. On August 7, 2019 the Planning Commission voted 3/0 (one Abstention) to deny the request for 12-month Time Extension for Tentative Tract Map No. 31978. There was one Commissioner whom abstained, two commissioners absent, and the Alternative Commissioner voted. The findings of the Planning Commission in denying the request, was that the project does not fully comply with the City’s General Plan 2035 in that there will be no public park within ¼ mile for at least 75 % of the residents. Additionally, the block distances and minimum community entrance policies of the General Plan are not fully met with the original subdivision design because the project lacks a community entrance on Avenue 53 and some of the interior streets are greater than 800 feet in length. Notwithstanding the project’s inconsistency with the General Plan, the Commission voted to deny the request.

DISCUSSION/ANALYSIS:

Environmental Setting:

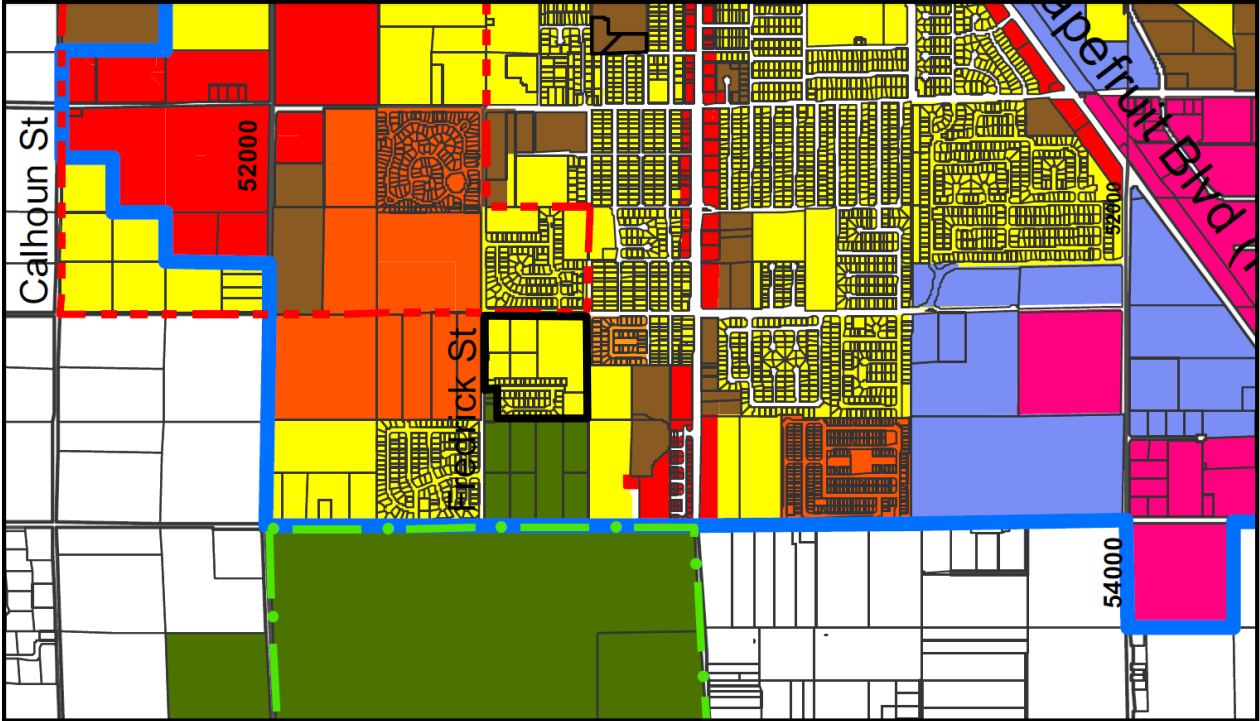
The proposed project is located on 38 acres of land that has been partially developed with the first phase of the subdivision, in the R-S (Single Family Residential) Zone and is the remaining unrecorded portions of Tract No. 31978.

The site is designated “Suburban Neighborhood” on the 2035 Coachella General as illustrated below:



Surrounding properties to the north, south, east and west are also designated “Suburban Neighborhood.”

The site is zoned R-S (Residential single Family) as illustrated below:



Surrounding properties to the north are R-S (Residential Single Family), east and west are R-PUD (Residential Planned Unit Development and south is A-T (Agricultural Transition).

Environmental Review:

On October 13, 2004, the City Council adopted a Mitigated Negative Declaration as part of Environmental Assessment No. 04-13 and approved Tentative Tract Map 31978. There are no changes to the proposed subdivision as a result of this time extension request, that would result in any new environmental effects. Similarly, the vicinity has not experienced any major change in the built environment, except that the Tierra Bonita community to the north is now built out and had been previously anticipated as a single family residential community. Several other large subdivisions on the west side of Frederick have remained vacant and are waiting for the residential market to return to this part of the City. Therefore, no further environmental review is required for the proposed 12 month time extension.

ALTERNATIVES:

- 1) Approve the 3rd and final time extension for Tentative Tract Map No. 31978
- 2) Deny the time extension request.
- 3) Continue this item and provide staff with direction.

FISCAL IMPACT:

There are no fiscal impacts associated with a 12-month time extension for Tentative Tract Map No. 31978 in that the project will be required to pay all normal user fees for cost recovery, and all future development will pay applicable permit fees and development impact fees.

RECOMMENDED ALTERNATIVE(S):

Staff recommends No. 1 as noted above.

Attachments: Resolution No. 2019-58
 Aerial Photo
 TTM 31978 Map Exhibit
 Appeal Request

RESOLUTION NO. 2019-58

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA OVERTURNING THE PLANNING COMMISSION'S DECISION AND APPROVING A THIRD AND FINAL 12-MONTH TIME EXTENSION FOR TENTATIVE TRACT MAP NO. 31978 (BELLISSIMA) TO ALLOW THE REMAINING PHASES OF A SUBDIVISION OF 38 ACRES INTO 158 SINGLE-FAMILY RESIDENTIAL LOTS, ON PROPERTY LOCATED AT THE SOUTHEAST CORNER OF AVENUE 53 AND FREDERICK STREET. (COACHELLA INVESTMENTS, LLC, APPELLANT).

WHEREAS, Coachella Investments, LLC filed a timely request for the third 12-month Time Extension request for Tentative Tract Map No. 31978 to allow the remaining phases of an approved subdivision of 38 acres of land into 158 single-family residential lots, on property located at the southeast corner of Avenue 53 and Frederick Street (APN 768-430-002, -015, -017, -022 & -023); and,

WHEREAS, the City has processed said application pursuant to the Subdivision Map Act (commencing with Section 64600, Title 7 of the Government Code and the California Environmental Quality Act of 1970) as amended; and

WHEREAS, on August 7, 2019, the Planning Commission of the City of Coachella held a duly noticed and published Public Hearing and considered the Tentative Tract Map as presented by the applicant, adopting the finding, amended and added conditions, and staff recommendations; and,

WHEREAS, the Planning Commission on August 7, 2019 voted 3/0 to deny the time extension request for Tentative Tract Map No. 31978 making certain oral findings of inconsistency with the City's General Plan policies; and,

WHEREAS, the applicant filed a timely appeal of the Planning Commission's denial of the time extension request; and,

WHEREAS, the City Council of the City of Coachella held a hearing on October 23, 2019 to review the appeal request, and allowed public testimony on the matter; and,

WHEREAS, the City Council of the City of Coachella finds that Tentative Tract Map 31978 is in compliance with the Subdivision Map Act and the City's Subdivision Ordinance.

NOW, THEREFORE, BE IT RESOLVED, that the City Council of the City of Coachella, California does hereby overturn the decision of the Planning Commission and approves a third and final 12-month time extension for Tentative Tract Map 31978 with the findings listed below, incorporating all of the prior conditions of approval contained in City Council Resolution No. 2006-79 as adopted on October 13, 2004, with no modifications or additional conditions.

Findings for Tentative Tract Map No. 31978 (12-month time extension)

1. The proposed map and design of improvements are substantially consistent with the General Plan, and the City of Coachella Official Zoning Map and any specific plan governing the site. The proposed subdivision time extension is within a land use designation of Suburban Neighborhood according to the General Plan 2035 Land Use Policy Diagram. The subdivision is a low-density residential project (4.15 units per acre) that is consistent with the Suburban Neighborhood intensity permitted by the land use category. Tentative Tract Map 31978 is in compliance with the subdivision standards of the Zoning Ordinance with respect to the R-S (Single Family Residential) Zone.
2. The site is physically suitable for the type of development and the proposed density. The proposed remaining phases of the proposed subdivision will provide adequately sized lots for future residential development. All proposed lots will have adequate dimensions, and ingress and egress to accommodate future development.
3. The design of the subdivision and type of improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. There are no sensitive habitats or bodies of water in the immediate vicinity of the site. All drainage from increased impervious material on the site will be contained on site for a 100-year storm event, as required by City regulations. As such there would be no impact to the Coachella Valley Whitewater Channel which is the nearest body of water to the site.
4. The design of the subdivision and type of improvements are not likely to cause any serious public health problems. The proposed subdivision already has the first phase of development completed and this time extension would allow for future development of residential uses consistent with the established pattern. The existing neighborhood has not had any observable adverse effects on the public health or safety. All future development would be reviewed for compliance with applicable California Building Code regulations prior to issuance of any building permits.
5. The design of the subdivision and type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. The proposed subdivision would create 158 lots for residential development with adequate street frontage, access, and utility connections to all properties. There are no known easements that would conflict with the proposed subdivision.
6. An Environmental Assessment/Initial Study (EA No. 04-13) was prepared for the subject project pursuant to the State of California Environmental Quality Act Guidelines (CEQA). On October 13, 2004, the City Council adopted a Mitigated Negative Declaration as part of EA 04-13 and approved Tentative Tract Map 31978. There are no changes to the proposed subdivision as a result of this time extension request, that would result in any new environmental effects. Similarly, the vicinity has not experienced any major change in the built environment, except that the Tierra Bonita community to the

north is now built out and had been previously anticipated as a single family residential community. Several other large subdivisions on the west side of Frederick have remained vacant and are waiting for the residential market to return to this part of the City. Therefore, no further environmental review is required for the 12-month time extension.

PASSED, APPROVED and ADOPTED this 23rd day of October 2019:

Steven A. Hernandez
Mayor

ATTEST:

Angela M. Zepeda
City Clerk

APPROVED AS TO FORM:

Carlos Campos
City Attorney

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF COACHELLA)

I HEREBY CERTIFY that the foregoing Resolution No. 2019-58 was duly adopted by the City Council of the City of Coachella at a regular meeting thereof, held on the 23rd day of October 2019, by the following vote of Council:

AYES:

NOES:

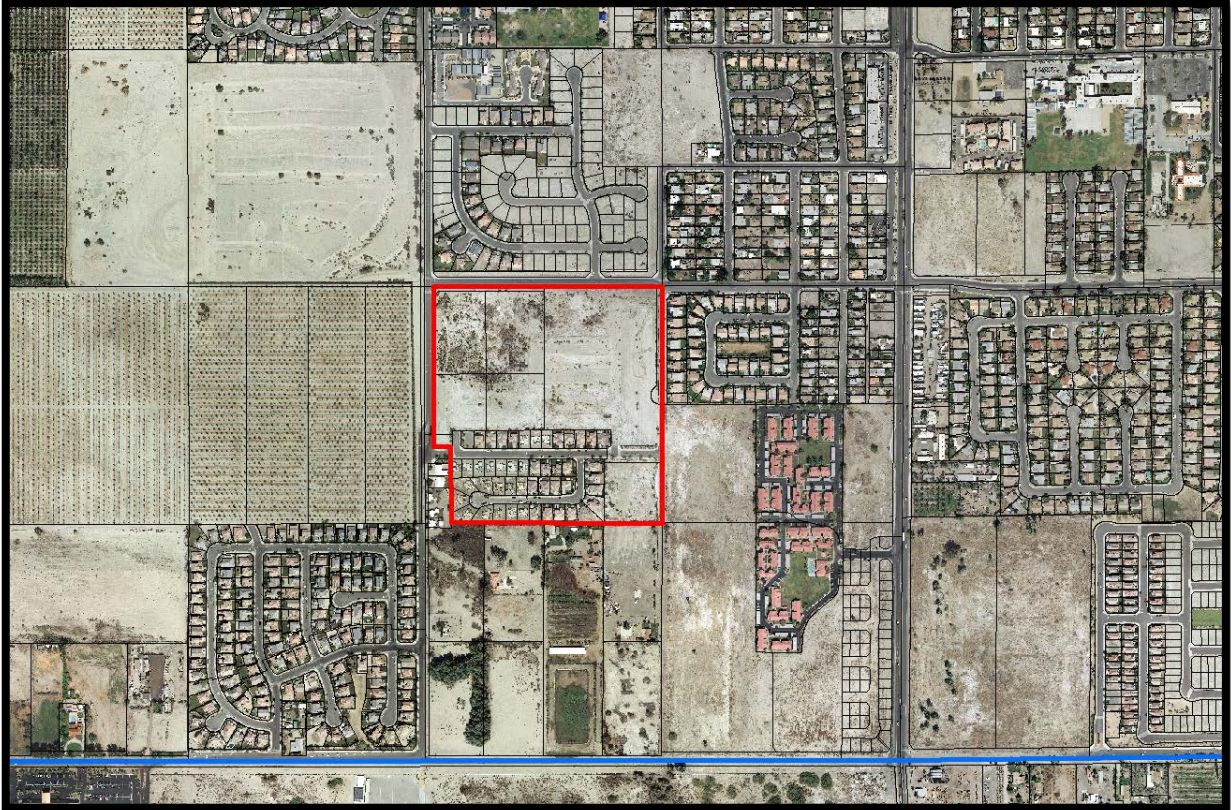
ABSENT:

ABSTAIN:

Andrea J. Carranza, MMC
Deputy City Clerk

Aerial Photograph

"BELLISSIMA"



NORTH
Not To Scale

IN THE CITY OF COACHELLA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE TRACT MAP NO. 31978
 PORTION SECTION 7, T.6S., R.8E., S.B.M., RIVERSIDE COUNTY, CALIFORNIA
 PACIFIC ENGINEERING & ASSOCIATES
 MARCH 2004
 38 ACRES

OWNER/DEVELOPER:
 THE REAL PROPERTY EQUITY GROUP, INC.
 10000 W. 10TH AVE., SUITE 100
 THOUSAND PALMS, CA 92278
 CONTACT: LARK LOWMY

LAND PLANNER:
 LARRY HUGHES
 10000 W. 10TH AVE., SUITE 100
 THOUSAND PALMS, CA 92278
 (805) 598-0119

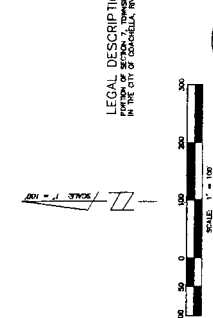
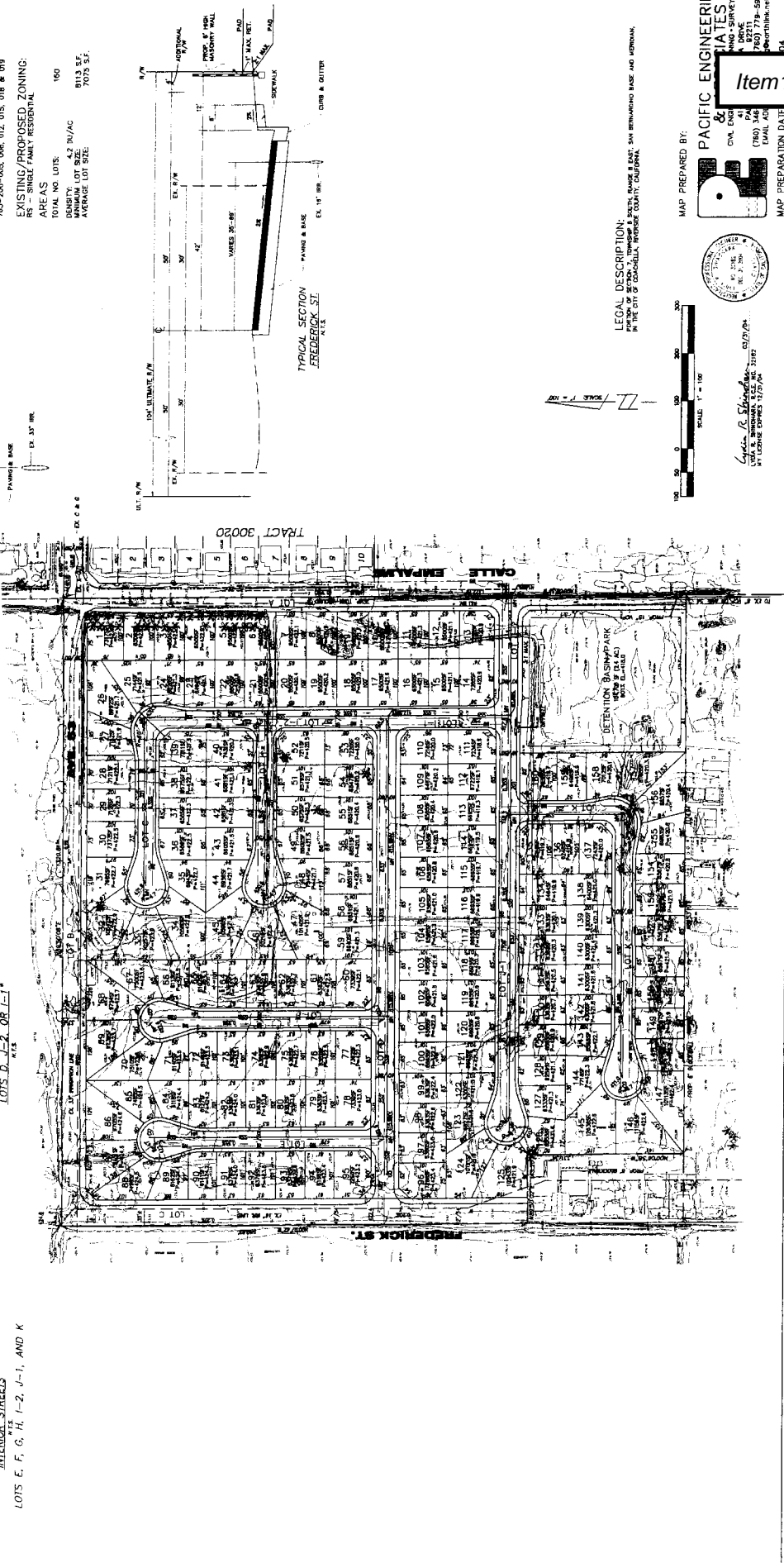
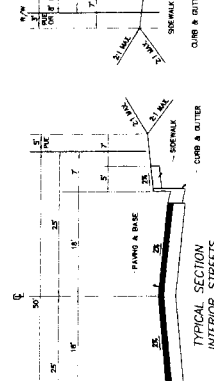
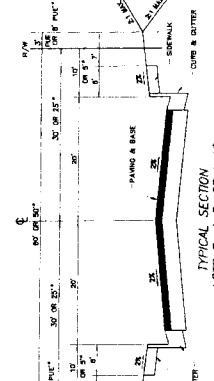
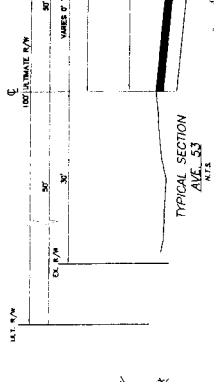
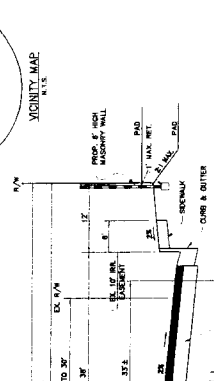
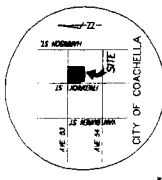
UTILITY AGENCIES:
 WATER: THE CITY OF COACHELLA 388-5744
 ELECTRIC: SOUTHWEST GAS (800) 388-5864
 GAS: SOUTHWEST GAS (800) 382-0713
 (INTERIOR UTILITIES TO BE PLACED UNDERGROUND)

SCHOOL DISTRICT:
 COACHELLA VALLEY UNIFIED SCHOOL DISTRICT
 785-209-0005, 006, 012, 015, 018 & 019

ASSESSOR'S PARCEL NUMBER:
 785-209-005, 006, 012, 015, 018 & 019

EXISTING/PROPOSED ZONING:
 R-1 SINGLE-FAMILY RESIDENTIAL

AREAS
 TOTAL NO. LOTS: 160
 DENSITY: 4.2 U/I/A/C
 MINIMUM LOT SIZE: 8113 S.F.
 AVERAGE LOT SIZE: 7075 S.F.



MAP PREPARED BY:
PACIFIC ENGINEERING & ASSOCIATES
 CIVIL ENGINEER
 10000 W. 10TH AVE., SUITE 100
 THOUSAND PALMS, CA 92278
 (800) 388-5864
 FAX (800) 382-0713
 EMAIL: info@paceng.com
 www.paceng.com

LEGAL DESCRIPTION:
 PORTION OF SECTION 7, T.6S., R.8E., S.B.M., RIVERSIDE COUNTY, CALIFORNIA
 IN THE CITY OF COACHELLA, RIVERSIDE COUNTY, CALIFORNIA

MAP PREPARATION DATE: 03/17/04

MAP PREPARATION DATE: 03/17/04

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MAP PREPARATION DATE: 03/17/04

Item 13.



CITY OF COACHELLA
DEVELOPMENT SERVICES DEPARTMENT
REQUEST FOR APPEAL

Date: August 12, 2019

APPLICATION INFORMATION:

Project Address: Southeast corner of Avenue 53 and Frederick Street

Case Type (CUP, TTM, etc) and Number: Tentative Tract Map No. 31978 (Bellissima)

Hearing/Decision Date: August 7, 2019 Appeal Deadline: August 23, 2019 (est.)

APPELLANT INFORMATION:

Appellant: Coachella Investments, LLC

Appellant Address: 4370 La Jolla Village Drive, Suite 640
San Diego, CA 92122

Appellant Phone: 858-535-9000 Appellant Fax: _____

Appellant e-mail: roberto@paragoncompany.com

Applicant (If Different): _____

I hereby appeal the decision of the:

Planning Director

Planning Commission

REASON FOR APPEAL:

The decision maker failed to comply with the provisions of the Zoning Code, General Plan or other applicable plans in the following manner (use additional sheets if necessary):

The Planning Commission offered no reason for the denial of the Extension of Time Request. This action is unwarranted and illegal.

Office Use Only: _____

Date Received: _____

Appeal Fees: _____

Received by: _____



STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Luis Lopez, Development Services Director

SUBJECT: Resolution No. 2019-59, Pueblo Viejo Implementation Strategy Plan

STAFF RECOMMENDATION:

Adopt Resolution 2019-59 to approve the Pueblo Viejo Implementation Strategy Plan, based on the Final Plan dated October 2019 as a policy guide in processing and conditioning projects in the Pueblo Viejo District, and to bring forth a future General Plan Amendment, Zoning Code Amendments, including environmental review documents, for adoption.

EXECUTIVE SUMMARY:

Staff and City stakeholders have been working with city consultant, Michael Baker, International over the past two years to finalize the Pueblo Viejo Implementation Strategy which is a follow-up to the 2010 “Pueblo Viejo Revitalization Plan” document. This document will update the City’s Design Guidelines and sets forth a path forward to revise the City’s Off-Street Parking regulations and to create new “mixed use zoning” districts for the identified sub-zones within the Pueblo Viejo District. Because these follow-up actions require a future General Plan Amendment and related environmental review documents, staff is deferring the adoption of ordinances at this time, and is seeking direction from City Council.

The Plan recommendations include the following:

- Four (4) new proposed special zoning districts specific to the Pueblo Viejo District that would serve to implement the vision outlined in both the City’s General Plan and the Pueblo Viejo Revitalization Plan.
- Proposed design guidelines to complement the proposed special zoning districts and to ensure implementation of quality design that is consistent with the vision for the district.
- Parking recommendations crafted through a parking study of the district focused on projected future development scenarios to ensure a balance of parking supply and demand while also ensuring parking regulations that are compatible with the vision for the district

Implementation of these recommendations will require the following next steps:

- Finalize proposed Zoning Ordinance Amendments, adopt design guidelines by reference in the Municipal Code, and parking recommendations.
- Harmonize the current General Plan land use with current land use recommendations and modern development needs. This necessitates a General Plan Amendment to create the “transition zone” around Veterans Park, and to create full-block mixed use zones between 6th Street and 7th Street.
- Prepare appropriate CEQA clearance for harmonization of the General Plan, zoning ordinance amendments, and the design guidelines.

BACKGROUND:

In the summer of 2008, the City of Coachella and Sunline Transit Agency were awarded a planning grant through the Southern California Association of Governments (SCAG) Compass Grant program to prepare the “Pueblo Viejo Revitalization Plan” which was approved by City Council on February 24, 2010 as a development guide for the downtown central business district. The City was awarded the grant in part because SCAG determined that the downtown area of Coachella fits the model as a “strategic growth area” of the Southern California region. These identified areas have capacity for urban growth and intensification through mixed land-use development and with access to public transit, resulting in improved jobs/housing balance and reduced vehicle trips (when compared to traditional suburban development patterns), promoting the long-range goals of SB 375 (Steinberg) legislation.

Accordingly, on April 22, 2015, the City finalized the General Plan 2035 comprehensive update which re-designated the 6th Street corridor, Cesar Chaves Street, and Grapefruit Boulevard with a “Downtown Center” land use designation allowing very-high density residential uses in combination with ground floor commercial uses, to help facilitate the vision plan set forth in the Pueblo Viejo Revitalization Plan. The next step in implementing this vision was authorized by City Council in summer 2017 when Michael Baker, International was hired to prepare the Pueblo Viejo Implementation Strategy.

Public Engagement:

Engaging all members of the community has been central in the development of the Pueblo Viejo Revitalization Plan and the Implementation Recommendations Report. During the preparation of the Revitalization Plan, public outreach included:

- Community Workshop No. 1 – January 27, 2009
- Stakeholder Interviews – January 27, 2009
- Community Workshop No. 2 – April 28, 2009
- Joint Study Session with Planning Commission and City Council – May 18, 2009

For the Implementation Plan, staff and the consultants had a public meeting with the Planning Commission on January 31, 2018, and a meeting with Sunline Transit Agency on March 27,

2018. Subsequently, a 3-day charrette (November 14-18, 2018) was hosted by the City to gather feedback from City staff, City Councilmembers, Planning Commissioners, and public stakeholders. The primary goal of the charrette was to gain confirmation of the vision presented in the Pueblo Viejo Revitalization Plan and discuss the best methods for codifying the items within the Revitalization Plan. The charrette included the following:

- Site Tour – November 14, 2017
- Stakeholder Interviews – November 14, 2017
- Technical Charrette with staff to focus on policy-based decisions – November 15, 2017
- Study Session with Planning Commission – November 15, 2017
- Meeting with Mayor
- Meeting with City Councilmember Betty Sanchez

The Draft Design Guidelines were completed in December 2017, and the Planning Commission recommended approval of the Implementation Strategy Plan in January 2018, and subsequent actions by the City staff are explained in the section below.

DISCUSSION/ANALYSIS:

The Pueblo Viejo Implementation Strategy Plan is an updated version of the Pueblo Viejo Revitalization Plan’s architectural guidance and creates a revised “design guidelines” document. Additionally, the Plan provides a revised zoning map for the downtown, and a Parking Study with conclusions and guidance for subsequent ordinance adoptions. The future districts in the downtown will create a “mixed use” designation to allow the conversion of existing residential properties around Veterans Park into neighborhood commercial and multifamily residential uses. Similarly, in order to promote vertical “mixed use” development along 6th Street, it is necessary to create mixed use zoning with “super blocks” that allow for parking garages to be accessed off of 7th Street. These zoning map schemes will trigger a General Plan Amendment and an Environmental Assessment which were outside of the scope of work for the Plan. Therefore, staff intends to publish the Plan with all the background and source documents with justification for reducing the City’s Parking regulations, and to create the new zoning districts and related land use regulations and development standards.

The final Plan is organized into three parts available to download on the City’s webpage for the Pueblo Viejo documents, as shown below:

<https://www.coachella.org/departments/pueblo-viejo-revitalization-plan>

The web links at the above webpage are organized into three parts as follows:

- Part I (Architecture / Design Guidelines)
- Part II (Parking Study / Visual Preferences A)
- Part III (Visual Preferences B)

The table of contents includes the following sections:

- 1) Executive Summary
- 2) Sixth Street Pueblo Viejo Zone
- 3) Grapefruit Boulevard Pueblo Viejo Zone
- 4) Cesar Chavez Street Pueblo Viejo Zone
- 5) Transition Area Pueblo Viejo Zone
- 6) Design Guidelines
- 7) Parking Study
- 8) Public Engagement Summary
- 9) Appendix- Visual Preference Survey

The Planning Commission held an initial public workshop on the Implementation Strategy Plan on January 31, 2018. However, on June 13, 2018 the City Council augmented the consultant's scope of work to include tasks for completion of the environmental documents and further development of the ordinances related to new zoning regulations and reduced parking standards for the Pueblo Viejo District. The new design guidelines were intended to be adopted by reference into the Municipal Code. There were other costs related to additional meetings that were needed for the Planning Commission's review of the Visual Preferences Survey and one new meeting with the City Council to review the progress of the draft design guidelines prior to final adoption. The Parking Study scope was approved to include the following tasks:

- *Conduct an inventory of existing on and off street parking spaces including unmarked spaces on commercial and residential streets. Inventory will also document restrictions such as time limits, private property with parking restrictions, etc.*
- *Coordinate with Engineering/Public Works to determine extension of 6th Street design to other streets and calculate changes to parking based upon design changes, for the purpose of increasing availability of on-street parking.*
- *Calculate current demand based upon available spaces and typical parking requirements per business type.*
- *Calculate future demand based upon a build-out analysis of the Pueblo Viejo based upon proposed changes to land use.*
- *Calculate demand for parking for special events held within the Pueblo Viejo.*
- *Analyze potential surface parking opportunities.*
- *Provide recommendations on potential street design to accommodate additional parking, business cooperatives or parking districts to make more efficient use of off-street parking, demand management techniques, land use regulations required for off-street parking (including parking structures) for commercial, mixed-use and or residential uses, and for financing parking alternatives such as public-private partnerships, infrastructure financing districts, parking districts, BIDs and other options to create partnerships and revenue to increase and maintain parking within the Pueblo Viejo.*

- *Make recommendations to improve wayfinding and parking regulations including, but not limited to marking public parking areas and homogenizing parking signage and restrictions.*

The Final Plan – October 2019 incorporates all of the originally-approved tasks, including the Addendum #1 tasks but excluded the environmental documents because there was a need to pursue a General Plan Amendment, and the scope of work for the environmental effects for this task exceed the budget that was originally granted. Rather than delay the adoption of the Plan as a policy guide, staff is recommending that City Council approve the Plan, and direct staff to bring back a General Plan Amendments, Zoning Code Amendments, and related environmental documents at a later date, when the work can be budgeted into the current work program.

Conclusions:

The attached draft Resolution 2019-59 contemplates the adoption of the Pueblo Viejo Implementation Strategy Plan, and includes the visual preferences recommendations provided by the Planning Commission during the public workshop. Staff and the consultant worked closely with the stakeholders, City staff, and the Chamber of Commerce to bring this Plan forward. The City has been successful in procuring grant funds for projects that are in keeping with the City’s vision for the Pueblo Viejo Revitalization Plan and this Implementation Strategy solidifies the City’s next steps for formal action items and includes documented public engagement for adoption of this Plan.

The Final Plan dated October 2019 is attached to this staff report.

ALTERNATIVES:

1. Approve Resolution 2019-59.
2. Approve Resolution 2019-59 with modifications.
3. Continue this matter and provide direction to staff on the Pueblo Viejo Implementation Strategy Plan.
4. Take no action.

FISCAL IMPACT:

There are no fiscal impacts associated with approving the Pueblo Viejo Implementation Strategy Plan as a policy document in that it will merely guide the Planning Commission and City Council in the review and approval process of new projects, and future General Plan Amendments and Ordinance adoptions.

RECOMMENDED ALTERNATIVE(S):

Staff recommends Alternative #1 as stated above.

Attachments: Resolution 2019-59
Pueblo Viejo Revitalization Plan (Final Plan - October 2019)

RESOLUTION NO. 2019-59

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA ACCEPTING THE PUEBLO VIEJO IMPLEMENTATION STRATEGY PLAN AS A GUIDE IN AMENDING THE GENERAL PLAN, DEVELOPING FUTURE ORDINANCES/REGULATIONS AND IN PROCESSING AND CONDITIONING PROJECTS IN THE PUEBLO VIEJO DISTRICT. APPLICANT: CITY-INITIATED.

WHEREAS, in the summer of 2008 the City of Coachella and Sunline Transit Agency were awarded a Compass Grant through the Southern California Association of Governments to prepare the Pueblo Viejo Revitalization Plan for the area known as the “Pueblo Viejo District”, generally bounded by Grapefruit Boulevard, Harrison Street, and Ninth Street; and,

WHEREAS, the Planning Commission reviewed the Pueblo Viejo Revitalization Plan and made recommendations to City Council and in February 2010 the City Council approved the Pueblo Viejo Revitalization Plan as a policy guide to develop future ordinances and to review and approve projects in the Pueblo Viejo District; and,

WHEREAS, the Planning Commission of the City of Coachella, California conducted a visual preferences survey and reviewed the staff and consultant recommendations at their regularly scheduled meeting of January 31, 2018 wherein the voted to recommend that the City Council approval of the Pueblo Viejo Implementation Strategy Plan; and,

WHEREAS, the adoption of the Pueblo Viejo Implementation Strategy Plan does not qualify as a “project” as defined in Section 15378 of the California Environmental Quality Act (CEQA) Guidelines because the Implementation Strategy Plan is a policy document that will guide future development and does not independently result in any physical changes to the environment; and,

WHEREAS, the City Council of the City of Coachella finds that approval of the Pueblo Viejo Implementation Strategy Plan, final Plan dated October 2019, will require a future General Plan Amendment in to adopt ordinances and implement new zoning district boundaries based on the City Council’s vision for the downtown central business district, and is in the public interest.

NOW, THEREFORE, the City Council of the City of Coachella, California hereby resolves and orders:

Section 1. The Pueblo Viejo Implementation Strategy Plan (Final Plan dated October 2019) is hereby approved as a policy document for the defined Pueblo Viejo District to guide staff and applicants in designing and reviewing new and renovated projects, and for the Planning Commission and City Council to use in approving and conditioning new projects in the Pueblo Viejo District.

Section 2. The Revitalization Plan is to be used by staff and the Planning Commission as

a guide in developing a future General Plan Amendment, and an update to the City’s Off-Street Parking Regulations, and to create new Zoning Districts and related Ordinances for the Pueblo Viejo sub-zones as identified in the Pueblo Viejo Implementation Strategy Plan.

Section 3. The Pueblo Viejo Implementation Strategy Plan is approved and staff is directed to bring back a future General Plan Amendment, Zoning Ordinance Amendments, and related environmental assessments based on the vision in the Implementation Strategy Plan.

PASSED, APPROVED AND ADOPTED at a regular meeting of the City Council of the City of Coachella held on October 23, 2019 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Steven A. Hernandez, Mayor
City of Coachella

ATTEST:

Angela M. Zepeda, City Clerk
City of Coachella

APPROVED AS TO FORM:

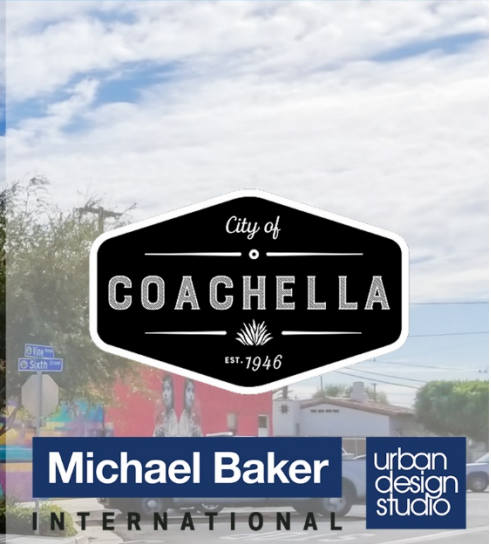
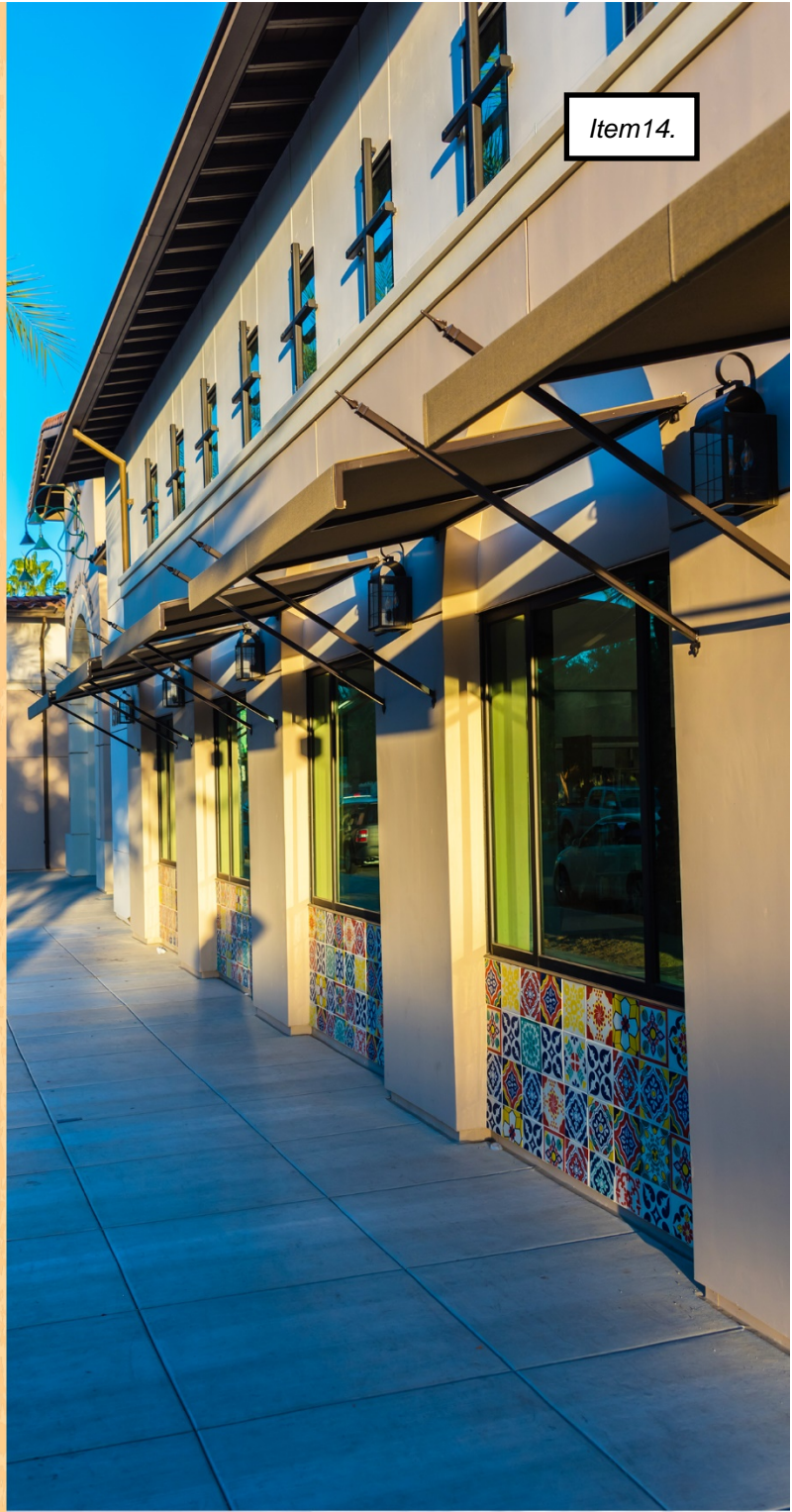
Carlos L. Campos, City Attorney
City of Coachella



Item 14.

Pueblo Viejo Implementation Strategy Plan

Final Plan
October 2019





"Pueblo Viejo is the civic and cultural heart of Coachella. The community is proud of the historic charm, locally-owned businesses, and vibrant civic center. As you enter through the attractive gateways on Sixth Street, you are immersed in a lively street scene offering shady walkways, cooling water fountains, outdoor dining, and unique shopping. Once empty lots are now filled with mixed-use buildings that respect the heritage, climate, and community values. Family-friendly events and festivals fill the streets and public spaces. As you relax in the clean, well maintained civic center core, you know . . .

you have arrived in Pueblo Viejo!"



ACKNOWLEDGMENTS

City Council

- Steven Hernandez - *Mayor*
- Emmanuel Martinez - *Mayor Pro Tem*
- Philip "Felipe" Bautista
- Megan Beaman Jacinto
- Josie Gonzalez

Planning Commission Members

- Mike Etheridge - *Chair*
- Mario Zamora - *Vice Chair*
- Denise Delgado
- Atay Ramirez
- Kimberly Miranda

Staff

- Luis Lopez - *Development Services Director*
- Jonathan Hoy, P.E. - *City Engineer*

Focus Group Participants

- | | |
|-----------------|-------------------|
| Janell Percy | David Davis |
| Jocelyn Vargas | Patrick Swarthout |
| Noel Ramos | Silvia Paz |
| Katie Stice | Randy |
| Kenneth Rodgers | Diana Soto |
| Dr. Edwin Gomez | Josh Bonner |

Consultants





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Purpose of the Report

The purpose of this report is to provide implementation strategy recommendations for the Pueblo Viejo Revitalization Plan adopted in March 2010. The Plan identified preferred architectural styles for existing and future development, major gateways as opportunities for development and enhancement, and catalyst projects to spur growth. It also included suggestions for parking, and circulation improvements. The purpose of this report is to provide a framework of recommendations and draft implementation documents to implement the regulatory framework for development within the Pueblo Viejo District.

The implementation recommendations include:

- Four new proposed special zoning districts specific to the Pueblo Viejo District that would serve to implement the vision outlined in both the City's' General Plan and the Pueblo Viejo Revitalization Plan.
- Proposed design guidelines, to complement the proposed special zoning districts, to ensure implementation of quality design that is consistent with the vision for the district.
- Parking recommendations crafted through a parking study of the district focused on projected future development scenarios to ensure a balance of parking supply and demand while also ensuring parking regulations that are compatible with the vision for the district.

Implementation of these recommendations will require the following next steps:

- Finalize Design Guidelines, Zoning, and Parking recommendations.
- Harmonize the current General Plan land use with current land use recommendations and modern development needs.
- Prepare appropriate CEQA clearance for harmonization of the General Plan, Zoning Ordinance amendments and the design guidelines.

Zoning

There are four new zoning districts proposed for the Pueblo Viejo district:

- CC-PV – Cesar Chavez Street Pueblo Viejo Zone
- GB-PV – Grapefruit Boulevard Pueblo Viejo Zone
- TR-PV – Transition Area Pueblo Viejo Zone
- SS-PV – Sixth Street Pueblo Viejo Zone

These new zones are crafted to provide the primary regulatory framework for implementation of the General Plan in the Pueblo Viejo District and ensure an orderly transition of uses, structures and continuing compatibility with surrounding residential areas. Each zone also has a corresponding subarea within the Design Guidelines that provides complementary and more detailed information, best practices and guidelines as part of required design review within the Pueblo Viejo District. Overall the intent of the new zones is to:



- Allow for the intensity of development envisioned in the General Plan and Pueblo Viejo Revitalization Plan.
- Allow for flexibility and mixed-use development to incorporate more modern demands for housing choice, experiential retail, and a more active and vibrant Pueblo Viejo District.
- Ensure the compatibility of new development with existing development and surrounding residential areas.
- Create opportunities for more street life and public/private open spaces for activities, people watching and recreation.
- Ensure a balance in parking standards that will not hinder private sector investment and will also account for changing trends in automobile ownership, mixed-use development and eventual changes in vehicle operations (i.e. autonomous vehicles).

This section describes the intent and purpose of each zone:

CC-PV – Cesar Chavez Street Pueblo Viejo Zone: This zone encourages future mixed-use development but recognizes the need to support existing uses that were previously allowed under the C-G General Commercial Use Zone. While encouraging pedestrian-friendly connections and safe multi-modal access, this zone recognizes current automobile-oriented uses but allows for flexibility in reducing parking requirements and retrofitting existing development as the market-demands call for new, innovative and higher intensity development styles.

SS-PV – Sixth Street Pueblo Viejo Zone: This zone is envisioned as a higher-density mixed-use (either vertical and/or horizontal) downtown area that provides for a wide variety of multi-family residential housing, office, and retail uses. This zone encourages a high-energy pedestrian-friendly environment with street-facing buildings, maintained building lines, a variation of architectural character, and sidewalk and public spaces to provide for gathering spaces and promote outdoor activities including dining, people watching, public art and passive recreation. Vehicle access would be designed to minimize impacts on the pedestrian environment with alley and rear entry access, parking in structures or internal to blocks, and services located behind buildings, in alleyways or rear parking areas.

TR-PV – Transition Area Pueblo Viejo Zone: This zone is intended to allow for an orderly transition in uses from single-family, industrial and mixed-commercial into a low-intensity commercial and office. The intent is to utilize and improve existing structures, encourage infill development and co-exist with surrounding single-family residential uses for convenience services and a transition between automobile uses and the core of the Pueblo Viejo. Uses would include professional offices, small retail establishments, bed and breakfasts, short-term rentals, and similar uses to the downtown area, except at a lower level of intensity that limits deliveries, on-site parking and impacts to existing residential land uses. The area would serve to transition commercial development from that along Grapefruit Boulevard to the core of the Pueblo Viejo around Veteran’s Park and City Hall.

GB-PV – Grapefruit Boulevard Pueblo Viejo Zone: This zone encourages future mixed-use development but recognizes the need to support existing uses previously allowed under the C-G General Commercial Use and M-S Manufacturing Service zones and allow for an orderly transition to higher intensity development, defined as the Downtown Center in the City’s General Plan. While encouraging pedestrian-friendly connections and safe multi-modal access, this zone recognizes current automobile-oriented uses, but allows for flexibility in reducing parking requirements, maintaining existing historical



automobile-oriented architectural character along Grapefruit Boulevard. It allows for retrofitting existing development as the market-demands call for new, innovative, and/or higher intensity development styles.

Design Guidelines

The Pueblo Viejo Revitalization Plan laid the foundation for design guidelines. The proposed Pueblo Viejo Design Guidelines were developed through public engagement and are intended to provide all users with a clear understanding of the City's expectations for the planning and design of properties in the Pueblo Viejo District. The guidelines explain the design intent of the Revitalization Plan and to ensure that new development and reuse of existing buildings will create a new, dynamic, harmonious, and unique downtown of which all citizens can be proud.

The Pueblo Viejo district is divided into four distinct subareas: Sixth Street subarea, Grapefruit Boulevard, Cesar Chavez Street, and the Transition Area surrounding Veterans' Park. The Design Guidelines establish the character, architectural style, massing, materials, and colors for architecture in each of these subareas. The Design Guidelines also focus on signage and public realm, including streetscape and landscaping.

A mix of architectural styles and details can create an authentic and timeless downtown. Four primary architectural styles are explicitly allowed- Spanish Colonial Revival, Mission Revival, Neoclassical, Postmodern/Architectural Innovation. The intent is to preserve the architectural heritage of the Pueblo Viejo district and encourage its eclectic nature. The style guidelines apply to all subareas. However, specific subarea guidelines help distinguish the character from one subarea to another.

Sixth Street Subarea: The Sixth Street subarea is envisioned as a walkable "main street" environment with higher densities and multi-story structures with a mix of both horizontal and vertical uses. However, for the density to not be overwhelming for a pedestrian, the building architecture guidelines focus on the human scale. This is achieved by incorporating significant details in the buildings' architecture, especially on the ground floor. Businesses should have high-quality pedestrian-scale frontages to offer a rewarding visual experience to the people passing by.

Grapefruit Boulevard Subarea: The Grapefruit Boulevard subarea is envisioned to be automobile-focused but also a walkable and bikeable environment. The corridor caters to freestanding structures with their parking lots and used for commercial, light industrial, and automobile-oriented uses.

Cesar Chavez Street Subarea: Cesar Chavez Street Subarea is a commercial street with mainly auto-oriented franchise stores. The Design Guidelines for this area are similar to those for Grapefruit Boulevard. They focus on the improvement of building façades and the implementation of complete and walkable streets.

Transition Area: The Transition area refers to the parcels surrounding Veterans' Park. The area is envisioned to serve as a transition between residential and commercial use, using the existing residential structures and maintaining similar density and massing.



Signage: The signage guidelines provide a clear understanding of the types of signage, materials, illumination, and other signage characteristics that are encouraged in the district. They also guide the type and quality of art installation and murals that are desirable in the area. Public realm guidelines provided conceptual street sections for each of the major commercial streets. These are included in the document primarily to guide the City staff. Landscape guidelines offer a selection of trees and shrubs by the street.

The guideline package is designed to supplement the Zoning Code, propose a coordinated and consistent design review process, and promote a more thoughtful and holistic approach to the development of the Pueblo Viejo district.

Parking Recommendations

The Pueblo Viejo district is poised to witness higher-intensity development as the General Plan is implemented. The General Plan and the Pueblo Viejo Revitalization Plan's lays out the vision of a mixed-use development in the district. The proposed design guidelines and zoning have been crafted to realize this vision.

As a basis for parking regulations and recommendations for public realm design as part of the design guidelines, a parking study was conducted to analyze existing and projected future parking demand, and existing and needed future parking supply. Based on this information, the City can create balanced regulations and potential future public and private partnerships to encourage the proper balance of parking demand and supply. The information and recommendations of this study informed the zoning recommendations and also provide recommendations for other mechanisms for managing parking demand and ensuring that parking supply is not overbuilt or overly burdensome on future development.

The study area for this parking study is a 24-block area that primarily forms the commercial area of the Pueblo Viejo district. The study found that there are ample parking spaces available to cater to the current demand based on current land uses. However, the distribution of parking is not optimal and hence, creates a false demand. Moreover, only on-street parking on these blocks can be used for mitigation since the off-street parking areas of most blocks in the walkshed of the deficient blocks have restricted access.

The medium-built and full-built scenarios were created based on the design guidelines. These scenarios were analyzed for parking. The analysis was based on the premise that any new development would provide all the parking required by the development on-site. Both future scenarios show a need for structured parking, which may be a limiting factor due to the cost of construction and economic viability of the development. The cost of structured parking will be close to \$33 million and will satisfy only one-third of required parking demand in the medium-built scenario and be \$141 million and satisfy three-fourths of required parking demand in the full-built scenario.

Recommendations were made to create efficiencies and reduce the parking required, and thereby reduce the cost of development. This included options such as cooperative parking arrangements, shared centralized structured parking, bicycle parking, time-restricted and paid parking, lower parking ratios, temporary and overflow parking, and unbundled parking.



Some recommendations require revenue sharing, revenue generation, and mechanisms for sharing the cost of infrastructure development between property owners and/or the City. State law allows for various districts to help finance and operate infrastructure such as shared parking. Different sources of financing and organizational mechanisms were also suggested in the study to implement these recommendations.

Public Engagement Summary

Public engagement for this plan included a site tour, stakeholder interviews, a technical charrette with City staff, and a study session with the Planning Commission. During these activities, the project team gathered feedback from the public, interested stakeholders and property owners, City staff, and elected and appointed officials. The summary and findings are provided in the **Public Engagement Summary** section of the report.

Stakeholder Interviews: Six interviews were held with a variety of community stakeholders. The conversations focused on the opportunities, issues, and constraints that the participants saw in the Pueblo Viejo district. Issues such as lack of employment and affordable housing, which affect the City of Coachella were also discussed. Overall, the stakeholders agreed that with an increase in programming, service-oriented land uses, and more diverse residential units, the Pueblo Viejo district could become a special and unique place.

Technical Charrette: City staff from various departments participated in the technical charrette. It included a discussion of the general project background and a conversation to define mixed-use and gain feedback from City staff regarding the appropriateness of mixed-use development in the Pueblo Viejo district. The City staff in attendance expressed support for this type of flexibility in mixed-use. They also agreed that there is a need to reprogram certain areas in the district and improve the facades and curb appeal of many businesses. The summary of the charrette can be found in **Public Engagement Summary** at the end of the report.

Visual Preference Survey: The Pueblo Viejo Implementation Strategy Visual Preference Survey was conducted at the study session of the City of Coachella Planning Commission. The design features were presented in 19 categories: 1) architectural styles; 2) pavement types; 3) on-street parking; 4) light poles; 5) luminaires; 6) traffic calming; 7) bicycle facilities; 8) sidewalk pavement; 9) bus shelters; 10) sidewalk lighting; 11) furnishing materials; 12) bike racks; 13) landscaping; 14) outdoor dining; 15) gateways; 16) water features; 17) public art; 18) business signage; and 19) shade structures. The results of the survey can be found in **Appendix - Visual Preference Survey Summary** at the end of the report.

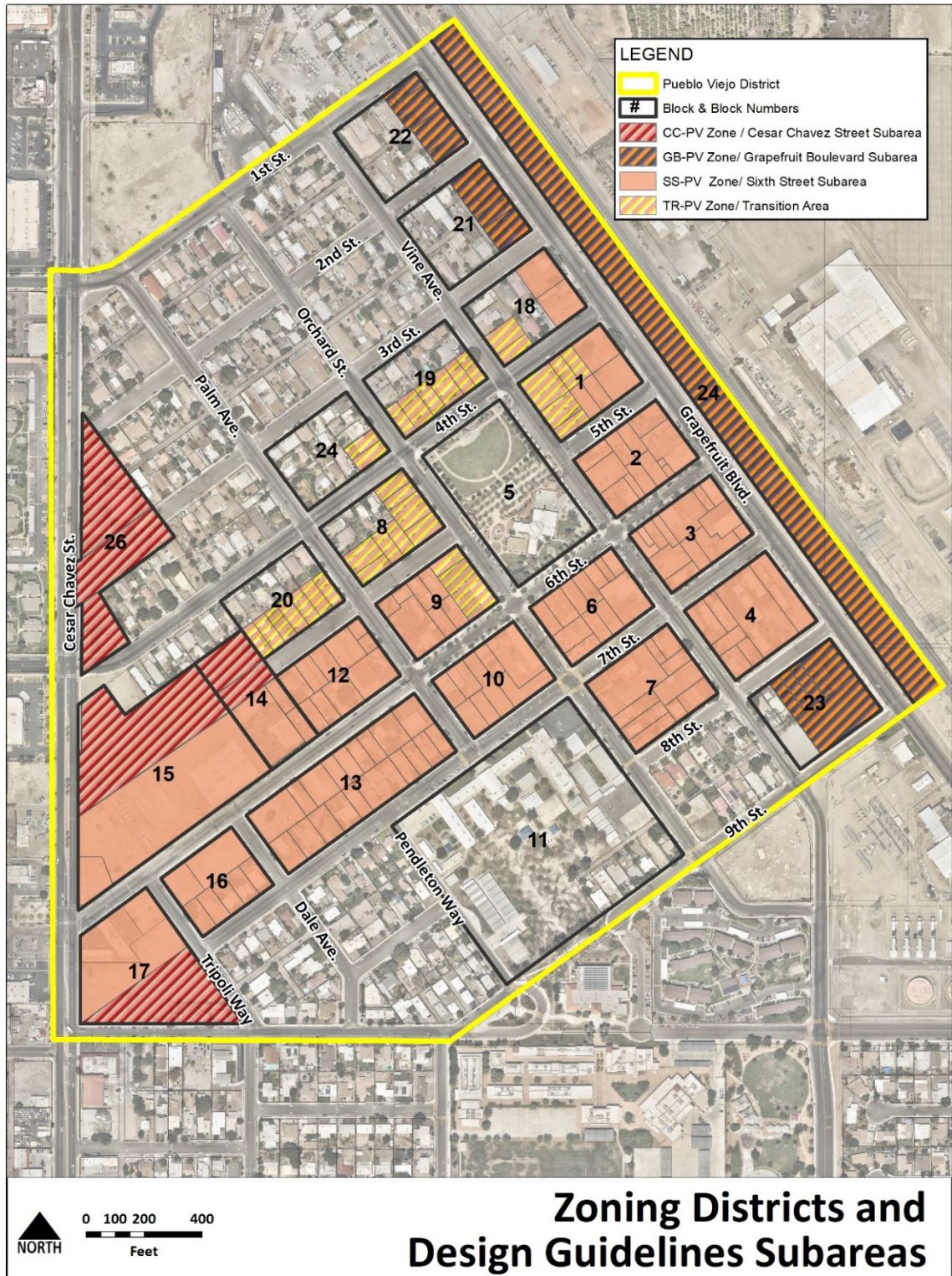


STUDY REPORT

ZONING ORDINANCE (DRAFT)



SS-PV – SIXTH STREET PUEBLO VIEJO ZONE





17.29.010 Intent and purpose.

This zone is intended to provide for and encourage the orderly development of the core of the Pueblo Viejo District. the core of the Pueblo Viejo District is envisioned, by the city’s General Plan and Pueblo Viejo Vision Plan, as a higher-density mixed-use (either vertical and/or horizontal) downtown area that provides for a wide variety of multi-family residential housing, office, and retail uses. This zone encourages a high-energy pedestrian-friendly environment with street-facing buildings, maintained building lines, a variation of architectural character, and sidewalk and public spaces to provide for gathering spaces and promote outdoor activities including dining, people watching, public art and passive recreation. Motorized vehicle access would minimize impacts on a highly connected pedestrian environment with alley and rear entry access, parking in structures or internal to blocks, and services located behind buildings, in alleyways or rear parking areas. The use of the public realm is encouraged with on-street dining and temporary uses encouraged on sidewalks and adjoining setbacks for these purposes.

17.29.020 Summary of District Regulations

The following summary table shall not be a substitute for the specific language of each part of this section. Any language and interpretation of that language shall supersede the information contained within this summary table.

Permitted / Conditional / Temporary Use Summary	Multi-family Residential, Mixed-Use, Commercial, Retail, Surface Parking, Parking Structures (see 17.29.030 Uses)
Architectural Review Required	Yes
Applicable Architectural Guidelines	Pueblo Viejo Design Guidelines
General Building Types Allowed	Multi-story Residential, Mixed-use (vertical and horizontally oriented) (See 17.29.040)
Minimum Building Height	Three (3) stories or forty (40) feet
Front Setback	Zero (0) to ten (10) feet from property line
Upper Floor Setbacks	Ten (10) foot front setback for stories exceeding four (4) and an additional five (5) foot setback for stories oriented towards 5 th or 7 th streets
Allowed Residential Densities	Minimum twenty (20) D.U. per acre/Maximum sixty-five (65) D.U. per acre
Allowed office/Commercial/Retail Intensities	Minimum FAR 0.5. Maximum FAR 3.0.
Maximum Block Size	Five hundred (500) linear feet on 5 th 6 th or 7 th streets
Minimum Lot Area	Five Thousand (5,000) square feet
Minimum Lot Depth	None
Minimum Lot Frontage	None
Minimum Lot Frontage Coverage	One hundred (100) percent
Encroachment for Colonnades or Arcades Allowed	Yes. Subject to encroachment permit. Maximum encroachment six (6) foot minimum clearance to curb line



Housing Affordability Requirements	Yes
Public Open Space Requirements	Ten (10) percent for developments over ½ acre
Minimum Residential Exclusive Use Common Space	One hundred and fifty (150) square feet
Minimum Residential Common Space	10 square feet per unit or 1000 square feet, whichever is greater

17.29.030 Uses.

Uses are classified according to the 2017 North American Industry Classification System (NAICS). The NAICS use most associated with the actual use (not necessarily the uses primary NAICS code) shall be used to determine use. Additional uses are defined by the City of Coachella and located within the definitions under Chapter 17.06 of this ordinance. the Planning Director or their designee may re-classify uses based upon the most appropriate use under this section based upon this section’s intent and purpose. Appeals of the Planning Director’s decision may be made to the Planning Commission.

A. Permitted Uses

1. Activities Related to Real Estate
2. Amusement Arcades (Indoor)
3. Animal Hospitals and Veterinary Services (No Outdoor Facilities)
4. Antique Dealers and Shop
5. Art and Architecture Supply Shops and Studios
6. Art Dealers
7. Beer and/or Winemaking Supply Retail Stores
8. Bowling Centers
9. Business Professional, Labor, Political and Similar organizations
10. Business Service Centers
11. Candle Shops
12. Civic and Social organizations
13. Clothing and Clothing Accessories Stores
14. Collectors Shops
15. Computer Systems Design and Related Services
16. Consignment Shops
17. Cosmetics, Beauty Supplies and Perfume Stores
18. Dance Halls
19. Diet and Weight Reducing Centers
20. Drug Stores or Retail Pharmacies
21. Dry-Cleaning and Laundry Services (Except Linen and Uniform Supply and Industrial Launderers)
22. Educational Services
23. Electronics and Appliance Stores
24. Employment Services
25. Finance and Insurance Retail Establishments (No Distribution and/or Telephone Call Centers)



26. Fitness and Recreational Sports Centers
27. Flag and Banner Shops
28. Florist Shops
29. Footwear and Leather Goods Repair
30. Fruit and Vegetable Markets
31. Grantmaking and Giving Services
32. Hair, Nail and Skin Care Services (Including Barber Shops and Beauty Salons)
33. Hardware Stores
34. Health and Personal Care Stores
35. Home Furnishing Stores
36. Home Security Equipment Stores
37. Hotels, Resort Hotels and Motels
38. Household Furniture or Stores
39. Independent Artists, Writers and Performers
40. Information
41. Jewelry Repair Shops
42. Jewelry, Luggage and Leather Goods Stores
43. Libraries and Archives
44. Meat, Fish and Seafood Markets (No On-Site Slaughtering)
45. Office Administrative Services
46. Office Machinery and Equipment Rental and Leasing
47. Office Supplies, Stationary and Gift Stores
48. Office, Professional
49. Offices of Agents and Managers of Artists, Athletes, Entertainers and Other Public Figures
50. Offices of Business Support Services (Except Collection Agencies, Repossession Services, Telephone Answering Services and Telemarketing Bureaus and Other Contact Centers)
51. Offices of Construction Industries
52. Offices of Dentists
53. Offices of Finance and Insurance
54. Offices of Lessors of Nonresidential Buildings (Except Mini-warehouses)
55. Offices of Lessors of Residential Buildings and Dwellings
56. Offices of Management of Companies and Enterprises
57. Offices of Motion Picture and Video Industries
58. Offices of Newspaper, Periodical, Book and Directory Publishers
59. Offices of Other Health Care Practitioners
60. Offices of Physicians
61. Offices of Professional, Scientific and Technical Services (Other Than Testing Laboratories and Scientific Research and Development Services)
62. Offices of Promoters of Performing Arts, Sports and Similar Events
63. Offices of Real Estate Agents and Brokers
64. Offices of Software Publishers
65. Optical Goods Stores
66. Other Building Materials Dealers Retail Stores (Except Lumber Stores, Fencing Dealers, Garage Door Dealers and Prefabricated Building Dealers – No Construction or Trade Services Permitted)



- 67. Other Personal and Household Goods Repair and Maintenance
- 68. Other Personal Care Services Including Day Spas, Depilatory or Electrolysis Salons, Salons, Saunas, Ear Piercing Services, Steam or Turkish Baths, Hair Replacement or Weaving Services, Tanning Salons, Massage Parlors, Tattoo Parlors or Permanent Makeup Salons
- 69. Paint and Wallpaper Stores
- 70. Pet and Pet Supply Stores
- 71. Pet Care Services (Except Animal Shelters, Outdoor Boarding Services, Outdoor Catteries, Dog Pounds, Guard Dog Training Services and Outdoor Kennels)
- 72. Photographic Services and Photofinishing
- 73. Recreational or Youth Sports Teams
- 74. Religious Goods Store
- 75. Residential, Multi-Family
- 76. Restaurants, Delicatessens, and Other Eating Establishments (No Drive-Thru Permitted)
- 77. Retail Bakeries
- 78. Reupholstery and Furniture Repair
- 79. Social Advocacy organizations
- 80. Sound Recording Industries
- 81. Specialty Food Stores (No On-Site Slaughtering)
- 82. Sporting Goods, Hobby, Musical Instrument, toy and Book Stores
- 83. Supermarkets and Other Grocery Stores
- 84. Tailor and Alterations Stores
- 85. Thrift Shops
- 86. tourist Information Centers
- 87. Travel Arrangement and Reservation Services
- 88. Trophy (Including Awards and Plaques) Shops
- 89. Vocational Rehabilitation Services

B. Conditional Uses

The following uses may be allowed by administrative approval of the Planning Director or their designee based upon their overall impact and compatibility with the intent and purpose of the zoning district. Conditional uses are subject to Chapter 17.74 of this ordinance.

- 1. Adult Novelty Stores
- 2. Alcoholic Beverage Sales (For Off-Premise Consumption)
- 3. Appliance Repair and Maintenance
- 4. Bed and Breakfast Inns
- 5. Billiard and Pool Halls
- 6. Cannabis Retail Establishment
- 7. Caterers
- 8. Combined Live/Work Dwellings
- 9. Community Gardens
- 10. Child Day Care Centers
- 11. Drinking Places



12. Emergency Services Stations (Including Police and Fire)
13. Funeral Homes and Funeral Services
14. Interurban and Rural Bus Transportation
15. Investigation and Security Services (Except Locksmiths)
16. Jewelry and Silverware Manufacturing
17. Laboratory, Research
18. Laboratory, Support
19. Locksmiths
20. Medical and Diagnostic Laboratories
21. Microbreweries, Wine Tasting Facilities and Micro-Distilleries
22. Museums, Historical Sites, and Similar Institutions (Except Zoos)
23. Outpatient Care Centers
24. Parking Structures
25. Performing Arts Companies
26. Post Services
27. Public Utility Substations and Storage Buildings
28. Recycling Center, Neighborhood Only
29. Religious Institution
30. Scenic and Sightseeing Transportation, Land
31. Scientific Research and Development Services
32. Telephone Exchanges and Switching Equipment
33. Testing Laboratories
34. theaters and Auditoriums
35. tobacco, E-Cigarette, Vapor Accessories, Smoking Accessories or Hookah Shops and Lounges
36. Urban Transit Systems
37. Water and Gas Company Service Facilities

C. Accessory Uses

The following uses shall constitute accessory uses and are subject to administrative approval of the Planning Director or their designee. Accessory uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Arcades (Shade Structures)
2. Canopies
3. Carports
4. Community Gardens
5. Nurseries, Garden Center and Farm Supply Stores
6. Outdoor Dining Areas
7. Parking Lots
8. Tree Nut Farming



D. Temporary Uses

The following uses constitute temporary uses and are subject to administrative approval of the Planning Director or their designee. Temporary uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Flea Markets/Swap Meets, Temporary Location, Direct-Selling
2. Outdoor Dining Areas
3. Outdoor Sales Areas

E. Prohibited Uses

The following uses are expressly prohibited within this zone and are considered incompatible with the intent and use of this zone. An applicant may file an administrative appeal with the Planning Director or their designee for re-classification.

1. Animal Hospitals and Veterinary Services (with Outdoor Facilities)
2. Archery or Shooting Ranges
3. New Car Dealers
4. Used Car Dealers
5. Automobile Parts and Accessories Stores
6. Automobile Service Station
7. Automotive Equipment Rental and Leasing
8. Automotive Repair and Maintenance
9. Blind and Shade Manufacturing
10. Blood and organ Banks
11. Campgrounds
12. Cemeteries and Crematories
13. Chocolate Confectionary Manufacturing
14. Coffee and Tea Manufacturing
15. Commercial and Industrial Machinery and Equipment (Except Automotive and Electronic) Repair and Maintenance
16. Commercial and Industrial Machinery and Equipment Rental and Leasing (Except Construction, Mining, Forestry Machinery and Equipment Rental and Leasing, office Machinery and Equipment Rental and Leasing, and Commercial Air, Rail and Water Transportation Equipment Rental and Leasing)
17. Communications and Microwave Installations
18. Communications Equipment Manufacturing
19. Community Food and Housing, and Emergency and Other Relief Services
20. Computer and Peripheral Equipment Manufacturing
21. Consumer Goods Rentals
22. Continuing Care Retirement Communities and Assisted Living Facilities For the Elderly
23. Convenience Stores with Gas Stations
24. Converted Paper Product Manufacturing



25. Dairy Product Manufacturing
26. Distribution Centers
27. Drive-Thru Windows
28. Electric Lighting Equipment Manufacturing
29. Electronic and Precision Equipment Repair and Maintenance
30. Emergency Shelters
31. Equipment Sales, Rental and Storage
32. Facilities Support Services
33. Food Service Contractors
34. Footwear Manufacturing
35. Freight Transportation Arrangement
36. Refrigerated Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
37. Fuel Service Stations
38. Gambling Industries
39. General Medical and Surgical Hospitals
40. General Rental Centers
41. General Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
42. Glass and Glass Product Manufacturing
43. Golf Courses and Country Clubs
44. Grain and Bakery Products
45. Home and Garden Equipment Repair and Maintenance
46. Home Health Care Services
47. Household and Institutional Furniture and Kitchen Cabinet Manufacturing
48. Household Appliance Manufacturing
49. Leather and Allied Product Manufacturing (Except Footwear and Leather and Hide Tanning and Finishing)
50. Lessors of Mini-warehouses and Self-Storage Units
51. Manufacturing of Reproducing Magnetic and Optical Media
52. Mattress Manufacturing
53. Medical Equipment and Supplies Manufacturing
54. Miniature Golf Courses
55. Mobile Food Services
56. Mobile Homes, provided they Are Kept Mobile and Licensed Pursuant to State Law, When Used For Construction offices and Caretaker's Quarters On Construction Sites For the Duration of A Valid Building Permit
57. Motorcycle and ATV Dealers
58. Musical Instrument Manufacturing
59. Navigational, Measuring, Electro-Medical and Control Instruments Manufacturing
60. Non-Chocolate Confectionery Manufacturing
61. Nursing Care Facilities
62. office Furniture (Including Fixtures) Manufacturing
63. office Machinery Equipment Rental and Leasing
64. office Supplies (Except Paper) Manufacturing
65. offices of Lessors of Other Real Estate Property



- 66. Optical Instrument and Lens Manufacturing
- 67. Other Ambulatory Health Care Services
- 68. Other Residential Care Facilities
- 69. Other Support Services
- 70. Parcel Delivery Services
- 71. Photographic and Photocopying Equipment Manufacturing
- 72. Printing
- 73. Printing Machinery Equipment Manufacturing
- 74. Psychiatric and Substance Abuse Hospitals
- 75. Remediation and Other Waste Management Services
- 76. Residential Intellectual and Developmental Disability, Mental Health, and Substance Abuse Facilities
- 77. Residential, Accessory Dwelling Unit
- 78. Residential, Proprietor/Caretaker Dwelling Unit
- 79. Residential, Single-Family
- 80. Riding Stables
- 81. Rooming and Boarding Houses, Dormitories and Worker's Camps
- 82. Scale and Balance Manufacturing
- 83. Semiconductor and Other Electronic Component Manufacturing
- 84. Services to Buildings and Dwellings
- 85. Sign Manufacturing
- 86. Signs, On-Site Advertising
- 87. Soft Drink and Ice Manufacturing
- 88. Specialty Hospitals (Except Psychiatric and Substance Abuse)
- 89. Spectator Sports
- 90. Sporting and Athletic Goods Manufacturing
- 91. Telephone Call Center
- 92. Waste Collection
- 93. Waste Treatment and Disposal

17.29.040 Property Development Standards.

A. Architectural Guidelines

- 1. All development within this zoning district is subject to architectural review as set forth in Chapter 17.72 of this ordinance.
- 2. All property within this zone shall be subject to the Pueblo Viejo Design Guidelines as the governing architectural guidelines for the zone.

B. Mixed-Use and Building Type

- 1. Buildings may be oriented initially in integrated-horizontal development or vertical mixed-use patterns depending upon current market demands.
- 2. Residential uses may be allowed on the first floor of a building provided all of the following are met:



- a. the first-floor height meets requirement (D)(2) of this part;
 - b. the first floor must be reconfigurable to accommodate a transition to non-residential uses in the future;
 - c. Residential first floor development floor height must be elevated at least three (3) feet above the sidewalk plane to provide for privacy for residents.
 - d. No ownership tenure is allowed for first-floor residential use.
- 3. The lower floor of proposed parking garages that face or partially face Sixth Street must include usable commercial space and are required to follow the architectural guidelines for parking garages.
 - 4. The Planning Director or their designee may waive the requirements of part (2) of this section except for part (2)(c) of this section.
 - 5. Vertical mixed use buildings or integrated horizontal developments may reduce parking requirements by forty (40) percent.
 - 6. Integrated horizontal mixed-use development must include a mix of at least two (2) uses including one use being residential.

C. Building orientation

- 1. Buildings shall be oriented towards the street and engage the public realm.
- 2. Corner lots shall be oriented towards both streets with building entrances encouraged to be oriented towards the corner.
- 3. Non-residential entrances must be located at the level of the sidewalk plane.

D. Height, Massing and Articulation

- 1. Building height must be a minimum of three (3) usable stories or forty (40) feet, whichever is greater.
- 2. The first floor must be a minimum of fifteen (15) feet in height to accommodate modern commercial and retail activities, even if the initial use is residential in nature.
- 3. All buildings exceeding four stories in height shall require a minimum ten (10) foot additional front setback (including corner lots) for stories above the fourth story to reduce overall massing and impact on the street. An additional five (5) foot setback for every additional story shall be required on building facades facing 5th or 7th street.
- 4. All buildings must include articulation a minimum of every fifty (50) feet on all facades to break the vertical plane and provide visual interest for pedestrians.
- 5. All buildings must utilize four-sided architecture in which all facades must receive architectural treatment and meet all requirements of this section and the design guidelines.



E. Density and Lot Size Requirements

1. Development should comply with the allowed development intensities of the General Plan, which include:
 - a. Residential: A minimum of twenty (20) and maximum of sixty-five (65) dwelling units per acre
 - b. office/Commercial/Retail: Floor Area Ratio of a minimum of 0.5 FAR to a maximum of 3.0 FAR.
2. Minimum Lot Area: Five thousand (5,000) square feet.
3. Minimum Lot Depth: None.
4. Minimum Lot Frontage: One hundred (100) percent.
5. All development shall maintain existing block sizes. New development without existing streets shall mimic prevailing block dimensions with maximum block lengths no longer than five hundred (500) feet.
6. The Planning Director or their designee may approve a waiver in minimum lot frontage for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

F. Yard Requirements

1. FRONT YARD: A minimum build-to line equal to the front property line facing the street is required. For corner lots, the minimum size build-to line is equal to the front property line facing the street and the side property line facing the adjoining street. A setback of up to ten (10) feet from the build-to line is allowed for accessory uses such as outdoor dining and other public spaces. the setback must blend with the public realm.
2. SIDE YARD: the property line shall serve as the minimum build-to line. A setback of up to twenty (20) feet may be utilized anywhere upon the property, including within the side yard, for paseos and pedestrian passage-ways that facilitate passage through the block.
3. REAR YARD: there are no rear yard requirements.
4. Colonnades and/or arcades may occur forward of the build-to-line and may encroach upon the rights of way, if an encroachment permit is approved by the Department of Public Works. Colonnades and/or arcades that encroach within the rights of way must not impede pedestrian traffic and be setback a minimum of six (6) feet from the curb line.
5. The Planning Director or their designee may approve a waiver in front and/or side yard requirements for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

G. Housing Affordability

1. A minimum of ten (10) percent of all residential units must be priced for low and/or moderate-income residents. Units should be disbursed throughout the structure and must proportionally reflect the size of market rate units. While



trim detail may vary, the overall quality of building materials may not depart from those used within market rate units within the same structure.

H. Off-street Parking, Loading and Circulation

1. A circulation plan must be submitted for development that includes more than one (1) building or more than (1) parking facility. The circulation plan must address pedestrian, vehicle, transit (if required), and bicycle circulation, ingress/egress and parking and meet the requirements of this section. A professionally conducted parking study is required for any requests for parking reductions provided for in subsection (H)(8) of this section.
2. Off-street parking and loading facilities shall be provided in accordance with the provision of Section 17.54.010 of this ordinance except where the following provisions shall supersede those of 17.54.010:
 - a. Horizontal or vertical mixed-use development shall require a parking space ratio of three (3) spaces for 1000 square feet of gross floor area.
 - b. For single-use development that does not meet the requirements for mixed-use development, the following minimum parking requirements shall apply:
 - i. Multi-family residential: 1.25 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 0.60 spaces per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: 3.75 spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: twelve (12) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: eleven (11) spaces per 1,000 square feet of gross floor area.
 - c. For single-use development that does not meet the requirements for mixed-use development, the maximum parking requirements shall apply:
 - i. Multi-family residential: 1.33 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 1 space per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: four (4) spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: fifteen (15) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: fifteen (15) spaces per 1,000 square feet of gross floor area.
3. No development that was not approved or in existence prior to July 1st, 2019 shall be allowed to locate off-street parking lots adjacent to the primary street.



4. On-street parking that is within five hundred (500) feet of the main entrance(s) of a development may be utilized to satisfy the requirement for off-street parking.
5. Shared curb-cuts are required for all new development. All curb-cuts are subject to City approval. An internal circulation plan with provisions for shared curb-cuts and internal circulation with neighboring properties is required.
6. New development must consider existing development and provide connections to existing development within each block to allow for internal block circulation.
7. All internal vehicle circulation roads, except for those leading to non-public areas or loading access, must include sidewalks with compliant ADA facilities and landscaping. Pedestrian facilities must connect all building entrances, retail entrances and residential entrances. Safe and adequate pedestrian connectivity within the development and connections to adjacent development and existing pedestrian facilities is required.
8. The parking requirements of this section may be reduced, with the approval of the Planning Commission, if any of the following provisions are provided for in the circulation plan:
 - a. The proposed development is located within a Business Improvement District or a special district that institutes shared-parking, timed parking restrictions, and/or paid parking.
 - b. The proposed development incorporates paid parking into the development.
 - c. A cooperative use agreement is executed with another property owner to provide for a portion of the required parking of the proposed development.
 - d. A payment-in-lieu agreement is executed with the City or another entity responsible for parking management to defray the cost for accommodating additional demand generated by the proposed development.
 - e. Land set-aside or structural design of proposed parking surface lots or structured parking is provided to allow for expansion to accommodate additional parking supply when demand exceeds approved supply. The parking study must justify current supply, based upon current demand, and provide for future demand thresholds that would trigger the expansion of additional supply. Any future demand thresholds and supply expansion provisions shall become a condition of approval and continued use of the property.

I. Screening and Fencing

1. Where this zoning district abuts upon any residential zone, there shall be provided screening not less than six (6) feet or more than eight (8) feet in height on the zoning boundary line. Said screening shall be reduced to forty-two (42) inches in height within a setback area adjacent to a street or highway.



2. Outdoor dining areas shall be separated from the remainder of the sidewalk with the use of appropriate use of planters, fences or other barriers as approved by the design guidelines.

J. Public Art

1. A minimum of one (1) percent of total construction costs must be either invested in public art, visible to the public realm, or provided as payment-in-lieu to the City to fund larger public art projects within the Pueblo Viejo.
2. The applicant shall furnish a performance bond equivalent to the estimated public art investment or payment-in-lieu.

K. Public Open Space

1. Development over one-half (1/2) acre or more in gross land area, must reserve a minimum of ten (10) percent of the development for public open space accessible from the public realm in the form of a park, pocket park, plaza, paseo, and/or other public gathering space.
2. The applicant may make a payment to the City in lieu of public space reservation for larger park or plaza development within the Pueblo Viejo upon approval of the Planning Director or their designee.

L. Multi-Family Residential Dwelling Unit Size and Common Space Requirements

1. A minimum of one-hundred and fifty (150) square feet of uninterrupted exclusive use common space shall be available to each unit. This may be in outdoor living areas, balconies and/or decks.
2. Decks shall be allowed on rooftops providing they are appropriately screened with architectural features such as a parapet.
3. Ten (10) square feet per unit, or a minimum of one-thousand (1000) square feet, whichever is greater, shall be required for common space for a common recreation and/or leisure area.
4. All common spaces shall be screened from the street by landscaping and/or decorative fencing.
5. The following minimum dwelling unit sizes shall be required:
 - a. Micro-Unit: Three Hundred and Fifty (350) square feet
 - b. Efficiency: Six Hundred (600) square feet
 - c. One-bedroom: Seven-hundred and Fifty (750) square feet
 - d. Each additional bedroom beyond one bedroom: An additional one-hundred and fifty (150) square feet per dwelling unit is required in addition to the minimum requirement of part (C) above.



M. Development Standards Applicable to Specific Uses

1. Outdoor Dining Areas
 - a. Required parking shall be provided for outdoor seating areas, except for common outdoor seating areas not attributable to a single establishment.
 - b. Common outdoor seating areas may be provided as part of required open space areas with provisions for management and maintenance of the area.
 - c. Outdoor seating areas that are within the public rights-of-way shall not be used for entertainment. The Planning Director or their designee may waive the provision on a temporary or permanent basis for entertainment in consultation with the City Engineer.
 - d. Sound amplification devices, such as speakers, shall be limited to devices that are necessary to provide low-level background music. Noise levels shall comply with Chapter 7.04 of the City of Coachella Municipal Code. The Planning Director or their designee may waive this provision, except for Chapter 7.04 compliance, on a temporary or permanent basis in conjunction with a waiver granted in part (c) above.
 - e. Outdoor dining areas are subject to all permit approvals and the design guidelines and configuration and design must be included on any drawings and application submissions.
 - f. Outdoor dining areas located adjacent to, or within the public rights-of-way must leave a minimum of five (5) feet of sidewalk, open always to accommodate pedestrian traffic.
 - g. All outdoor dining within the public rights-of-way must obtain an encroachment permit from the City Engineering Department.
 - h. Outdoor dining areas that include the sales of alcohol must meet all requirements and regulations of the California Department of Alcoholic Beverage Control.

17.29.050 Existing Uses and Structures at the Time of Adoption of this Section.

A. The provisions of this section shall supersede Section 17.78.010 of this ordinance:

1. All uses, lots, structures and characteristics, except for signage, that were lawful, and in existence, prior to July 1st, 2019 shall remain as legally conforming uses, lots, structures and characteristics with all the previous entitlements intact provided:
 - a. the use, lot, structure and/or characteristics remain otherwise lawful.
 - b. No use, lot, structure and/or characteristics may cease operation for a period greater than one (1) year.



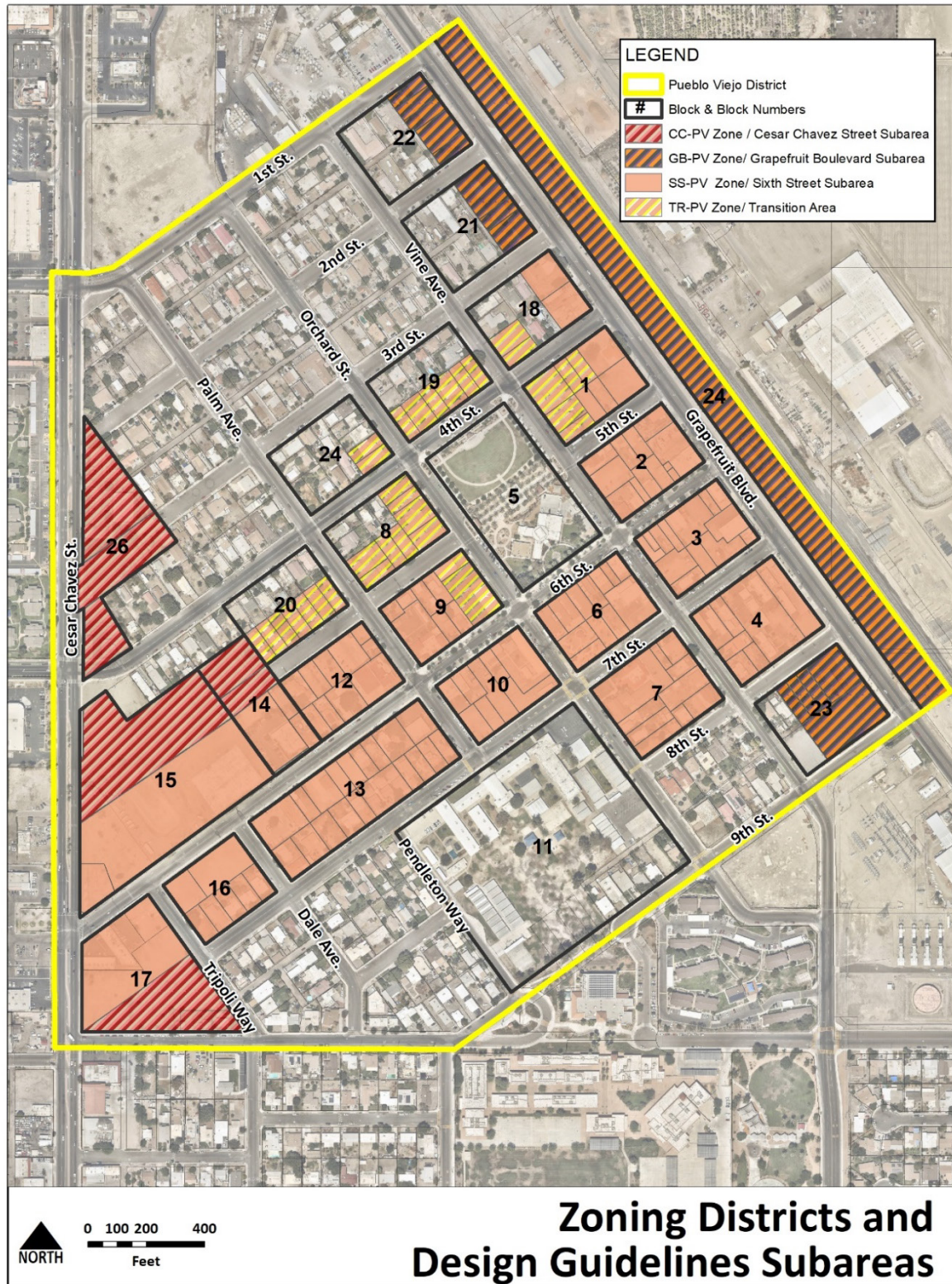
- c. No use, lot, and/or structure may be abandoned for a period greater than one (1) year.
 - d. If the use, lot, structure and/or characteristics fall within a permitted and/or conditional use, as defined in section 17.029.030, the provisions of this section will no longer apply and the previous use, lot, structure and/or characteristics must fully comply with the provisions of this zone thereafter.
2. If the provisions of this section are no longer met, the property is then subject to the provisions of Chapter 17.78 of this ordinance.

B. Provisions for ineligible uses, lots, structures and characteristics and revocation of legal conforming status:

- 1. All existing lawful signage, that no longer is permitted, or meets the standards of this district, shall be subject to Chapter 17.78 of this ordinance.
- 2. All uses, lots, structures and characteristics that were not lawful, and in existence, prior to July 1st, 2019 shall remain illegal non-conforming uses and subject to the provisions of Chapter 17.78 of this ordinance.



GB-PV – GRAPEFRUIT BOULEVARD PUEBLO VIEJO ZONE





17.25.010 Intent and purpose.

This zone is intended to provide for and encourage the orderly development of the areas of the Pueblo Viejo that borders Grapefruit Boulevard and serve as gateways into the district. The Pueblo Viejo is envisioned as a higher-density downtown area that provides for a wide variety of multi-family residential housing, office, and retail uses. Centered around a mixed-use core, this zone encourages future mixed-use development, but recognizes the need to support existing uses previously allowed under the C-G General Commercial Use and M-S Manufacturing Service zones. This zone provides for an orderly transition to higher intensity development, defined as the Downtown Center in the city’s General Plan. While encouraging pedestrian-friendly connections and safe multi-modal access, this zone recognizes current automobile-oriented uses, but allows for flexibility in reducing parking requirements, maintaining existing historical automobile-oriented architectural character along Grapefruit Boulevard and retrofitting existing development as market-demands call for new, innovative and/or higher intensity development styles.

17.25.020 Summary of District Regulations

The following summary table shall not be a substitute for the specific language of each part of this section. Any language and interpretation of that language shall supersede the information contained within this summary table.

Permitted / Conditional / Temporary Use Summary	Multi-family Residential, Mixed-Use, Commercial, Retail, Service, limited Manufacturing, Surface Parking, Parking Structures (see 17.29.030 Uses)
Architectural Review Required	Yes
Applicable Architectural Guidelines	Pueblo Viejo Design Guidelines
General Building Types Allowed	Multi-story Residential, Mixed-Use, Retail, Office
Minimum Building Height	Two (2) stories or thirty-five (35) feet
Front Setback	Zero (0) to ten (10) feet from property line except for where adjoining zoning is residential, then front setback shall be of equal depth to the average of the required front-yards of the adjoining residential zone
Upper Floor Setbacks	Additional five (5) foot setback for each story on buildings adjacent to a single-family residential zone.
Allowed Residential Densities	Minimum twenty (20) D.U. per acre/Maximum sixty-five (65) D.U. per acre
Allowed office/Commercial/Retail Intensities	Minimum FAR 0.5. Maximum FAR 3.0
Maximum Block Size	The integrity of existing blocks shall be maintained
Minimum Lot Area	Five Thousand (5,000) square feet
Minimum Lot Depth	None
Minimum Lot Frontage	None
Minimum Lot Frontage Coverage	One hundred (100) percent.



Encroachment for Colonnades or Arcades Allowed	Yes, except for Grapefruit Boulevard. Subject to encroachment permit. Maximum encroachment six (6) foot minimum clearance to curb line.
Housing Affordability Requirements	Yes
Public Open Space Requirements	Ten (10) percent for developments over ½ acre
Minimum Residential Exclusive Use Common Space	One hundred and fifty (150) square feet
Minimum Residential Common Space	10 square feet per unit or 1000 square feet, whichever is greater.

17.25.030 Uses.

Uses are classified according to the 2017 North American Industry Classification System (NAICS). The NAICS use most associated with the actual use (not necessarily the uses primary NAICS code) shall be used to determine use. Additional uses are defined by the City of Coachella and located within the definitions under Chapter 17.06 of this ordinance. the Planning Director or their designee may re-classify uses based upon the most appropriate use under this section based upon this section’s intent and purpose. Appeals of the Planning Director’s decision may be made to the Planning Commission.

A. Permitted Uses

1. Activities Related to Real Estate
2. Alcoholic Beverage Sales (For Off-Premise Consumption)
3. Amusement Arcades (Indoor)
4. Animal Hospitals and Veterinary Services (No Outdoor Facilities)
5. Antique Dealers and Shop
6. Appliance Repair and Maintenance
7. Art and Architecture Supply Shops and Studios
8. Art Dealers
9. Automobile Parts and Accessories Stores
10. Automobile Service Station
11. Automotive Equipment Rental and Leasing
12. Automotive Repair and Maintenance
13. Beer and/or Winemaking Supply Retail Stores
14. Business Professional, Labor, Political and Similar Organizations
15. Business Service Centers
16. Candle Shops
17. Caterers
18. Civic and Social Organizations
19. Clothing and Clothing Accessories Stores
20. Collectors Shops
21. Combined Live/Work Dwellings
22. Computer Systems Design and Related Services
23. Consignment Shops
24. Consumer Goods Rentals



25. Cosmetics, Beauty Supplies and Perfume Stores
26. Dance Halls
27. Child Day Care Centers
28. Diet and Weight Reducing Centers
29. Drug Stores or Retail Pharmacies
30. Dry-Cleaning and Laundry Services (Except Linen and Uniform Supply and Industrial Launderers)
31. Educational Services
32. Electronic and Precision Equipment Repair and Maintenance
33. Electronics and Appliance Stores
34. Emergency Services Stations (Including Police and Fire)
35. Employment Services
36. Finance and Insurance Retail Establishments (No Distribution and/or Telephone Call Centers)
37. Fitness and Recreational Sports Centers
38. Flag and Banner Shops
39. Florist Shops
40. Food Service Contractors
41. Footwear and Leather Goods Repair
42. Fruit and Vegetable Markets
43. Funeral Homes and Funeral Services
44. General Rental Centers
45. Grantmaking and Giving Services
46. Hair, Nail and Skin Care Services (Including Barber Shops and Beauty Salons)
47. Hardware Stores
48. Health and Personal Care Stores
49. Home Furnishing Stores
50. Home Health Care Services
51. Home Security Equipment Stores
52. Hotels, Resort Hotels and Motels
53. Household Furniture or Stores
54. Independent Artists, Writers and Performers
55. Information
56. Interurban and Rural Bus Transportation
57. Investigation and Security Services (Except Locksmiths)
58. Jewelry and Silverware Manufacturing
59. Jewelry Repair Shops
60. Jewelry, Luggage and Leather Goods Stores
61. Laboratory, Research
62. Laboratory, Support
63. Libraries and Archives
64. Locksmiths
65. Meat, Fish and Seafood Markets (No On-Site Slaughtering)
66. Medical and Diagnostic Laboratories
67. Motorcycle and ATV Dealers
68. Office Administrative Services
69. Office Machinery and Equipment Rental and Leasing



- 70. Office Machinery Equipment Rental and Leasing
- 71. Office Supplies, Stationary and Gift Stores
- 72. Office, Professional
- 73. Offices of Lessors of Other Real Estate Property
- 74. Offices of Agents and Managers of Artists, Athletes, Entertainers and Other Public Figures
- 75. Offices of Business Support Services (Except Collection Agencies, Repossession Services, Telephone Answering Services and Telemarketing Bureaus and Other Contact Centers)
- 76. Offices of Construction Industries
- 77. Offices of Dentists
- 78. Offices of Finance and Insurance
- 79. Offices of Lessors of Nonresidential Buildings (Except Mini-warehouses)
- 80. Offices of Lessors of Residential Buildings and Dwellings
- 81. Offices of Management of Companies and Enterprises
- 82. Offices of Motion Picture and Video Industries
- 83. Offices of Newspaper, Periodical, Book and Directory Publishers
- 84. Offices of Other Health Care Practitioners
- 85. Offices of Physicians
- 86. Offices of Professional, Scientific and Technical Services (Other Than Testing Laboratories and Scientific Research and Development Services)
- 87. Offices of Promoters of Performing Arts, Sports and Similar Events
- 88. Offices of Real Estate Agents and Brokers
- 89. Offices of Software Publishers
- 90. Optical Goods Stores
- 91. Other Building Materials Dealers Retail Stores (Except Lumber Stores, Fencing Dealers, Garage Door Dealers and Prefabricated Building Dealers – No Construction or Trade Services Permitted)
- 92. Other Personal and Household Goods Repair and Maintenance
- 93. Other Personal Care Services Including Day Spas, Depilatory or Electrolysis Salons, Salons, Saunas, Ear Piercing Services, Steam or Turkish Baths, Hair Replacement or Weaving Services, Tanning Salons, Massage Parlors, Tattoo Parlors or Permanent Makeup Salons
- 94. Paint and Wallpaper Stores
- 95. Parcel Delivery Services
- 96. Pet and Pet Supply Stores
- 97. Pet Care Services (Except Animal Shelters, Outdoor Boarding Services, Outdoor Catteries, Dog Pounds, Guard Dog Training Services and Outdoor Kennels)
- 98. Photographic Services and Photofinishing
- 99. Post Services
- 100. Recreational or Youth Sports Teams
- 101. Religious Goods Store
- 102. Religious Institution
- 103. Residential, Multi-Family
- 104. Restaurants, Delicatessens, and Other Eating Establishments (No Drive-Thru Permitted)
- 105. Retail Bakeries



- 106. Reupholstery and Furniture Repair
- 107. Social Advocacy Organizations
- 108. Sound Recording Industries
- 109. Specialty Food Stores (No On-Site Slaughtering)
- 110. Sporting Goods, Hobby, Musical Instrument, Toy and Book Stores
- 111. Supermarkets and Other Grocery Stores
- 112. Tailor and Alterations Stores
- 113. Theaters and Auditoriums
- 114. Thrift Shops
- 115. Tourist Information Centers
- 116. Travel Arrangement and Reservation Services
- 117. Trophy (Including Awards and Plaques) Shops
- 118. Urban Transit Systems
- 119. Vocational Rehabilitation Services

B. Conditional Uses

The following uses may be allowed by administrative approval of the Planning Director or their designee based upon their overall impact and compatibility with the intent and purpose of the zoning district. Conditional uses are subject to Chapter 17.74 of this ordinance.

- 1. Adult Novelty Stores
- 2. New Car Dealers
- 3. Used Car Dealers
- 4. Bed and Breakfast Inns
- 5. Billiard and Pool Halls
- 6. Blind and Shade Manufacturing
- 7. Blood and Organ Banks
- 8. Bowling Centers
- 9. Cannabis Retail Establishment
- 10. Chocolate Confectionary Manufacturing
- 11. Coffee and Tea Manufacturing
- 12. Commercial and Industrial Machinery and Equipment (Except Automotive and Electronic) Repair and Maintenance
- 13. Commercial and Industrial Machinery and Equipment Rental and Leasing (Except Construction, Mining, Forestry Machinery and Equipment Rental and Leasing, Office Machinery and Equipment Rental and Leasing, and Commercial Air, Rail and Water Transportation Equipment Rental and Leasing)
- 14. Community Gardens
- 15. Convenience Stores with Gas Stations
- 16. Dairy Product Manufacturing
- 17. Distribution Centers
- 18. Drinking Places
- 19. Drive-Thru Windows
- 20. Electric Lighting Equipment Manufacturing
- 21. Emergency Shelters



22. Equipment Sales, Rental and Storage
23. Freight Transportation Arrangement
24. General Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
25. Grain and Bakery Products
26. Home and Garden Equipment Repair and Maintenance
27. Household and Institutional Furniture and Kitchen Cabinet Manufacturing
28. Mattress Manufacturing
29. Microbreweries, Wine Tasting Facilities and Micro-Distilleries
30. Museums, Historical Sites, and Similar Institutions (Except Zoos)
31. Non-Chocolate Confectionery Manufacturing
32. Office Supplies (Except Paper) Manufacturing
33. Outpatient Care Centers
34. Parking Lots
35. Parking Structures
36. Performing Arts Companies
37. Printing
38. Public Utility Substations and Storage Buildings
39. Recycling Center, Neighborhood Only
40. Scenic and Sightseeing Transportation, Land
41. Scientific Research and Development Services
42. Services to Buildings and Dwellings
43. Sign Manufacturing
44. Soft Drink and Ice Manufacturing
45. Sporting and Athletic Goods Manufacturing
46. Telephone Call Center
47. Telephone Exchanges and Switching Equipment
48. Testing Laboratories
49. Tobacco, E-Cigarette, Vapor Accessories, Smoking Accessories or Hookah Shops and Lounges
50. Water and Gas Company Service Facilities

C. Accessory Uses

The following uses shall constitute accessory uses and are subject to administrative approval of the Planning Director or their designee. Accessory uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Arcades (Shade Structures)
2. Canopies
3. Carports
4. Community Gardens
5. Fuel Service Stations
6. Nurseries, Garden Center and Farm Supply Stores
7. Outdoor Dining Areas
8. Parking Lots



- 9. Residential, Proprietor/Caretaker Dwelling Unit
- 10. Retail Distribution of Products Manufactured on Premises
- 11. Tree Nut Farming

D. Temporary Uses

The following uses constitute temporary uses and are subject to administrative approval of the Planning Director or their designee. Temporary uses in this section may require additional permitting, in addition to zoning approval under this chapter.

- 1. Flea Markets/Swap Meets, Temporary Location, Direct-Selling
- 2. Outdoor Dining Areas
- 3. Outdoor Sales Areas

E. Prohibited Uses

The following uses are expressly prohibited within this zone and are considered incompatible with the intent and use of this zone. An applicant may file an administrative appeal with the Planning Director or their designee for re-classification.

- 1. Animal Hospitals and Veterinary Services (with Outdoor Facilities)
- 2. Archery or Shooting Ranges
- 3. Campgrounds
- 4. Cemeteries and Crematories
- 5. Communications and Microwave Installations
- 6. Communications Equipment Manufacturing
- 7. Community Food and Housing, and Emergency and Other Relief Services
- 8. Computer and Peripheral Equipment Manufacturing
- 9. Continuing Care Retirement Communities and Assisted Living Facilities for the Elderly
- 10. Converted Paper Product Manufacturing
- 11. Facilities Support Services
- 12. Footwear Manufacturing
- 13. Refrigerated Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
- 14. Gambling Industries
- 15. General Medical and Surgical Hospitals
- 16. Glass and Glass Product Manufacturing
- 17. Golf Courses and Country Clubs
- 18. Household Appliance Manufacturing
- 19. Leather and Allied Product Manufacturing (Except Footwear and Leather and Hide Tanning and Finishing)
- 20. Lessors of Mini-warehouses and Self-Storage Units
- 21. Manufacturing of Reproducing Magnetic and Optical Media
- 22. Medical Equipment and Supplies Manufacturing



- 23. Miniature Golf Courses
- 24. Mobile Food Services
- 25. Mobile Homes, Provided They Are Kept Mobile and Licensed Pursuant to State Law, When Used for Construction Offices and Caretaker's Quarters on Construction Sites for the Duration of a Valid Building Permit
- 26. Musical Instrument Manufacturing
- 27. Navigational, Measuring, Electro-Medical and Control Instruments Manufacturing
- 28. Nursing Care Facilities
- 29. Office Furniture (Including Fixtures) Manufacturing
- 30. Optical Instrument and Lens Manufacturing
- 31. Other Ambulatory Health Care Services
- 32. Other Residential Care Facilities
- 33. Other Support Services
- 34. Photographic and Photocopying Equipment Manufacturing
- 35. Printing Machinery Equipment Manufacturing
- 36. Psychiatric and Substance Abuse Hospitals
- 37. Remediation and Other Waste Management Services
- 38. Residential Intellectual and Developmental Disability, Mental Health, and Substance Abuse Facilities
- 39. Residential, Accessory Dwelling Unit
- 40. Residential, Single-Family
- 41. Riding Stables
- 42. Rooming and Boarding Houses, Dormitories and Worker's Camps
- 43. Scale and Balance Manufacturing
- 44. Semiconductor and Other Electronic Component Manufacturing
- 45. Services to Buildings and Dwellings
- 46. Sign Manufacturing
- 47. Signs, On-Site Advertising
- 48. Specialty Hospitals (Except Psychiatric and Substance Abuse)
- 49. Spectator Sports
- 50. Waste Collection
- 51. Waste Treatment and Disposal

17.25.040 Property Development Standards.

A. Architectural Guidelines

- 1. All development within this zoning district is subject to architectural review as set forth in Chapter 17.72 of this ordinance.
- 2. All property within this zone shall be subject to the Pueblo Viejo Design Guidelines as the governing architectural guidelines for the zone.



B. Building Types

1. Mixed-Use
 - a. Mixed-use may be oriented in a horizontal and/or vertical development pattern if uses are integrated within the same block.
 - b. Residential uses may be allowed on the first floor of a building provided all the following are met:
 - i. the first-floor height meets requirement (C)(2) of this part;
 - ii. the first floor must be reconfigurable to accommodate a transition to non-residential uses in the future;
 - iii. Residential first floor development floor height must be elevated at least three (3) feet above the sidewalk plane to provide for privacy for residents.
 - iv. No ownership tenure is allowed for first-floor residential use.
 - c. The Planning Director or their designee may waive the requirements of section (B)(2) of this part above except for section (B)(2)(c) of this part.
 - d. Integrated horizontal mixed-use development must include a mix of at least two (2) uses including one use being residential.
2. The lower floor of proposed parking garages that face or partially face any street must include usable commercial space and are required to follow the design guidelines for parking garages.
3. All uses, except for outdoor dining, nurseries, tree-nut farming, parking, and other conditional or temporary outdoor uses permitted in section 17.23.030 shall be conducted entirely within a completely enclosed building.
4. Nurseries must be completely screened from view on all sides. See (I) of this section for screening and fencing requirements.
5. New buildings, that are of only one story and meet only the minimum height criteria of this zone, must be built to structurally support future vertical expansion as market forces may command expansion or reuse as not to inhibit intensification of this zone as envisioned by the General Plan except:
 - a. Flex Buildings. Flex buildings shall meet all other applicable criteria of this section and in addition:
 - i. May contain singularly or in combination, industrial, commercial, retail or service uses.
 - ii. Must provide for reconfigurable interior spaces and partitions to suit the needs of current and future uses.
 - iii. Must adhere to the design guidelines and incorporate surrounding architectural styles, character and/or elements that creates a unique contribution to the Pueblo Viejo District.

C. Building Orientation

1. Buildings shall be oriented towards the street and engage the public realm.
2. Corner lots shall be oriented towards both streets with building entrances encouraged to be oriented towards the corner.
3. Non-residential entrances must be located at the level of the sidewalk plane.



D. Height, Massing and Articulation

1. Building height must be a minimum of two (2) usable stories or twenty-five (25) feet.
2. the first floor must be a minimum of fifteen (15) feet in height to accommodate modern commercial and retail activities, even if the initial use is residential in nature.
3. Buildings that are not two (2) floors or more in height must utilize a faux floor style on the exterior façade to give the visual appearance of more than one story.
4. All buildings shall incorporate articulation and façade treatments as outlined in the applicable design guidelines for this zone.
5. All buildings exceeding four stories in height shall require a minimum ten (10) foot additional front setback (including corner lots) for stories above the fourth story to reduce overall massing and impact on the street.
6. An additional five (5) foot setback for every story shall be required on all building faces adjacent to a single-family residential zone.
7. All buildings must include articulation a minimum of every fifty (50) feet on all facades to break the vertical plane and provide visual interest for pedestrians.
8. All buildings must utilize four-sided architecture in which all facades must receive architectural treatment and meet all requirements of this section and the design guidelines.

E. Density and Lot Size Requirements

1. Development should comply with the allowed development intensities of the General Plan, which include:
 - a. Residential: A minimum of twenty (20) and maximum of sixty-five (65) dwelling units per acre
 - b. office/Commercial/Retail: Floor Area Ratio of a minimum of 0.5 FAR to a maximum of 3.0 FAR.
2. Minimum Lot Area: Five thousand (5,000) square feet.
3. Minimum Lot Depth: None.
4. Minimum Lot Frontage: One hundred (100) percent.
5. All development shall maintain existing block sizes. New development without existing streets shall mimic prevailing block dimensions with maximum block lengths no longer than five hundred (500) feet.
6. The Planning Director or their designee may approve a waiver in minimum lot frontage for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

F. Yard Requirements

1. FRONT YARD: A minimum build-to line equal to the front property line facing the street is required. For corner lots, the minimum size build-to line is equal to



the front property line facing the street and the side property line facing the adjoining street. A setback of up to ten (10) feet from the build-to line is allowed for accessory uses such as outdoor dining and other public spaces. the setback must blend with the public realm.

2. SIDE YARD: the property line shall serve as the minimum build-to line. A setback of up to twenty (20) feet may be utilized anywhere upon the property, including within the side yard, for paseos and pedestrian passage-ways that facilitate passage through the block.
3. REAR YARD: there are no rear yard requirements.
4. Additional requirements for buildings that contain or partially contain manufacturing uses:
 - a. A side or rear yard of not less than thirty (30) feet is required for any building directly adjacent to a single-family residential district. If an alleyway separates the uses, the overall yard may be reduced by ten (10) feet.
5. The Planning Director or their designee may approve a waiver in front and/or side yard requirements for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

G. Housing Affordability

1. A minimum of ten (10) percent of all residential units must be priced for low and/or moderate-income residents. Units should be disbursed throughout the structure and must proportionally reflect the size of market rate units. While trim detail may vary, the overall quality of building materials may not depart substantially from those used within market rate units within the same structure.

H. Off-street Parking, Loading and Circulation

1. A circulation plan must be submitted for development that includes more than one (1) building or more than one (1) parking facility. The circulation plan must address pedestrian, vehicle, transit (if required), and bicycle circulation, ingress/egress and parking and meet the requirements of this section. A professionally conducted parking study is required for any requests for parking reductions provided for in part (8) of this section.
2. Off-street parking and loading facilities shall be provided in accordance with the provision of Section 17.54.010 of this ordinance except where the following provisions shall supersede those of 17.54.010:
 - a. Horizontal or vertical mixed-use development shall require a parking space ratio of three (3) spaces for 1,000 square feet of gross floor area.
 - b. For single-use development that does not meet the requirements for mixed-use development, the following minimum parking requirements shall apply:



- i. Multi-family residential: 1.25 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 0.60 spaces per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: 3.75 spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: twelve (12) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: eleven (11) spaces per 1,000 square feet of gross floor area.
 - c. For single-use development that does not meet the requirements for mixed-use development, the maximum parking requirements shall apply:
 - i. Multi-family residential: 1.33 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 1 space per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: four (4) spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: fifteen (15) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: fifteen (15) spaces per 1,000 square feet of gross floor area.
- 3. No development that was not approved or in existence prior to July 1st, 2019 shall be allowed to locate off-street parking lots adjacent to the primary street.
- 4. On-street parking that is within five hundred (500) feet of the main entrance(s) of a development may be utilized to satisfy the requirement for off-street parking.
- 5. Shared curb-cuts are required for all new development. All curb-cuts are subject to City approval. An internal circulation plan with provisions for shared curb-cuts and internal circulation with neighboring properties is required.
- 6. New development must consider existing development and provide connections to existing development within each block to allow for internal block circulation.
- 7. All internal vehicle circulation roads, except for those leading to non-public areas or loading access, must include sidewalks with compliant ADA facilities and landscaping. Pedestrian facilities must connect all building entrances, retail entrances and residential entrances. Safe and adequate pedestrian connectivity within the development and connections to adjacent development and existing pedestrian facilities is required.
- 8. The requirements of subsection (a) above may be reduced, with the approval of the Planning Commission, if any of the following provisions are provided for in the circulation plan:
 - a. If the proposed development is located within a Business Improvement District or a special district that institutes shared-parking, timed parking restrictions, and/or paid parking.
 - b. The proposed development incorporates paid parking into the development.



- c. A cooperative use agreement is executed with another property owner to provide for a portion of the required parking of the proposed development.
- d. A payment-in-lieu agreement with the City or another entity responsible for parking management to defray the cost for accommodating additional demand generated by the proposed development.
- e. Land set-aside or structural design of proposed parking surface lots or structured parking is provided to allow for expansion to accommodate additional parking supply when demand exceeds approved supply. The parking study must justify current supply, based upon current demand, and provide for future demand thresholds that would trigger the expansion of additional supply. Any future demand thresholds and supply expansion provisions shall become a condition of approval and continued use of the property.

I. Screening and Fencing

- 1. Where this zoning district abuts upon any residential zone, there shall be provided screening not less than six (6) feet or more than eight (8) feet in height on the zoning boundary line. Said screening shall be reduced to forty-two (42) inches in height within a setback area adjacent to a street or highway.
- 2. For nurseries, screening of not less than eight (8) feet in height and not more than twelve (12) feet in height shall fully encompass the nursery.
- 3. Outdoor dining areas shall be separated from the remainder of the sidewalk with the use of appropriate use of planters, fences or other barriers as approved by the design guidelines.
- 4. All screening and fencing shall be subject to the design guidelines.

J. Public Art

- 1. A minimum of one (1) percent of total construction costs must be either invested in public art, visible to the public realm, or provided as payment-in-lieu to the City to fund larger public art projects within the Pueblo Viejo.
- 2. the applicant shall furnish a performance bond equivalent to the estimated public art investment or payment-in-lieu.

K. Public Open Space

- 1. Development over one-half (1/2) acre or more in gross land area, must reserve a minimum of ten (10) percent of the development for public open space accessible from the public realm in the form of a park, pocket park, plaza, paseo, and/or other public gathering space.



2. the applicant may make a payment to the City in lieu of public space reservation for larger park or plaza development within the Pueblo Viejo upon approval of the Planning Director or their designee.

L. Multi-Family Residential Dwelling Unit Size and Common Space Requirements

1. A minimum of one-hundred and fifty (150) square feet of uninterrupted exclusive use common space shall be available to each unit. This may be in outdoor living areas, balconies and/or decks.
2. Decks shall be allowed on rooftops providing they are appropriately screened with architectural features such as a parapet.
3. Ten (10) square feet per unit, or a minimum of one-thousand (1000) square feet, whichever is greater, shall be required for common space for a common recreation and/or leisure area.
4. All common spaces shall be screened from the street by landscaping and/or decorative fencing.
5. The following minimum dwelling unit sizes shall be required:
 - a. Micro-Unit: Three Hundred and Fifty (350) square feet
 - b. Efficiency: Six Hundred (600) square feet
 - c. One-bedroom: Seven-hundred and Fifty (750) square feet
 - d. Each additional bedroom beyond one bedroom: An additional one-hundred and fifty (150) square feet per dwelling unit is required in addition to the minimum requirement above.

M. Development Standards Applicable to Specific Uses

1. Emergency Shelters shall comply with the following criteria:
 - a. Emergency shelters shall be operated by a responsible agency or organization, with experience in managing or providing social services.
 - b. The shelter shall always provide at least one qualified on-site supervisor, plus one attendant for each fifty (50) occupants.
 - c. A shelter shall not be approved when another homeless shelter is existing within three hundred (300) feet of the proposed site.
 - d. Emergency shelters shall provide a setback of thirty (30) feet from the shelter building to any residential zone.
 - e. Parking shall be supplied at a ratio of one vehicle space per ten (10) beds, and one secured bicycle parking area designed to accommodate up to one bicycle per ten (10) beds.
 - f. Each shelter shall be limited to a maximum occupancy of fifty (50) persons, including warming shelters and daytime facilities.
 - g. A management plan shall be required to address how the immediate sheltering needs of individuals who may be turned away from the shelter will be handled. The management plan shall establish a maximum length of time for which clients may be accommodated.
2. Residential, Proprietor/Caretaker Dwelling Unit



- a. No structure originally designed or intended for single-family residential purposes shall be occupied by uses permitted in this zone, except when used as a dwelling unit by a proprietor, manager, custodian or caretaker of a permitted use.
- 3. Manufacturing
 - a. No use shall be established in this zone which causes or emits any dust, gas, smoke, fumes, odors, noises, vibrations, electromagnetic disturbance, radiation, or other similar effects which are or may be detrimental to the public health, safety or general welfare. All uses shall be continuously maintained so that they are neither obnoxious or offensive by reason of the above emissions.
- 4. Outdoor Dining Areas
 - a. Required parking shall be provided for outdoor seating areas, except for common outdoor seating areas not attributable to a single establishment.
 - b. Common outdoor seating areas may be provided as part of required open space areas with provisions for management and maintenance of the area.
 - c. Outdoor seating areas that are within the public rights-of-way shall not be used for entertainment. The Planning Director or their designee may waive the provision on a temporary or permanent basis for entertainment in consultation with the City Engineer.
 - d. Sound amplification devices, such as speakers, shall be limited to devices that are necessary to provide low-level background music. Noise levels shall comply with Chapter 7.04 of the City of Coachella Municipal Code. The Planning Director or their designee may waive this provision, except for Chapter 7.04 compliance, on a temporary or permanent basis in conjunction with a waiver granted in part (c) above.
 - e. Outdoor dining areas are subject to all permit approvals and the design guidelines and configuration and design must be included on any drawings and application submissions.
 - f. Outdoor dining areas located adjacent to, or within the public rights-of-way must leave a minimum of five (5) feet of sidewalk, open always to accommodate pedestrian traffic.
 - g. All outdoor dining within the public rights-of-way must obtain an encroachment permit from the City Engineering Department.
 - h. Outdoor dining areas that include the sales of alcohol must meet all requirements and regulations of the California Department of Alcoholic Beverage Control.



17.25.050 Existing Uses and Structures at the Time of Adoption of this Section.

A. The provisions of this section shall supersede Section 17.78.010 of this ordinance:

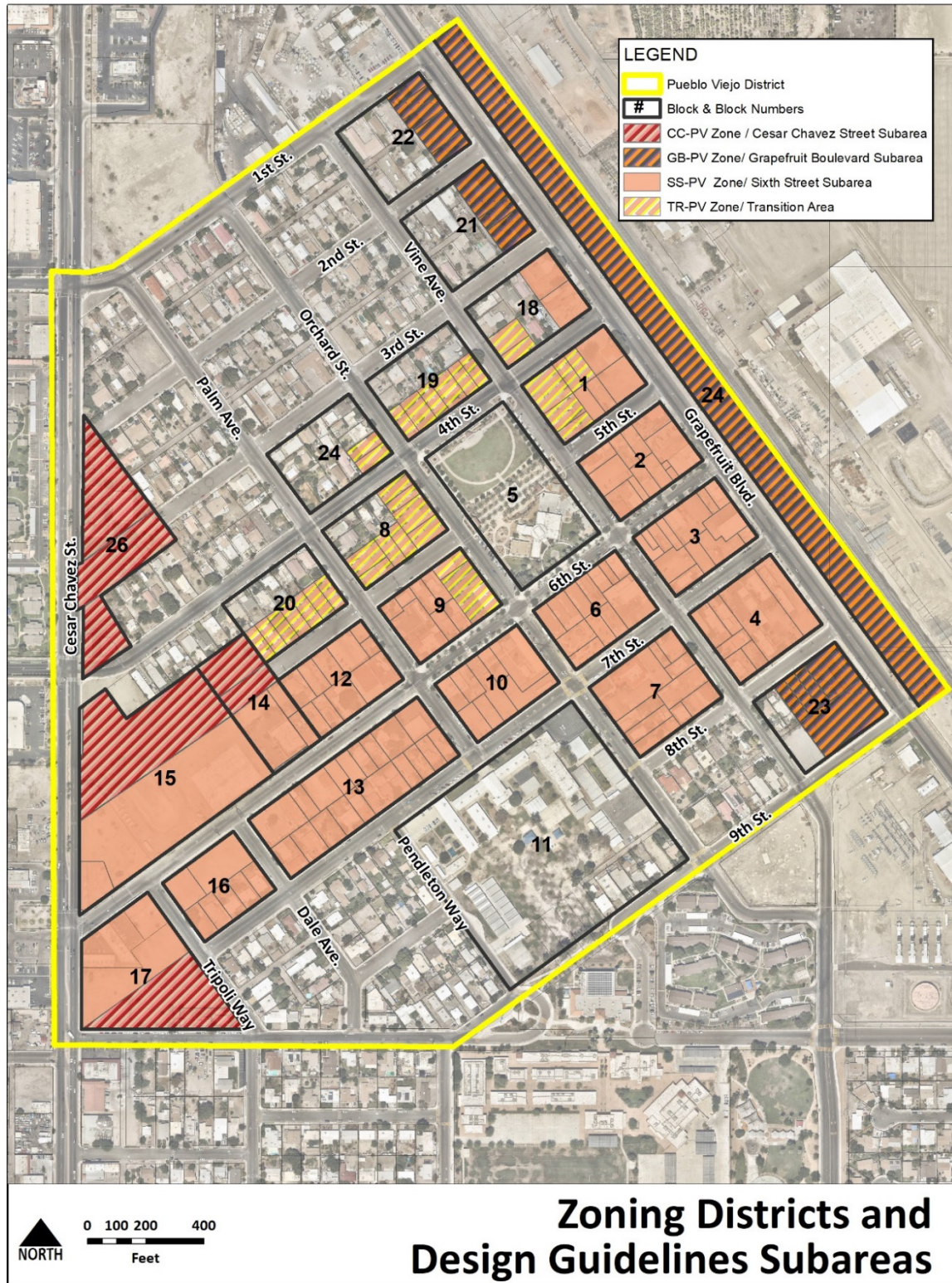
- 1. All uses, lots, structures and characteristics, except for signage, that were lawful, and in existence, prior to July 1st, 2019 shall remain as legally conforming uses, lots, structures and characteristics with all the previous entitlements intact provided:
 - a. The use, lot, structure and/or characteristics remain otherwise lawful.
 - b. No use, lot, structure and/or characteristics may cease operation for a period greater than one (1) year.
 - c. No use, lot, and/or structure may be abandoned for a period greater than one (1) year.
 - d. If the use, lot, structure and/or characteristics fall within a permitted and/or conditional use, as defined in section 17.025.030, the provisions of this section will no longer apply and the previous use, lot, structure and/or characteristics must fully comply with the provisions of this zone thereafter.
- 2. If the provisions of this section are no longer met, the property is then subject to the provisions of Chapter 17.78 of this ordinance.

B. Provisions for ineligible uses, lots, structures and characteristics and revocation of legal conforming status:

- 3. All existing lawful signage, that no longer is permitted, or meets the standards of this district, shall be subject to Chapter 17.78 of this ordinance.
- 4. All uses, lots, structures and characteristics that were not lawful, and in existence, prior to July 1st, 2019 shall remain illegal non-conforming uses and subject to the provisions of Chapter 17.78 of this ordinance.
- 5. Changing of a use governed by this section to a use not permitted in this zone shall immediately terminate the application of this section, and such use shall be reclassified as an illegal non-conforming use and shall be subject to Chapter 17.78 of this ordinance.



CC-PV – CESAR CHAVEZ STREET PUEBLO VIEJO ZONE





17.23.010 Intent and Purpose.

This zone is intended to provide for and encourage the orderly development of the areas of the Pueblo Viejo that border Cesar Chavez Street and serve as gateways into the district. The Pueblo Viejo is envisioned as a higher-density downtown area that provides for a wide variety of multi-family residential housing, office, and retail uses. This zone is intended to be centered around a mixed-use core and encourages future mixed-use development but recognizes the need to support existing uses previously allowed under the C-G General Commercial Use Zone. It provides for an orderly transition to higher intensity development, defined as the Downtown Center in the city’s General Plan. While encouraging pedestrian-friendly connections and safe multi-modal access, this zone recognizes current automobile-oriented uses, but allows for flexibility in reducing parking requirements and retrofitting existing development as market-demands call for new, innovative and higher intensity development styles.

17.23.020 Summary of District Regulations

The following summary table shall not be a substitute for the specific language of each part of this section. Any language and interpretation of that language shall supersede the information contained within this summary table.

Permitted / Conditional / Temporary Use Summary	Retail, Mixed-Use Residential/Commercial, Office and Service
Architectural Review Required	Yes
Applicable Architectural Guidelines	Pueblo Viejo Design Guidelines
General Building Types Allowed	Multi-story Residential, Mixed-Use, Retail, Office, and Shopping Centers.
Minimum Building Height	Two (2) stories or twenty-five (25) feet
Front Setback	Zero (0) to ten (10) feet from property line except for where adjoining zoning is residential, then front setback shall be of equal depth to the average of the required front-yards of the adjoining residential zone.
Upper Floor Setbacks	Ten (10) foot setback for stories adjacent to a single-family residential zone.
Allowed Residential Densities	Minimum twenty (20) D.U. per acre/Maximum sixty-five (65) D.U. per acre
Allowed office/Commercial/Retail Intensities	Minimum FAR 0.5. Maximum FAR 3.0
Maximum Block Size	Five hundred (500) linear feet on Cesar Chavez Street
Minimum Lot Area	Five Thousand (5,000) square feet
Minimum Lot Depth	None
Minimum Lot Frontage	None
Minimum Lot Frontage Coverage	One hundred (100) percent.



Encroachment for Colonnades or Arcades Allowed	On all streets, except Cesar Chavez Street, subject to encroachment permit. Maximum encroachment six (6) foot minimum clearance to curb line.
Housing Affordability Requirements	Yes
Public Open Space Requirements	Ten (10) percent for developments over ½ acre
Minimum Residential Exclusive Use Common Space	One hundred and fifty (150) square feet
Minimum Residential Common Space	10 square feet per unit or 1000 square feet, whichever is greater.

17.23.030 Uses.

Uses are classified according to the 2017 North American Industry Classification System (NAICS). The NAICS use most associated with the actual use (not necessarily the uses primary NAICS code) shall be used to determine use. Additional uses are defined by the City of Coachella and located within the definitions under Chapter 17.06 of this ordinance. the Planning Director or their designee may re-classify uses based upon the most appropriate use under this section based upon this section’s intent and purpose. Appeals of the Planning Director’s decision may be made to the Planning Commission.

A. Permitted Uses

1. Activities Related to Real Estate
2. Alcoholic Beverage Sales (For Off-Premise Consumption)
3. Amusement Arcades (Indoor)
4. Animal Hospitals and Veterinary Services (No Outdoor Facilities)
5. Antique Dealers and Shop
6. Appliance Repair and Maintenance
7. Art and Architecture Supply Shops and Studios
8. Art Dealers
9. Automobile Parts and Accessories Stores
10. Beer and/or Winemaking Supply Retail Stores
11. Business Professional, Labor, Political and Similar Organizations
12. Business Service Centers
13. Candle Shops
14. Caterers
15. Civic and Social Organizations
16. Clothing and Clothing Accessories Stores
17. Collectors Shops
18. Combined Live/Work Dwellings
19. Computer Systems Design and Related Services
20. Consignment Shops
21. Consumer Goods Rentals
22. Cosmetics, Beauty Supplies and Perfume Stores
23. Dance Halls



24. Child Day Care Centers
25. Diet and Weight Reducing Centers
26. Drug Stores or Retail Pharmacies
27. Dry-Cleaning and Laundry Services (Except Linen and Uniform Supply and Industrial Launderers)
28. Educational Services
29. Electronic and Precision Equipment Repair and Maintenance
30. Electronics and Appliance Stores
31. Emergency Services Stations (Including Police and Fire)
32. Employment Services
33. Finance and Insurance Retail Establishments (No Distribution and/or Telephone Call Centers)
34. Fitness and Recreational Sports Centers
35. Flag and Banner Shops
36. Florist Shops
37. Food Service Contractors
38. Footwear and Leather Goods Repair
39. Fruit and Vegetable Markets
40. Funeral Homes and Funeral Services
41. Grantmaking and Giving Services
42. Hair, Nail and Skin Care Services (Including Barber Shops and Beauty Salons)
43. Hardware Stores
44. Health and Personal Care Stores
45. Home Furnishing Stores
46. Home Health Care Services
47. Home Security Equipment Stores
48. Hotels, Resort Hotels and Motels
49. Household Furniture or Stores
50. Independent Artists, Writers and Performers
51. Information
52. Interurban and Rural Bus Transportation
53. Investigation and Security Services (Except Locksmiths)
54. Jewelry and Silverware Manufacturing
55. Jewelry Repair Shops
56. Jewelry, Luggage and Leather Goods Stores
57. Laboratory, Research
58. Laboratory, Support
59. Libraries and Archives
60. Locksmiths
61. Meat, Fish and Seafood Markets (No On-Site Slaughtering)
62. Medical and Diagnostic Laboratories
63. Office Administrative Services
64. Office Machinery and Equipment Rental and Leasing
65. Office Supplies, Stationary and Gift Stores
66. Office, Professional
67. Offices of Lessors of Other Real Estate Property



- 68. Offices of Agents and Managers of Artists, Athletes, Entertainers and Other Public Figures
- 69. Offices of Business Support Services (Except Collection Agencies, Repossession Services, Telephone Answering Services and Telemarketing Bureaus and Other Contact Centers)
- 70. Offices of Construction Industries
- 71. Offices of Dentists
- 72. Offices of Finance and Insurance
- 73. Offices of Lessors of Nonresidential Buildings (Except Mini-warehouses)
- 74. Offices of Lessors of Residential Buildings and Dwellings
- 75. Offices of Management of Companies and Enterprises
- 76. Offices of Motion Picture and Video Industries
- 77. Offices of Newspaper, Periodical, Book and Directory Publishers
- 78. Offices of Other Health Care Practitioners
- 79. Offices of Physicians
- 80. Offices of Professional, Scientific and Technical Services (Other Than Testing Laboratories and Scientific Research and Development Services)
- 81. Offices of Promoters of Performing Arts, Sports and Similar Events
- 82. Offices of Real Estate Agents and Brokers
- 83. Offices of Software Publishers
- 84. Optical Goods Stores
- 85. Other Building Materials Dealers Retail Stores (Except Lumber Stores, Fencing Dealers, Garage Door Dealers and Prefabricated Building Dealers – No Construction or Trade Services Permitted)
- 86. Other Personal and Household Goods Repair and Maintenance
- 87. Other Personal Care Services Including Day Spas, Depilatory or Electrolysis Salons, Salons, Saunas, Ear Piercing Services, Steam or Turkish Baths, Hair Replacement or Weaving Services, Tanning Salons, Massage Parlors, Tattoo Parlors or Permanent Makeup Salons
- 88. Paint and Wallpaper Stores
- 89. Pet and Pet Supply Stores
- 90. Pet Care Services (Except Animal Shelters, Outdoor Boarding Services, Outdoor Catteries, Dog Pounds, Guard Dog Training Services and Outdoor Kennels)
- 91. Photographic Services and Photofinishing
- 92. Post Services
- 93. Recreational or Youth Sports Teams
- 94. Religious Goods Store
- 95. Religious Institution
- 96. Residential, Multi-Family
- 97. Restaurants, Delicatessens, and Other Eating Establishments (No Drive-Thru Permitted)
- 98. Retail Bakeries
- 99. Reupholstery and Furniture Repair
- 100. Social Advocacy Organizations
- 101. Sound Recording Industries
- 102. Specialty Food Stores (No On-Site Slaughtering)
- 103. Sporting Goods, Hobby, Musical Instrument, Toy and Book Stores



- 104. Supermarkets and Other Grocery Stores
- 105. Tailor and Alterations Stores
- 106. Theaters and Auditoriums
- 107. Thrift Shops
- 108. Tourist Information Centers
- 109. Travel Arrangement and Reservation Services
- 110. Urban Transit Systems
- 111. Vocational Rehabilitation Services

B. Conditional Uses

The following uses may be allowed by administrative approval of the Planning Director or their designee based upon their overall impact and compatibility with the intent and purpose of the zoning district. Conditional uses are subject to Chapter 17.74 of this ordinance.

- 1. Adult Novelty Stores
- 2. Bed and Breakfast Inns
- 3. Billiard and Pool Halls
- 4. Bowling Centers
- 5. Cannabis Retail Establishment
- 6. Community Gardens
- 7. Drinking Places
- 8. Drive-Thru Windows
- 9. Microbreweries, Wine Tasting Facilities and Micro-Distilleries
- 10. Motorcycle and ATV Dealers
- 11. Museums, Historical Sites, and Similar Institutions (Except Zoos)
- 12. Outpatient Care Centers
- 13. Parking Lots
- 14. Parking Structures
- 15. Performing Arts Companies
- 16. Public Utility Substations and Storage Buildings
- 17. Recycling Center, Neighborhood Only
- 18. Scenic and Sightseeing Transportation, Land
- 19. Scientific Research and Development Services
- 20. Services to Buildings and Dwellings
- 21. Telephone Exchanges and Switching Equipment
- 22. Testing Laboratories
- 23. Tobacco, E-Cigarette, Vapor Accessories, Smoking Accessories or Hookah Shops and Lounges
- 24. Water and Gas Company Service Facilities



C. Accessory Uses

The following uses shall constitute accessory uses and are subject to administrative approval of the Planning Director or their designee. Accessory uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Arcades (Shade Structures)
2. Canopies
3. Carports
4. Community Gardens
5. Nurseries, Garden Center and Farm Supply Stores
6. Outdoor Dining Areas
7. Parking Lots

D. Temporary Uses

The following uses constitute temporary uses and are subject to administrative approval of the Planning Director or their designee. Temporary uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Flea Markets/Swap Meets, Temporary Location, Direct-Selling
2. Outdoor Dining Areas
3. Outdoor Sales Areas

E. Prohibited Uses

The following uses are expressly prohibited within this zone and are considered incompatible with the intent and use of this zone. An applicant may file an administrative appeal with the Planning Director or their designee for re-classification.

1. Animal Hospitals and Veterinary Services (with Outdoor Facilities)
2. Archery or Shooting Ranges
3. New Car Dealers
4. Used Car Dealers
5. Automobile Service Station
6. Automotive Equipment Rental and Leasing
7. Automotive Repair and Maintenance
8. Blind and Shade Manufacturing
9. Blood and Organ Banks
10. Campgrounds
11. Cemeteries and Crematories
12. Chocolate Confectionary Manufacturing
13. Coffee and Tea Manufacturing



14. Commercial and Industrial Machinery and Equipment (Except Automotive and Electronic) Repair and Maintenance
15. Commercial and Industrial Machinery and Equipment Rental and Leasing (Except Construction, Mining, Forestry Machinery and Equipment Rental and Leasing, Office Machinery and Equipment Rental and Leasing, and Commercial Air, Rail and Water Transportation Equipment Rental and Leasing)
16. Communications and Microwave Installations
17. Communications Equipment Manufacturing
18. Community Food and Housing, and Emergency and Other Relief Services
19. Computer and Peripheral Equipment Manufacturing
20. Continuing Care Retirement Communities and Assisted Living Facilities For the Elderly
21. Convenience Stores with Gas Stations
22. Converted Paper Product Manufacturing
23. Dairy Product Manufacturing
24. Distribution Centers
25. Electric Lighting Equipment Manufacturing
26. Emergency Shelters
27. Equipment Sales, Rental and Storage
28. Facilities Support Services
29. Footwear Manufacturing
30. Freight Transportation Arrangement
31. Refrigerated Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
32. Fuel Service Stations
33. Gambling Industries
34. General Medical and Surgical Hospitals
35. General Rental Centers
36. General Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
37. Glass and Glass Product Manufacturing
38. Golf Courses and Country Clubs
39. Grain and Bakery Products
40. Home and Garden Equipment Repair and Maintenance
41. Household and Institutional Furniture and Kitchen Cabinet Manufacturing
42. Household Appliance Manufacturing
43. Leather and Allied Product Manufacturing (Except Footwear and Leather and Hide Tanning and Finishing)
44. Lessors of Mini-warehouses and Self-Storage Units
45. Manufacturing of Reproducing Magnetic and Optical Media
46. Mattress Manufacturing
47. Medical Equipment and Supplies Manufacturing
48. Miniature Golf Courses
49. Mobile Food Services
50. Mobile Homes, Provided They Are Kept Mobile and Licensed Pursuant to State Law, When Used for Construction Offices and Caretaker's Quarters on Construction Sites for the Duration of a Valid Building Permit



- 51. Musical Instrument Manufacturing
- 52. Navigational, Measuring, Electro-Medical and Control Instruments Manufacturing
- 53. Non-Chocolate Confectionery Manufacturing
- 54. Nursing Care Facilities
- 55. Office Furniture (Including Fixtures) Manufacturing
- 56. Office Machinery Equipment Rental and Leasing
- 57. Office Supplies (Except Paper) Manufacturing
- 58. Optical Instrument and Lens Manufacturing
- 59. Other Ambulatory Health Care Services
- 60. Other Residential Care Facilities
- 61. Other Support Services
- 62. Parcel Delivery Services
- 63. Photographic and Photocopying Equipment Manufacturing
- 64. Printing
- 65. Printing Machinery Equipment Manufacturing
- 66. Psychiatric and Substance Abuse Hospitals
- 67. Remediation and Other Waste Management Services
- 68. Residential Intellectual and Developmental Disability, Mental Health, and Substance Abuse Facilities
- 69. Residential, Accessory Dwelling Unit
- 70. Residential, Proprietor/Caretaker Dwelling Unit
- 71. Residential, Single-Family
- 72. Riding Stables
- 73. Rooming and Boarding Houses, Dormitories and Worker's Camps
- 74. Scale and Balance Manufacturing
- 75. Semiconductor and Other Electronic Component Manufacturing
- 76. Sign Manufacturing
- 77. Signs, On-Site Advertising
- 78. Soft Drink and Ice Manufacturing
- 79. Specialty Hospitals (Except Psychiatric and Substance Abuse)
- 80. Spectator Sports
- 81. Sporting and Athletic Goods Manufacturing
- 82. Telephone Call Center
- 83. Tree Nut Farming
- 84. Trophy (Including Awards and Plaques) Shops
- 85. Waste Collection
- 86. Waste Treatment and Disposal

17.23.040 Property Development Standards.

A. Architectural Guidelines

- 1. All development within this zoning district is subject to architectural review as set forth in Chapter 17.72 of this ordinance.



2. All property within this zone shall be subject to the Pueblo Viejo Design Guidelines as the governing architectural guidelines for the zone.

B. Building Types

1. Mixed-Use:
 - a. Mixed-use may be oriented in a horizontal and/or vertical development pattern if uses are integrated within the same block.
 - b. Residential uses may be allowed on the first floor of a building provided all the following are met:
 - i. the first-floor height meets requirement (C)(2) of this part;
 - ii. the first floor must be reconfigurable to accommodate a transition to non-residential uses in the future;
 - iii. Residential first floor development floor height must be elevated at least 3 feet above the sidewalk plane to provide for privacy for residents.
 - iv. No ownership tenure is allowed for first-floor residential use.
 - c. The Planning Director or their designee may waive the requirements of section (B)(2) of this part above except for section (B)(2)(c) of this part.
 - d. Vertical mixed-use buildings or integrated horizontal developments may reduce parking requirements by forty (40) percent.
 - e. Integrated horizontal mixed-use development must include a mix of at least two (2) uses including one use being residential.
2. The lower floor of proposed parking garages that face or partially face any street must include usable commercial space and are required to follow the architectural guidelines for parking garages as specified in the design guidelines.
3. All uses, except for outdoor nurseries, parking, and conditional or temporary outdoor uses permitted in section 17.23.030 shall be conducted entirely within a completely enclosed building.
4. Nurseries must be completely screened from view on all sides. See (I) of this section for screening and fencing requirements.
5. New buildings, that meet the minimum height criteria of this zone, must be built to structurally support future vertical expansion as market forces may command expansion or reuse as not to inhibit intensification of this zone as envisioned by the General Plan.

C. Building Orientation

1. Buildings shall be oriented towards the street and engage the public realm.
2. Corner lots shall be oriented towards both streets with building entrances encouraged to be oriented towards the corner.
3. Non-residential entrances must be located at the level of the sidewalk plane.



D. Height, Massing and Articulation

1. Building height must be a minimum of two (2) usable stories or twenty-five (25) feet.
2. the first floor must be a minimum of fifteen (15) feet in height to accommodate modern commercial and retail activities, even if the initial use is residential in nature.
3. Buildings that are not two (2) floors or more in height must utilize a faux floor style on the exterior façade for minimum massing.
4. All buildings shall incorporate articulation and façade treatments as outlined in the applicable design guidelines for this zone.
5. All buildings exceeding four stories in height shall require a minimum ten (10) foot additional front setback (including corner lots) for stories above the fourth story to reduce overall massing and impact on the street.
6. An additional five (5) foot setback for every story shall be required on all building faces adjacent to a single-family residential zone.
7. All buildings must include articulation a minimum of every fifty (50) feet on all facades to break the vertical plane and provide visual interest for pedestrians.
8. All buildings must utilize four-sided architecture in which all facades must receive architectural treatment and meet all requirements of this section and the design guidelines.

E. Density and Lot Size Requirements

1. Development should comply with the allowed development intensities of the General Plan, which include:
 - a. Residential: A minimum of twenty (20) and maximum of sixty-five (65) dwelling units per acre
 - b. office/Commercial/Retail: Floor Area Ratio of a minimum of 0.5 FAR to a maximum of 3.0 FAR.
2. Minimum Lot Area: Five thousand (5,000) square feet.
3. Minimum Lot Depth: None.
4. Minimum Lot Frontage: One hundred (100) percent.
5. All development shall maintain existing block sizes. New development without existing streets shall mimic prevailing block dimensions with maximum block lengths no longer than five hundred (500) feet.
6. the Planning Director or their designee may approve a waiver in minimum lot frontage for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

F. Yard Requirements

1. FRONT YARD: A minimum build-to line equal to the front property line facing the street is required. For corner lots, the minimum size build-to line is equal to



the front property line facing the street and the side property line facing the adjoining street. A setback of up to ten (10) feet from the build-to line is allowed for accessory uses such as outdoor dining and other public spaces. the setback must blend with the public realm.

- 2. SIDE YARD: the property line shall serve as the minimum build-to line. A setback of up to twenty (20) feet may be utilized anywhere upon the property, including within the side yard, for paseos and pedestrian passage-ways that facilitate passage through the block.
- 3. REAR YARD: there are no rear yard requirements.
- 4. The Planning Director or their designee may approve a waiver in front and/or side yard requirements for the provision of additional space for public plazas or open space which serve as an extension of the public realm.

G. Housing Affordability

- 1. A minimum of ten (10) percent of all residential units must be priced for low and/or moderate-income residents. Units should be disbursed throughout the structure and must proportionally reflect the size of market rate units. While trim detail may vary, the overall quality of building materials may not depart from those used within market rate units within the same structure.

H. Off-street Parking, Loading and Circulation

- 1. A circulation plan must be submitted for development that includes more than one (1) building or more than one (1) parking facility. The circulation plan must address pedestrian, vehicle, transit (if required), and bicycle circulation, ingress/egress and parking and meet the requirements of this section. A professionally conducted parking study is required for any requests for parking reductions provided for in subsection (H)(8) of this section.
- 2. Off-street parking and loading facilities shall be provided in accordance with the provision of Section 17.54.010 of this ordinance except where the following provisions shall supersede those of 17.54.010:
 - a. Horizontal or vertical mixed-use development shall require a parking space ratio of three (3) spaces per 1,000 square feet of gross floor area.
 - b. For single-use development that does not meet the requirements for mixed-use development, the following minimum parking requirements shall apply:
 - i. Multi-family residential: 1.25 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 0.60 spaces per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: 3.75 spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: twelve (12) spaces per 1,000 square feet of gross floor area.



- v. Drinking Places: eleven (11) spaces per 1,000 square feet of gross floor area.
 - c. For single-use development that does not meet the requirements for mixed-use development, the maximum parking requirements shall apply:
 - i. Multi-family residential: 1.33 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 1 space per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: four (4) spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: fifteen (15) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: fifteen (15) spaces per 1,000 square feet of gross floor area.
- 3. No development that was not approved or in existence prior to July 1st, 2019 shall be allowed to locate off-street parking lots adjacent to the primary street.
- 4. On-street parking that is within five hundred (500) feet of the main entrance(s) of a development may be utilized to satisfy the requirement for off-street parking.
- 5. Shared curb-cuts are required for all new development. All curb-cuts are subject to City approval. An internal circulation plan with provisions for shared curb-cuts and internal circulation with neighboring properties is required.
- 6. New development must consider existing development and provide connections to existing development within each block to allow for internal block circulation.
- 7. All internal vehicle circulation roads, except for those leading to non-public areas or loading access, must include sidewalks with compliant ADA facilities and landscaping. Pedestrian facilities must connect all building entrances, retail entrances and residential entrances. Safe and adequate pedestrian connectivity within the development and connections to adjacent development and existing pedestrian facilities is required.
- 8. The requirements of this section may be reduced, with the approval of the Planning Commission, if any of the following provisions are provided for in the circulation plan:
 - a. The proposed development is located within a Business Improvement District or a special district that institutes shared-parking, timed parking restrictions, and/or paid parking.
 - b. The proposed development incorporates paid parking into the development.
 - c. A cooperative use agreement is executed with another property owner to provide for a portion of the required parking of the proposed development.
 - d. A payment-in-lieu agreement is executed with the City or another entity responsible for parking management to defray the cost for accommodating additional demand generated by the proposed development.



- e. Land set-aside or structural design of proposed parking surface lots or structured parking is provided to allow for expansion to accommodate additional parking supply when demand exceeds approved supply. The parking study must justify current supply, based upon current demand, and provide for future demand thresholds that would trigger the expansion of additional supply. Any future demand thresholds and supply expansion provisions shall become a condition of approval and continued use of the property.

I. Screening and Fencing

- 1. Where this zoning district abuts upon any residential zone, there shall be provided screening not less than six (6) feet or more than eight (8) feet in height on the zoning boundary line. Said screening shall be reduced to forty-two (42) inches in height within a setback area adjacent to a street or highway.
- 2. For nurseries, screening of not less than eight (8) feet in height and not more than twelve (12) feet in height shall fully encompass the nursery.
- 3. Outdoor dining areas shall be separated from the remainder of the sidewalk with the use of appropriate use of planters, fences or other barriers as approved by the design guidelines.
- 4. All screening and fencing shall be subject to the design guidelines.

J. Public Art

- 1. A minimum of one (1) percent of total construction costs must be either invested in public art, visible to the public realm, or provided as payment-in-lieu to the City to fund larger public art projects within the Pueblo Viejo.
- 2. The applicant shall furnish a performance bond equivalent to the estimated public art investment or payment-in-lieu.

K. Public Open Space

- 1. Development over one-half (1/2) acre or more in gross land area, must reserve a minimum of ten (10) percent of the development for public open space accessible from the public realm in the form of a park, pocket park, plaza, paseo, and/or other public gathering space.
- 2. The applicant may make a payment to the City in lieu of public space reservation for larger park or plaza development within the Pueblo Viejo District upon approval of the Planning Director or their designee.



L. Multi-Family Residential Dwelling Unit Size and Common Space Requirements

1. A minimum of one-hundred and fifty (150) square feet of uninterrupted exclusive use common space shall be available to each unit. This may be in outdoor living areas, balconies and/or decks.
2. Decks shall be allowed on rooftops providing they are appropriately screened with architectural features such as a parapet.
3. Ten (10) square feet per unit, or a minimum of one-thousand (1000) square feet, whichever is greater, shall be required for common space for a common recreation and/or leisure area.
4. All common spaces shall be screened from the street by landscaping and/or decorative fencing.
5. The following minimum dwelling unit sizes shall be required:
 - a. Micro-Unit: Three Hundred and Fifty (350) square feet
 - b. Efficiency: Six Hundred (600) square feet
 - c. One-bedroom: Seven-hundred and Fifty (750) square feet
 - d. For each additional bedroom beyond one bedroom: an additional one-hundred and fifty (150) square feet minimum dwelling unit size is required in addition to the minimum requirements above.

M. Development Standards Applicable to Specific Uses

1. Outdoor Dining Areas
 - a. Required parking shall be provided for outdoor seating areas, except for common outdoor seating areas not attributable to a single establishment.
 - b. Common outdoor seating areas may be provided as part of required open space areas with provisions for management and maintenance of the area.
 - c. Outdoor seating areas that are within the public rights-of-way shall not be used for entertainment. The Planning Director or their designee may waive the provision on a temporary or permanent basis for entertainment in consultation with the City Engineer.
 - d. Sound amplification devices, such as speakers, shall be limited to devices that are necessary to provide low-level background music. Noise levels shall comply with Chapter 7.04 of the City of Coachella Municipal Code. The Planning Director or their designee may waive this provision, except for Chapter 7.04 compliance, on a temporary or permanent basis in conjunction with a waiver granted in part (c) above.
 - e. Outdoor dining areas are subject to all permit approvals and the design guidelines and configuration and design must be included on any drawings and application submissions.
 - f. Outdoor dining areas located adjacent to, or within the public rights-of-way must leave a minimum of five (5) feet of sidewalk, open always to accommodate pedestrian traffic.



- g. All outdoor dining within the public rights-of-way must obtain an encroachment permit from the City Engineering Department.
- h. Outdoor dining areas that include the sales of alcohol must meet all requirements and regulations of the California Department of Alcoholic Beverage Control.

17.23.050 Existing Uses and Structures at the Time of Adoption of this Section.

A. The provisions of this section shall supersede Section 17.78.010 of this ordinance:

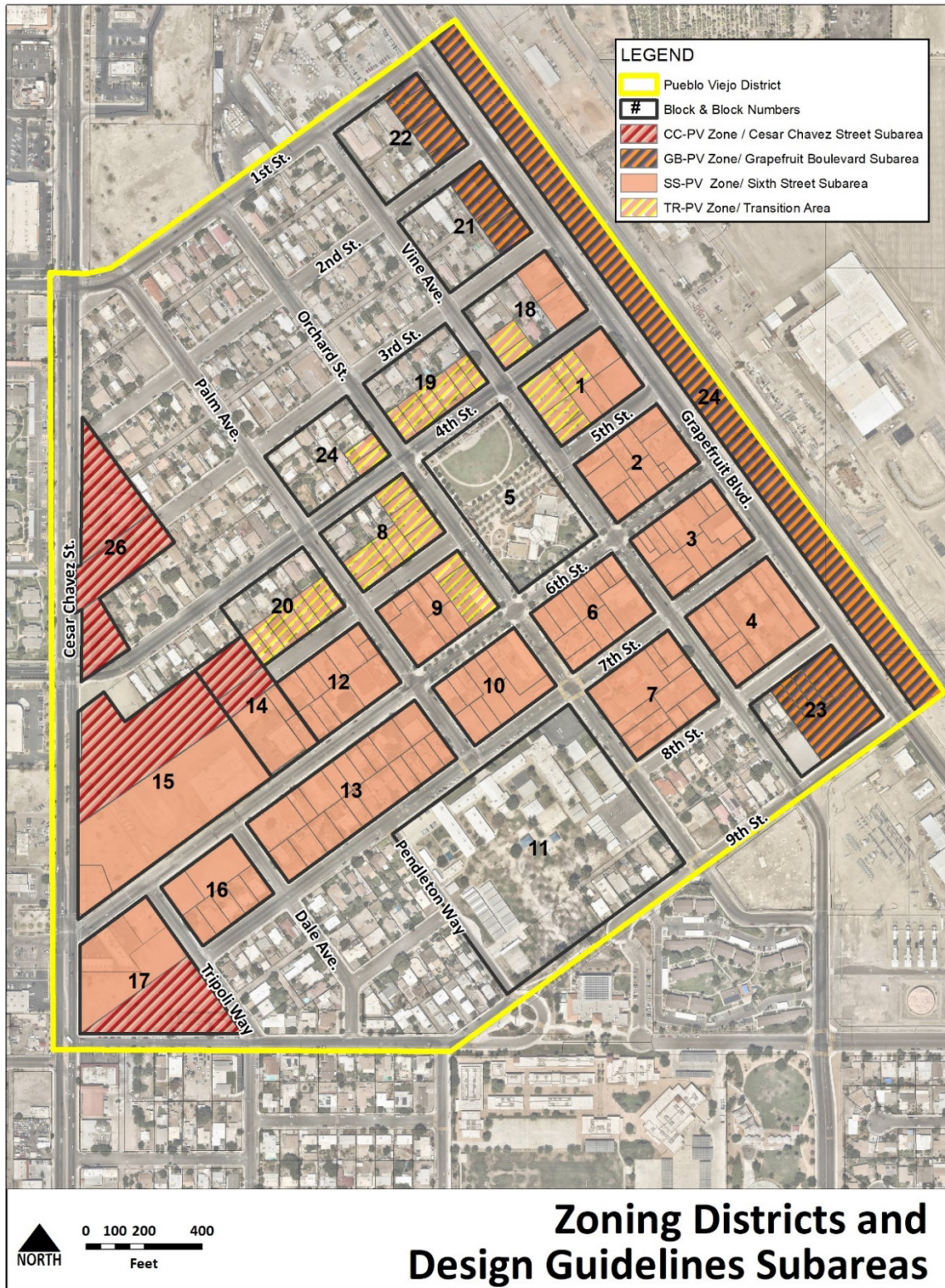
- 1. All uses, lots, structures and characteristics, except for signage, that were lawful, and in existence, prior to July 1st, 2019 shall remain as legally conforming uses, lots, structures and characteristics with all the previous entitlements intact provided:
 - a. The use, lot, structure and/or characteristics remain otherwise lawful.
 - b. No use, lot, structure and/or characteristics may cease operation for a period greater than one (1) year.
 - c. No use, lot, and/or structure may be abandoned for a period greater than one (1) year.
 - d. If the use, lot, structure and/or characteristics fall within a permitted and/or conditional use, as defined in section 17.029.030, the provisions of this section will no longer apply and the previous use, lot, structure and/or characteristics must fully comply with the provisions of this zone thereafter.
- 2. If the provisions of this section are no longer met, the property is then subject to the provisions of Chapter 17.78 of this ordinance.

B. Provisions for ineligible uses, lots, structures and characteristics and revocation of legal conforming status:

- 1. All existing lawful signage, that no longer is permitted, or meets the standards of this district, shall be subject to Chapter 17.78 of this ordinance.
- 2. All uses, lots, structures and characteristics that were not lawful, and in existence, prior to July 1st, 2019 shall remain illegal non-conforming uses and subject to the provisions of Chapter 17.78 of this ordinance.
- 3. Changing of a use governed by this section to a use not permitted in this zone shall immediately terminate the application of this section, and such use shall be reclassified as an illegal non-conforming use and shall be subject to Chapter 17.78 of this ordinance.



TR-PV – TRANSITION AREA PUEBLO VIEJO ZONE





17.27.010 Intent and Purpose.

This zone is intended to allow for an orderly transition in uses from single-family, industrial and mixed-commercial into a low-intensity commercial area that intends to utilize and improve existing structures, encourages infill development and co-exists with surrounding single-family residential use for convenience services and a transition between automobile uses and the core of the Pueblo Viejo. Uses would include professional offices, small retail establishments, bed and breakfasts, short-term rentals, and similar uses to the downtown area, except at a lower level of intensity that limits deliveries, on-site parking and impacts to existing residential land uses. The area would serve to transition commercial development from that along Grapefruit Boulevard to the core of the Pueblo Viejo around Veteran’s Park and City Hall.

17.27.020 Summary of District Regulations.

The following summary table shall not be a substitute for the specific language of each part of this section. Any language and interpretation of that language shall supersede the information contained within this summary table.

Permitted / Conditional / Temporary Use Summary	Transitional commercial, Office, and retail, surface parking, specialty lodging, and live-work units (see 17.27.030 Uses)
Architectural Review Required	Yes
Applicable Architectural Guidelines	Pueblo Viejo Architectural Guidelines
General Building Types Allowed	Adaptive reuse, Infill, Live-work Units (townhouse style)
Maximum Building Height	One story greater than surrounding properties with a maximum of three (3) stories and fifty (50) feet
Front Yard	Average of existing setbacks on block face
Side Yard	Five (5) feet, or ten (10) feet if adjacent to single-family residential use
Rear Yard	Ten (10) feet adjoining residential zone and if structure contains residential units, then a minimum of twenty (20) feet
Upper Floor Setbacks	Ten (10) foot front setback for stories exceeding three (3) facing existing single-family residential.
Allowed Residential Densities	Up to 25 dwelling units per acre
Allowed Office/Commercial/Retail Intensities	0.5 to 1.5 FAR
Maximum Block Size	Integrity of existing blocks shall be maintained
Minimum Lot Area	Five Thousand (5,000) square feet
Minimum Lot Depth	None
Minimum Lot Frontage	Fifty (50) feet
Maximum Lot Coverage	Seventy (70) percent
Housing Affordability Requirements	No



Public Open Space Requirements	Ten (10) percent, except for single-family residential uses.
Minimum Residential Exclusive Use Common Space	One hundred and fifty (150) square feet
Minimum Residential Common Space	10 square feet per unit or 1000 square feet, whichever is greater

17.27.030 Uses.

Uses are classified according to the 2017 North American Industry Classification System (NAICS). The NAICS use most associated with the actual use (not necessarily the uses primary NAICS code) shall be used to determine use. Additional uses are defined by the City of Coachella and located within the definitions under Section 17.06 of this ordinance. The Planning Director or their designee may re-classify uses based upon the most appropriate use under this section based upon this section’s intent and purpose. Appeals of the Planning Director’s decision may be made to the Planning Commission.

A. Permitted Uses

1. Activities Related to Real Estate
2. Antique Dealers and Shop
3. Art and Architecture Supply Shops and Studios
4. Art Dealers
5. Bed and Breakfast Inns
6. Beer and/or Winemaking Supply Retail Stores
7. Business Professional, Labor, Political and Similar organizations
8. Business Service Centers
9. Candle Shops
10. Collectors Shops
11. Combined Live/Work Dwellings
12. Computer Systems Design and Related Services
13. Consignment Shops
14. Finance and Insurance Retail Establishments (No Distribution and/or Telephone Call Centers)
15. Flag and Banner Shops
16. Florist Shops
17. Footwear and Leather Goods Repair
18. Grantmaking and Giving Services
19. Hair, Nail and Skin Care Services (Including Barber Shops and Beauty Salons)
20. Household Furniture or Stores
21. Jewelry, Luggage and Leather Goods Stores
22. Live/Work Units
23. Office Administrative Services
24. Office, Professional
25. Offices of Lessors of Other Real Estate Property



26. Offices of Agents and Managers of Artists, Athletes, Entertainers and Other Public Figures
27. Offices of Business Support Services (Except Collection Agencies, Repossession Services, Telephone Answering Services and Telemarketing Bureaus and Other Contact Centers)
28. Offices of Construction Industries
29. Offices of Dentists
30. Offices of Finance and Insurance
31. Offices of Lessors of Nonresidential Buildings (Except Mini-warehouses)
32. Offices of Lessors of Residential Buildings and Dwellings
33. Offices of Management of Companies and Enterprises
34. Offices of Motion Picture and Video Industries
35. Offices of Newspaper, Periodical, Book and Directory Publishers
36. Offices of Other Health Care Practitioners
37. Offices of Physicians
38. Offices of Professional, Scientific and Technical Services (Other Than Testing Laboratories and Scientific Research and Development Services)
39. Offices of Promoters of Performing Arts, Sports and Similar Events
40. Offices of Real Estate Agents and Brokers
41. Offices of Software Publishers
42. Religious Goods Store
43. Residential, Single-Family detached
44. Residential, Single-Family attached
45. Social Advocacy organizations
46. Sporting Goods, Hobby, Musical Instrument, toy and Book Stores
47. Tailor and Alterations Stores

B. Conditional Uses

The following uses may be allowed by administrative approval of the Planning Director or their designee based upon their overall impact and compatibility with the intent and purpose of the zoning district. All conditional uses are subject to Chapter 17.74 of this ordinance.

1. Animal Hospitals and Veterinary Services (No Outdoor Facilities)
2. Caterers
3. Civic and Social Organizations
4. Community Gardens
5. Continuing Care Retirement Communities and Assisted Living Facilities for The Elderly
6. Child Day Care Centers
7. Employment Services
8. Independent Artists, Writers and Performers
9. Information
10. Interurban and Rural Bus Transportation
11. Investigation and Security Services (Except Locksmiths)
12. Jewelry Repair Shops



13. Microbreweries, Wine Tasting Facilities and Micro-Distilleries
14. Museums, Historical Sites, and Similar Institutions (Except Zoos)
15. Optical Goods Stores
16. Other Personal Care Services Including Day Spas, Depilatory or Electrolysis Salons, Salons, Saunas, Ear Piercing Services, Steam or Turkish Baths, Hair Replacement or Weaving Services, Tanning Salons, Massage Parlors, Tattoo Parlors or Permanent Makeup Salons
17. Outdoor Dining Areas
18. Pet Care Services (Except Animal Shelters, Outdoor Boarding Services, Outdoor Catteries, Dog Pounds, Guard Dog Training Services and Outdoor Kennels)
19. Public Utility Substations and Storage Buildings
20. Residential, Accessory Dwelling Unit
21. Residential, Multi-family
22. Residential, Proprietor/Caretaker Dwelling Unit
23. Restaurants, Delicatessens, and Other Eating Establishments (No Drive-Thru Permitted)
24. Retail Bakeries
25. Reupholstery and Furniture Repair
26. Scenic and Sightseeing Transportation, Land
27. Sound Recording Industries
28. Specialty Food Stores (No On-Site Slaughtering)
29. Supermarkets and Other Grocery Stores
30. Telephone Exchanges and Switching Equipment
31. Tourist Information Centers
32. Travel Arrangement and Reservation Services
33. Trophy (Including Awards and Plaques) Shops
34. Urban Transit Systems
35. Vocational Rehabilitation Services
36. Water and Gas Company Service Facilities

C. Accessory Uses

The following uses shall constitute accessory uses and are subject to administrative approval of the Planning Director or their designee. Accessory uses in this section may require additional permitting, in addition to zoning approval under this chapter.

1. Arcades (Shade Structures)
2. Canopies
3. Carports
4. Community Gardens
5. Drinking Places
6. Florist Shops
7. Fruit and Vegetable Markets
8. Meat, Fish and Seafood Markets (No On-Site Slaughtering)
9. Mobile Food Services
10. Nurseries, Garden Center and Farm Supply Stores



- 11. Outdoor Dining Areas
- 12. Parking Lots

D. Temporary Uses

The following uses constitute temporary uses and are subject to administrative approval of the Planning Director or their designee. Temporary uses in this section may require additional permitting, in addition to zoning approval under this chapter.

- 1. Outdoor Sales Areas

E. Prohibited Uses

The following uses are expressly prohibited within this zone and are considered incompatible with the intent and use of this zone. An applicant may file an administrative appeal with the Planning Director or their designee for re-classification.

- 1. Adult Novelty Stores
- 2. Alcoholic Beverage Sales (For Off-Premise Consumption)
- 3. Amusement Arcades (Indoor)
- 4. Animal Hospitals and Veterinary Services (with Outdoor Facilities)
- 5. Appliance Repair and Maintenance
- 6. Archery or Shooting Ranges
- 7. New Car Dealers
- 8. Used Car Dealers
- 9. Automobile Parts and Accessories Stores
- 10. Automobile Service Station
- 11. Automotive Equipment Rental and Leasing
- 12. Automotive Repair and Maintenance
- 13. Billiard and Pool Halls
- 14. Blind and Shade Manufacturing
- 15. Blood and Organ Banks
- 16. Bowling Centers
- 17. Campgrounds
- 18. Cemeteries and Crematories
- 19. Chocolate Confectionary Manufacturing
- 20. Clothing and Clothing Accessories Stores
- 21. Coffee and Tea Manufacturing
- 22. Commercial and Industrial Machinery and Equipment (Except Automotive and Electronic) Repair and Maintenance
- 23. Commercial and Industrial Machinery and Equipment Rental and Leasing (Except Construction, Mining, Forestry Machinery and Equipment Rental and Leasing, Office Machinery and Equipment Rental and Leasing, and Commercial Air, Rail and Water Transportation Equipment Rental and Leasing)
- 24. Communications and Microwave Installations



- 25. Communications Equipment Manufacturing
- 26. Community Food and Housing, and Emergency and Other Relief Services
- 27. Computer and Peripheral Equipment Manufacturing
- 28. Consumer Goods Rentals
- 29. Convenience Stores with Gas Stations
- 30. Converted Paper Product Manufacturing
- 31. Cosmetics, Beauty Supplies and Perfume Stores
- 32. Dairy Product Manufacturing
- 33. Dance Halls
- 34. Diet and Weight Reducing Centers
- 35. Distribution Centers
- 36. Drive-Thru Windows
- 37. Drug Stores or Retail Pharmacies
- 38. Dry-Cleaning and Laundry Services (Except Linen and Uniform Supply and Industrial Launderers)
- 39. Educational Services
- 40. Electric Lighting Equipment Manufacturing
- 41. Electronic and Precision Equipment Repair and Maintenance
- 42. Electronics and Appliance Stores
- 43. Emergency Services Stations (Including Police and Fire)
- 44. Emergency Shelters
- 45. Equipment Sales, Rental and Storage
- 46. Facilities Support Services
- 47. Fitness and Recreational Sports Centers
- 48. Flea Markets/Swap Meets, Temporary Location, Direct-Selling
- 49. Food Service Contractors
- 50. Footwear Manufacturing
- 51. Freight Transportation Arrangement
- 52. Refrigerated Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
- 53. Fuel Service Stations
- 54. Funeral Homes and Funeral Services
- 55. Gambling Industries
- 56. General Medical and Surgical Hospitals
- 57. General Rental Centers
- 58. General Warehousing and Storage (Except of Noxious, Explosive or Dangerous Materials)
- 59. Glass and Glass Product Manufacturing
- 60. Golf Courses and Country Clubs
- 61. Grain and Bakery Products
- 62. Hardware Stores
- 63. Health and Personal Care Stores
- 64. Home and Garden Equipment Repair and Maintenance
- 65. Home Furnishing Stores
- 66. Home Health Care Services
- 67. Home Security Equipment Stores
- 68. Hotels, Resort Hotels and Motels



- 69. Household and Institutional Furniture and Kitchen Cabinet Manufacturing
- 70. Household Appliance Manufacturing
- 71. Jewelry and Silverware Manufacturing
- 72. Laboratory, Research
- 73. Laboratory, Support
- 74. Leather and Allied Product Manufacturing (Except Footwear and Leather and Hide Tanning and Finishing)
- 75. Lessors of Mini-warehouses and Self-Storage Units
- 76. Libraries and Archives
- 77. Locksmiths
- 78. Manufacturing of Reproducing Magnetic and Optical Media
- 79. Mattress Manufacturing
- 80. Medical and Diagnostic Laboratories
- 81. Medical Equipment and Supplies Manufacturing
- 82. Miniature Golf Courses
- 83. Mobile Homes, Provided They Are Kept Mobile and Licensed Pursuant to State Law, When Used For Construction Offices and Caretaker's Quarters On Construction Sites For The Duration of A Valid Building Permit
- 84. Motorcycle and ATV Dealers
- 85. Musical Instrument Manufacturing
- 86. Navigational, Measuring, Electro-Medical and Control Instruments Manufacturing
- 87. Non-Chocolate Confectionery Manufacturing
- 88. Nursing Care Facilities
- 89. Office Furniture (Including Fixtures) Manufacturing
- 90. Office Machinery and Equipment Rental and Leasing
- 91. Office Machinery Equipment Rental and Leasing
- 92. Office Supplies (Except Paper) Manufacturing
- 93. Office Supplies, Stationary and Gift Stores
- 94. Offices of Professional, Scientific and Technical Services (Other Than Testing Laboratories and Scientific Research and Development Services)
- 95. Optical Instrument and Lens Manufacturing
- 96. Other Ambulatory Health Care Services
- 97. Other Building Materials Dealers Retail Stores (Except Lumber Stores, Fencing Dealers, Garage Door Dealers and Prefabricated Building Dealers – No Construction or Trade Services Permitted)
- 98. Other Personal and Household Goods Repair and Maintenance
- 99. Other Residential Care Facilities
- 100. Other Support Services
- 101. Outpatient Care Centers
- 102. Paint and Wallpaper Stores
- 103. Parcel Delivery Services
- 104. Parking Structures
- 105. Performing Arts Companies
- 106. Pet and Pet Supply Stores
- 107. Photographic and Photocopying Equipment Manufacturing
- 108. Photographic Services and Photofinishing



- 109. Post Services
- 110. Printing
- 111. Printing Machinery Equipment Manufacturing
- 112. Psychiatric and Substance Abuse Hospitals
- 113. Recreational or Youth Sports Teams
- 114. Recycling Center, Neighborhood Only
- 115. Religious Institution
- 116. Remediation and Other Waste Management Services
- 117. Residential Intellectual and Developmental Disability, Mental Health,
and Substance Abuse Facilities
- 118. Residential, Multi-Family
- 119. Riding Stables
- 120. Rooming and Boarding Houses, Dormitories and Worker's Camps
- 121. Scale and Balance Manufacturing
- 122. Scientific Research and Development Services
- 123. Semiconductor and Other Electronic Component Manufacturing
- 124. Services to Buildings and Dwellings
- 125. Sign Manufacturing
- 126. Signs, On-Site Advertising
- 127. Soft Drink and Ice Manufacturing
- 128. Specialty Hospitals (Except Psychiatric and Substance Abuse)
- 129. Spectator Sports
- 130. Sporting and Athletic Goods Manufacturing
- 131. Telephone Call Center
- 132. Testing Laboratories
- 133. Theaters and Auditoriums
- 134. Thrift Shops
- 135. Tobacco, E-Cigarette, Vapor Accessories, Smoking Accessories or
Hookah Shops and Lounges
- 136. Tree Nut Farming
- 137. Waste Collection
- 138. Waste Treatment and Disposal

17.27.040 Property Development Standards.

A. Architectural Guidelines

- 1. All development within this zoning district is subject to architectural review as set forth in Chapter 17.72 of this ordinance.
- 2. All property within this zone shall be subject to the Pueblo Viejo Design Guidelines as the governing architectural guidelines for the zone.



B. Building Type

1. Mixed-Use
 - a. Mixed-use may be oriented in a horizontal and/or vertical development pattern if uses are integrated within the same block.
 - b. Residential uses may be allowed on the first floor of a building provided all the following are met:
 - i. Residential first floor development floor height must be elevated at least 3 feet above the sidewalk plane to provide for privacy for residents.
 - ii. No ownership tenure is allowed for first-floor residential use.
 - c. The Planning Director or their designee may waive the requirements of section (B)(2) of this part above except for section (B)(2)(c) of this part.
 - d. Integrated horizontal mixed-use development must include a mix of at least two (2) uses including one use being residential.
2. The re-use of existing structures is encouraged. Existing structures may be expanded as long as they maintain their architectural character.
3. In-fill development should follow the requirements of part (A) of this section, but also incorporate architectural elements, massing and compatibility with surrounding structures.
4. Residential uses may include single-family detached, in similar massing to the surrounding residential neighborhood, or single-family attached units as townhouses.

C. Building orientation

1. Buildings shall maintain the general orientation of surrounding properties.
2. Non-residential entrances must be located at the level of the sidewalk plane.
3. For existing structures, non-residential entrances must be ADA accessible and adaptation such as site grading and/or ramps may be utilized.

D. Height, Massing and Articulation

1. The maximum building height may be up to one story greater than that of immediate surrounding properties with an absolute maximum building height of three stories or fifty (50) feet, whichever is greater.
2. Buildings greater than two stories that are adjacent to a single-family residential use must utilize a setback of ten (10) feet above the second story for the façade facing the single-family residential use.
3. All buildings must include articulation a minimum of every thirty (30) feet on all facades to break the vertical plane and provide visual interest for pedestrians.
4. All buildings must utilize four-sided architecture in which all facades must receive architectural treatment and meet all requirements of this section and the design guidelines.



E. Density and Lot Size Requirements

1. Development should comply with the allowed development intensities of the General Plan, which include:
 - a. Residential: A maximum of twenty-five (25) dwelling units per acre.
 - b. Office/Commercial/Retail: Floor Area Ratio of a minimum of 0.5 FAR to a maximum of 1.5 FAR.
2. Minimum Lot Area: Five thousand (5,000) square feet.
3. Minimum Lot Depth: None.
4. Minimum Lot Frontage: Fifty (50) feet.
5. Maximum Lot Coverage: Seventy (70) percent.
6. All development shall maintain existing block sizes. New development without existing streets shall mimic prevailing block dimensions with maximum block lengths no longer than five hundred (500) feet.

F. Yard Requirements

1. FRONT YARD: A minimum build-to line equal to the average of front setbacks for existing primary structures on the block face is required. For corner lots, the side build-to line shall constitute the average setbacks for existing primary structures on that block face. The front yard shall be permanently maintained in landscaping as defined in Section 17.60.010(D)(4) of this ordinance.
2. SIDE YARD: The side yard shall be a minimum of five (5) feet. The side yard shall be a minimum of ten (10) feet if adjacent to a single-family residential use.
3. REAR YARD: Where the zone adjoins a residential zone, there shall be a rear yard of not less than ten (10) feet adjoining that residential zone. In the case of a building or structure for residential use, there shall be a rear yard of not less than twenty (20) feet.
4. Waivers to yard requirements may be administratively approved by the Planning Director or their designee based upon justification and design intent of the applicant.

G. Off-street Parking, Loading and Circulation

1. A circulation plan must be submitted for development that includes more than one (1) building or more than one (1) parking facility. The circulation plan must address pedestrian, vehicle, transit (if required), and bicycle circulation, ingress/egress and parking and meet the requirements of this section. A professionally conducted parking study is required for any requests for parking reductions provided for in subsection (H)(8) of this section.
2. Off-street parking and loading facilities shall be provided in accordance with the provision of Section 17.54.010 of this ordinance except where the following provisions shall supersede those of 17.54.010:



- a. Horizontal or vertical mixed-use development shall require a parking space ratio of three (3) spaces per 1000 square feet of gross floor area.
- b. For single-use development that does not meet the requirements for mixed-use development, the following minimum parking requirements shall apply:
 - i. Multi-family residential: 1.25 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 0.60 spaces per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: 3.75 spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: twelve (12) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: eleven (11) spaces per 1,000 square feet of gross floor area.
- c. For single-use development that does not meet the requirements for mixed-use development, the maximum parking requirements shall apply:
 - i. Multi-family residential: 1.33 spaces per unit plus one guest space per every five (5) units. Multi-family residential that is designated as senior housing only: 1 space per unit plus one (1) guest space per every five (5) units.
 - ii. Office: four (4) spaces per 1,000 square feet of gross floor area.
 - iii. Retail: four (4) spaces per 1,000 square feet of gross floor area.
 - iv. Restaurants: fifteen (15) spaces per 1,000 square feet of gross floor area.
 - v. Drinking Places: fifteen (15) spaces per 1,000 square feet of gross floor area.
- 3. No development that was not approved or in existence prior to July 1st, 2019 shall be allowed to locate off-street parking lots adjacent to the primary street.
- 4. On-street parking that is within five hundred (500) feet of the main entrance(s) of a development may be utilized to satisfy the requirement for off-street parking.
- 5. Shared curb-cuts are required for all new development. All curb-cuts are subject to City approval. An internal circulation plan with provisions for shared curb-cuts and internal circulation with neighboring properties is required.
- 6. New development must consider existing development and provide connections to existing development within each block to allow for internal block circulation.
- 7. All internal vehicle circulation roads, except for those leading to non-public areas or loading access, must include sidewalks with compliant ADA facilities and landscaping. Pedestrian facilities must connect all building entrances, retail entrances and residential entrances. Safe and adequate pedestrian connectivity within the development and connections to adjacent development and existing pedestrian facilities is required.
- 8. The parking requirements of this section may be reduced, with the approval of the Planning Commission, if any of the following provisions are provided for in the circulation plan:



- a. the proposed development is located within a Business Improvement District or a special district that institutes shared-parking, timed parking restrictions, and/or paid parking.
- b. The proposed development incorporates paid parking into the development.
- c. A cooperative use agreement is executed with another property owner to provide for a portion of the required parking of the proposed development.
- d. A payment-in-lieu agreement is executed with the City or another entity responsible for parking management to defray the cost for accommodating additional demand generated by the proposed development.
- e. Land set-aside or structural design of proposed parking surface lots or structured parking is provided to allow for expansion to accommodate additional parking supply when demand exceeds approved supply. The parking study must justify current supply, based upon current demand, and provide for future demand thresholds that would trigger the expansion of additional supply. Any future demand thresholds and supply expansion provisions shall become a condition of approval and continued use of the property.

H. Screening and Fencing

- 1. All screening and fencing shall be subject to the design guidelines.

I. Public Art

- 1. A minimum of one (1) percent of total construction costs must be either invested in public art, visible to the public realm, or provided as payment-in-lieu to the City to fund larger public art projects within the Pueblo Viejo.
- 2. The applicant shall furnish a performance bond equivalent to the estimated public art investment or payment-in-lieu.
- 3. The provisions of this section shall not apply to single-family detached residential development or the reuse of existing structures with no modification to the exterior of the structure.

J. Multi-Family Residential Dwelling Unit Size and Common Space Requirements

- 1. A minimum of one hundred and fifty (150) square feet of uninterrupted exclusive use common space shall be available to each unit. This may be in outdoor living areas, balconies and/or decks.
- 2. Decks shall be allowed on rooftops providing they are appropriately screened with architectural features such as a parapet.



- 3. Ten (10) square feet per unit, or a minimum of one thousand (1000) square feet, whichever is greater, shall be required for common space for a common recreation and/or leisure area.
- 4. All common spaces shall be screened from the street by landscaping and/or decorative fencing.
- 5. The following minimum dwelling unit sizes shall be required:
 - a. Micro-Unit: Three Hundred and Fifty (350) square feet
 - b. Efficiency: Six Hundred (600) square feet
 - c. One-bedroom: Seven hundred and Fifty (750) square feet
 - d. Each additional bedroom beyond one bedroom: one hundred and fifty (150) square feet in addition to the minimum requirement of c. in this section.

K. Signage

- 1. The provisions of Chapter 17.56 and the design guidelines shall apply to all signs within this zone. In addition, the following sign is defined and allowed for use in this zone with the following provisions:
 - a. Professional Shingle Sign:
 - i. Definition: A suspended sign that indicates the pertinent facts concerning a business or professional service conducted on the premises.
 - ii. A Professional Single Sign is allowed within this zone is the only allowed sign type for live/work units and/or former single-family detached structures changed to a commercial use.
 - iii. A Professional Shingle Sign may be suspended from a freestanding post that is no more than eight (8) feet high, any existing post, a mailbox post, or affixed to the building.
 - iv. A Professional Shingle Sign cannot exceed the dimensions of one (1) foot by three (3) feet, by three (3) inches, and cannot be internally lit.
 - v. A Professional Shingle Sign may be externally lit with a spotlight affixed to the sign or freestanding, that does not exceed the equivalent of one hundred (100) watts for a conventional light bulb.
 - vi. Any lit sign must not produce glare or projecting light that may spill onto another property or create a nuisance.

L. Multi-Family Residential Dwelling Unit Size and Common Space Requirements

- 1. A minimum of one-hundred and fifty (150) square feet of uninterrupted exclusive use common space shall be available to each unit. This may be in outdoor living areas, balconies and/or decks.
- 2. Decks shall be allowed on rooftops providing they are appropriately screened with architectural features such as a parapet.



- 3. Ten (10) square feet per unit, or a minimum of one-thousand (1,000) square feet, whichever is greater, shall be required for common space for a common recreation and/or leisure area.
- 4. All common spaces shall be screened from the street by landscaping and/or decorative fencing.
- 5. The following minimum dwelling unit sizes shall be required:
 - a. Micro-Unit: Three Hundred and Fifty (350) square feet
 - b. Efficiency: Six Hundred (600) square feet
 - c. One-bedroom: Seven-hundred and Fifty (750) square feet
 - d. Each additional bedroom beyond one bedroom: An additional one-hundred and fifty (150) square feet per dwelling unit is required in addition to the minimum requirements of this section.

M. Development Standards Applicable to Specific Uses

- 1. Residential, Proprietor/Caretaker Dwelling Unit
 - a. No structure originally designed or intended for single-family residential purposes shall be occupied by uses permitted in this zone, except when used as a dwelling unit by a proprietor, manager, custodian or caretaker of a permitted use.
- 2. Outdoor Dining Areas
 - a. Required parking shall be provided for outdoor seating areas, except for common outdoor seating areas not attributable to a single establishment.
 - b. Common outdoor seating areas may be provided as part of required open space areas with provisions for management and maintenance of the area.
 - c. Outdoor seating areas that are within the public rights-of-way shall not be used for entertainment. The Planning Director or their designee may waive the provision on a temporary or permanent basis for entertainment in consultation with the City Engineer.
 - d. Sound amplification devices, such as speakers, shall be limited to devices that are necessary to provide low-level background music. Noise levels shall comply with Chapter 7.04 of the City of Coachella Municipal Code. The Planning Director or their designee may waive this provision, except for Chapter 7.04 compliance, on a temporary or permanent basis in conjunction with a waiver granted in part (c) above.
 - e. Outdoor dining areas are subject to all permit approvals and the design guidelines and configuration and design must be included on any drawings and application submissions.
 - f. Outdoor dining areas located adjacent to, or within the public rights-of-way must leave a minimum of five (5) feet of sidewalk, open always to accommodate pedestrian traffic.
 - g. All outdoor dining within the public rights-of-way must obtain an encroachment permit from the City Engineering Department.



- h. Outdoor dining areas that include the sales of alcohol must meet all requirements and regulations of the California Department of Alcoholic Beverage Control.

17.27.050 Existing Uses and Structures at the Time of Adoption of this Section.

A. The Provisions of this Section Shall Supersede Section 17.78.010 of this Ordinance:

- 1. All uses, lots, structures and characteristics, except for signage, that were lawful, and in existence, prior to July 1st, 2019 shall remain as legally conforming uses, lots, structures and characteristics with all the previous entitlements intact provided:
 - a. The use, lot, structure and/or characteristics remain otherwise lawful.
 - b. No use, lot, structure and/or characteristics may cease for a period greater than one (1) year.
 - c. No use, lot, and/or structure may be abandoned for a period greater than one (1) year.
 - d. If the use, lot, structure and/or characteristics fall within a permitted and/or conditional use, as defined in section 17.027.030, the provisions of this section will no longer apply and the previous use, lot, structure and/or characteristics must fully comply with the provisions of this district thereafter.
- 2. If the provisions of this section are no longer met, the property is then subject to the provisions of Chapter 17.78 of this ordinance.

B. Provisions for Ineligible Uses, Lots, Structures and Characteristics and Revocation of Legal Conforming Status:

- 1. All existing lawful signage, that no longer is permitted, or meets the standards of this district, shall be subject to Chapter 17.78.
 - 2. All uses, lots, structures and characteristics that were not lawful, and in existence, prior to July 1st, 2019 shall remain illegal non-conforming uses and subject to the provisions of Chapter 17.78.
1. Changing of a use governed by this section to a use not permitted in this district shall immediately terminate the application of this section, and such use shall be reclassified as an illegal non-conforming use and shall be subject to Chapter 17.78.



STUDY REPORT
DESIGN GUIDELINES

Pueblo Viejo District

Design Guidelines

City of Coachella



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1 INTRODUCTION

1

1.1 LOCATION

The Pueblo Viejo District serves as the heart of the City of Coachella. The triangular district is bounded by Cesar Chavez Street (formerly Harrison Street) on the west, Grapefruit Boulevard on the east, and Bagdad Avenue on the south. The area houses a variety of businesses, institutional buildings such as the City Hall, the Post Office, the new Library, recreational facilities such as Veterans' Park and numerous other uses. The area is also poised for additional growth with a County facility to be located at 6th Street and Cesar Chavez Street as well as interest in additional development at 6th Street and Cesar Chavez Street.

With Grapefruit Boulevard bordering the plan area, the Pueblo Viejo District is connected to the wider region and has the potential for attracting additional retail and retail customers through tourism, additional housing opportunities and maintaining and growing this unique place and heart of the community. As part of the implementation of the Pueblo Viejo Revitalization Plan, these Design Guidelines serve as a tool to maintaining and growing the sense of place that defines what the Coachella community loves about the Pueblo Viejo and how the community can attain its vision for a more vibrant and active district.

1.2 PURPOSE AND INTENT

The Pueblo Viejo Revitalization Plan, created in 2010, established a vision for the area following extensive public engagement and consultation with stakeholders and City officials. This plan established the following vision:

“Pueblo Viejo is the civic and cultural heart of Coachella. The community is proud of the historic charm, locally-owned businesses, and vibrant civic center. As you enter through the attractive gateways on Sixth Street, you are immersed in a lively street scene offering shady walkways, cooling water fountains, outdoor dining, and unique shopping. Once empty lots are now filled with mixed-use buildings that respect the heritage, climate, and community values. Family-friendly events and festivals fill the streets and public spaces. As you relax in the clean, well maintained civic center core, you know . . . you have arrived in Pueblo Viejo!”

The plan included tools for implementation of this vision including conceptual design guidelines. The City has already started implementing the plan with the revamping of the Veterans’ Park area and the streetscape along three blocks of Sixth Street and around Veterans’ Park and the construction of the new Library.

Pueblo Viejo Vision Diagram. SOURCE: Michael Baker International



As part of the implementation, these Design Guidelines were adopted to create a cohesive look and feel and maintain and enhance the sense of place that is the Pueblo Viejo. More specifically these Design Guidelines:

- Provide additional guidance on private realm architecture and site design to enhance the physical, visual, and functional compatibility between uses;
- Provide guidance on the development of the public realm, including streetscapes and public infrastructure; and
- Ensure that new public and private development in the District is compatible, honors the history of the City of Coachella, and contributes to the unique character of Pueblo Viejo District.

These Design Guidelines are intended to provide design professionals, property owners, residents, and the City a clear understanding of the City of Coachella’s expectations for the planning and design of properties in Pueblo Viejo. The Design Guidelines are supplemental to the City’s Municipal Code (including Zoning Ordinance and Sign Ordinance), General Plan and the Pueblo Viejo Revitalization Plan. These Design Guidelines replace the citywide Design Guidelines for the defined Pueblo Viejo District as outlined in Section 1.1.

1.3 HISTORY AND HERITAGE

Coachella’s early beginnings date back to 1898 when a wood terminal was established on a Southern Pacific Railroad siding. This terminal transported mesquite wood to Los Angeles. Many Mexican American railroad track workers, also known as Traqueros, came to the area to work on this siding and the extension of the Southern Pacific Railroad to the Coachella Valley.

Development of the city did not truly begin until the Rector Brothers, Jason L. Rector and Lon B. Rector, dug a well to access artesian water in November 1900. This well is now covered by the intersection of Grapefruit Avenue and Fifth Street. The first dwelling in the city was an adobe structure owned by Mr. Rector. Many businesses were established by Mr. Rector, including the Coachella Valley Produce Association and the Coachella Valley Refrigerating Company. These businesses served as the basis for growth and future industrial development in the city.

Due in part to the establishment of the Coachella Valley Produce Association, the city has a very strong agrarian history, and for many years, it was the closest place for mostly Mexican-American farmworkers to receive services. In an effort to secure fair wages for these farmworkers, Cesar Chavez, Dolores Huerta, and Gilbert Padilla founded Farmworkers United in 1962 in the Coachella Valley. Cesar Chavez led strikes and boycotts in the 1960s. He is highly regarded throughout the

The history and heritage of the City of Coachella is extremely important to the city’s residents. The Design Guidelines help create a unique place and combat the architecture of nowhere by striving to protect this history and heritage. The Design Guidelines encourage development that is sensitive to both, the existing and historically significant buildings in the District while allowing for innovation and growth that maintains the eclectic and unique sense of place that is the Pueblo Viejo District.

city. In 2018, Harrison Street at the eastern boundary of Pueblo Viejo District was renamed Cesar Chavez Street.

1.4 DOCUMENT REVIEW

1.4.1 2010 Pueblo Viejo Revitalization Plan

The Pueblo Viejo Revitalization Plan established the vision for the redevelopment of the revitalized District. The plan analyzed the current citywide Design Guidelines for the area and called for amending those guidelines in its implementation program. These Design Guidelines provide clear guidance as to how new development and existing development modifications should be designed to be compliant with the Vision for the Pueblo Viejo.

1.4.2 Coachella General Plan

These Design Guidelines help to implement the General Plan. More specifically this includes:

- Implementation of the General Plan’s Guiding Principles
- Land Use and Community Character
 - Downtown Center
 - Public Designations including Mini Parks and Plazas/Greens
 - Public Facilities and Buildings
 - Building Types
 - Subarea 2 – Downtown policies
- Mobility
 - Goal 1. Complete Streets
 - Goal 2. Traffic Calming
 - Goal 3. Pedestrian Network
 - Goal 4. Bicycle Trail Network
 - Goal 5. Transit Supportive Development Patterns
- Community Health and Wellness
 - Policy 2.3 – Housing diversity
 - Policy 8.24 – Public Plazas
- Sustainability and Natural Environment
 - Policy 1.6 – Climate-appropriate building types
 - Policy 1.10 – Adaptation strategy
 - Policy 1.11 – Urban forest
 - Policy 1.14 – Designing for changing precipitation patterns
 - Policy 2.2 – Passive solar design
 - Policy 2.3 – Alternative energy
 - Policy 2.8 – Renewable energy-open space areas
 - Policy 2.9 – Energy-efficient street lighting

- Policy 3.4 – Low impact development
 - Policy 3.7 – Landscape design
 - Policy 3.8 – Groundwater Infiltration
 - Policy 4.6 – Public realm shading
 - Policy 6.5 – Dark sky
- Infrastructure and Public Services
 - Policy 1.7 – Infill
 - Policy 1.9 – Land use compatibility
- Noise
 - Policy 3.2 – Traffic Calming
- Housing
 - Policy 1.5 – Housing Downtown
 - Policy 1.8 – Innovative Construction

1.4.3 Coachella Zoning Ordinance

These design guidelines provide additional guidance for the properties within the Pueblo Viejo District. The guidelines are not intended to replace or supersede the design standards included in the City’s Municipal Code, Title 17, Zoning. These guidelines are intended to enhance those items required by the City’s Zoning Ordinance and provide additional guidance for architectural review, as required in the zoning districts within Pueblo Viejo District. Where conflicts between the Municipal Code and these Design Guidelines occur, the Municipal Code shall prevail. However, the planning Director and Planning Commission will use these Guidelines as a policy document in conditioning the approval of new development projects or substantive remodel projects.

1.5 ORGANIZATION AND USE

This document is organized into five sections:

1. Introduction
2. Design Principles
3. Private Realm Design Guidelines
4. Public Realm Design Guidelines
5. Definitions

The **Introduction** orients the reader to the design guidelines document. The section opens with a description of the Pueblo Viejo District and introduces the purpose and intent of the Design Guidelines. It also provides a brief history of Coachella so that the reader understands the District’s setting and its importance in relation to the larger city. The Introduction summarizes related documents such as the City’s General Plan, 2010 Pueblo Viejo Revitalization Plan, and Zoning Ordinance that also form part of the policy framework for the Pueblo Viejo.

The **Design Principles** section paints a picture of the overall character that is to be achieved in various subareas of the Pueblo Viejo District rather than citing details. It sets forth the overarching principles and/or best practices that are to be followed when considering the Design Guidelines. The section also includes case studies of recent developments in the Pueblo Viejo District.

The **Private Realm Design Guidelines** section details the guidelines that are to be followed when developing private property. The section begins by describing the Pueblo Viejo's historic background and discusses appropriate architectural styles for the commercial areas of the district based on history, cultural influences, changing demographics, and public engagement feedback. It also includes detailed guidelines for various elements based on the styles of architecture that are allowed in the Pueblo Viejo's three commercial subareas. The section also includes suggestions and encouragement for public art, plazas, paseos and gathering spaces.

The **Public Realm Design Guidelines** section provides suggestions for development of public streets, alleys, and sidewalk areas. The section provides suggested typical cross sections and facilities for various streets. While detailed plans are to be created for streetscape concepts for each commercial street, the guidelines include best practices to incorporate in public infrastructure design or expectations the City has for private development of spaces to be included in the public realm. Residential streetscapes are not suggested; rather, traffic calming strategies are laid out for these streets. The section also provides guidance on landscape improvements and includes a planting palette for commercial areas.

The **Definitions** section explains various planning terms that have been used in the document. The words are arranged in alphabetical order for ease of use by the reader. Care has been taken to use simple terms in the document and to avoid planning jargon as much as possible.

2 CHARACTER AND DESIGN PRINCIPLES

2

2.1 DEFINED SUBAREA CHARACTER

The Pueblo Viejo District is divided into six distinct subareas. The focus of the Design Guidelines is on those subareas that may experience in-fill development, commercial and/or mixed-use development, such as the Sixth Street Subarea, Grapefruit Boulevard, Cesar Chavez Street, and the Transition Area surrounding Veterans' Park. The Design Guidelines establish the character, architectural style, massing, materials, and colors for architecture in each of these subareas.

While most of the Pueblo Viejo District consists of low-density residential uses and will remain so, the vision calls for more mixed-use development throughout the core of the Pueblo Viejo. However, the guidelines do not suggest the nature and character of development that might occur in the residential areas except as envisioned in General Plan; rather, they focus on the public realm aspect in these areas and do not provide Design Guidelines for residential uses in these areas.

Below is a brief summary of the subareas, as shown on Map A.

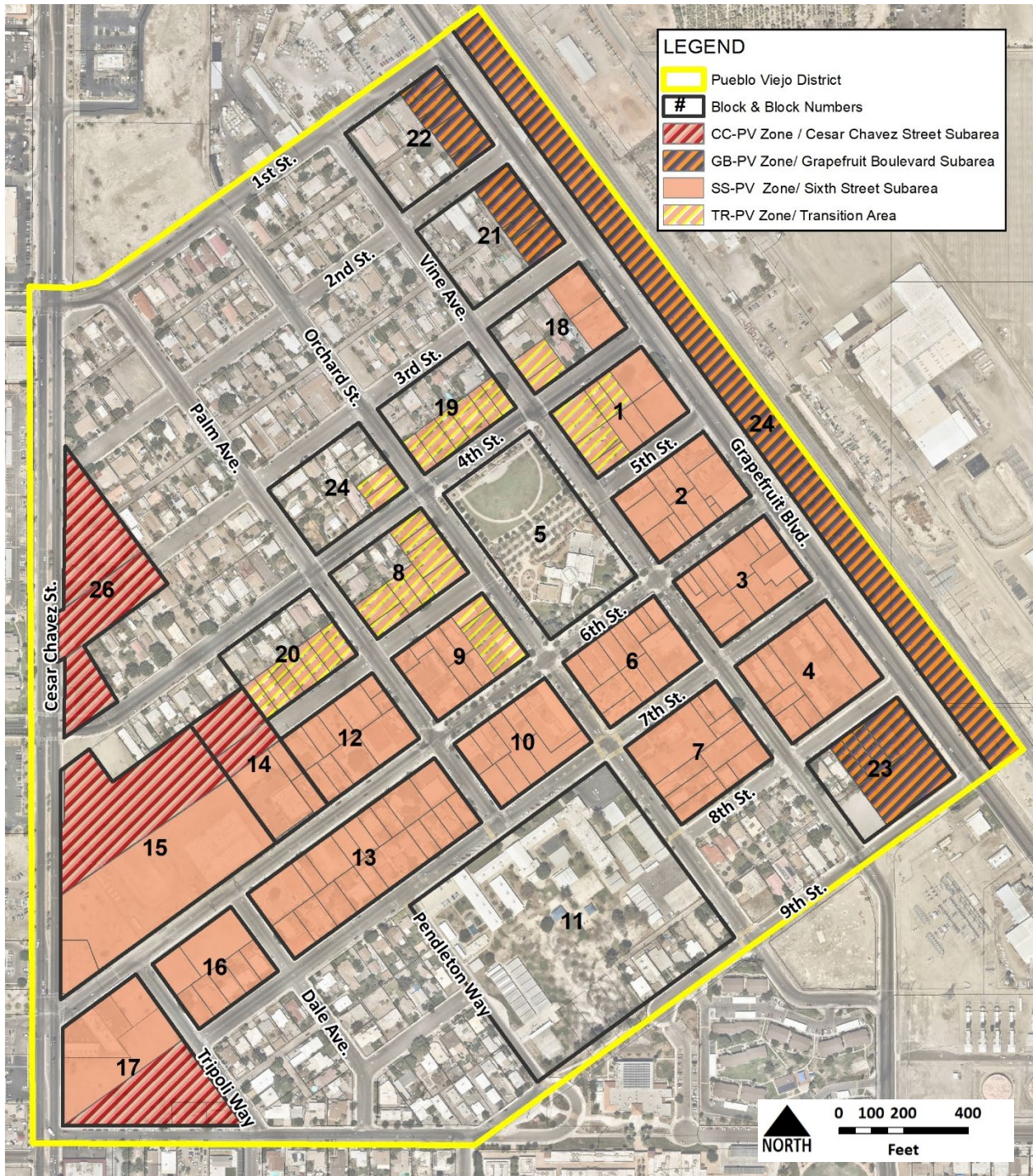
2.1.1 Sixth Street Subarea

The Sixth Street Subarea comprises the area along Fifth, Sixth, and Seventh Streets and includes the Post Office as well as the area surrounding intersections with Grapefruit Boulevard and Cesar Chavez Street. The area is envisioned as a walkable "main street" environment with higher densities, multistory structures with a mix of uses both horizontally and vertically. However, the density should not be overwhelming for a pedestrian, and building architecture should focus on the human scale by creating a superior pedestrian environment.

Sixth Street Subarea Views. SOURCE: Michael Baker International



Map A Subarea Limits Map. SOURCE: Michael Baker International



This is achieved by incorporating significant details in the buildings' architecture, especially on the ground floor. Businesses should have good pedestrian scale frontages to provide a rewarding visual experience for the people passing by. Retail stores should incorporate displays that trigger the interest of pedestrians. The two newly built Library building and the County office building under construction at the time of this report) are good examples of architecture and details that will be suitable for Sixth Street Subarea.

*Library Building along Sixth Street.
SOURCE: Michael Baker International*



Fifth and Seventh Streets are expected to enjoy a spillover effect from Sixth Street and will follow similar development patterns. However, the predominant shaded walkway and wider sidewalks along Sixth Street will create a different look and feel for the space between the streets. The intersections with Grapefruit Boulevard and Cesar Chavez Street are also part of the Sixth Street Subarea. These are entryways to the Sixth Street main street; thus, greater detail in both the private and public realms is advocated in the guidelines to create an inviting environment.

*Sixth Street Subarea Views. SOURCE:
Michael Baker International*



2.1.2 Grapefruit Boulevard

The Grapefruit Boulevard Subarea is envisioned to be automobile focused but also a walkable and bikeable environments. The corridor caters to freestanding structures with their own parking lots and used for commercial, light industrial, and automobile-oriented uses. Multi-family structures may be integrated in the horizontal mix of the area.

*Grapefruit Boulevard Subarea Views.
SOURCE: Michael Baker International*



The Design Guidelines focus on the cosmetic improvement of existing building façades and enhancement of the streetscape to accommodate pedestrians and bicyclists. New development and modifications to existing development should experiment with architectural styles to create auto-oriented yet attractive buildings.



2.1.3 Cesar Chavez Street Subarea

Cesar Chavez Street is a commercial street with mainly auto-oriented franchise stores. The Design Guidelines for this area are similar to those for Grapefruit Boulevard and focus on the improvement of building façades and the implementation of complete and walkable streets. While the uses may differ, the style of architecture, density, height, and general ambiance/placemaking elements will follow the "main street" characteristics described above for Sixth Street as the district transitions towards 6th Street and away from the existing built retail in the subarea.

2.1.4 Transition Area

The Transition subarea refers to the parcels surrounding Veterans’ Park. Because the area is envisioned to serve as a transition between residential and commercial use utilizing the existing residential structures and maintaining similar density and massing. New development and modification of existing uses that fits a transition between residential and commercial scale should be encouraged.

Palm View Elementary subarea views



2.1.5 Palm View Elementary School

The block in which the school is located features institutional uses. Built in 1928, Palm View Elementary is the oldest school in the District and has been plagued for years by obsolescence due to the building age and materials originally used. The current school building features Spanish Revival style architecture with an arched opening at the entrance. It is proposed to be demolished and a new school is to be built on the existing property. The historic nature of the school will be taken into account and will be incorporated into the new design. Since the development is underway, and approved by the City, Design Guidelines are not provided for this subarea.

Civic Center and Veterans’ Park Subarea Views. SOURCE: Michael Baker International



2.1.6 Civic Center and Veterans’ Park

The 6th Street streetscape and Veterans’ Park projects provide a respite from the surrounding buildings. The wide sidewalks in this subarea can incorporate public art that complements the style of these developments. Since the development has recently taken place and is not expected to change in near future, the Design Guidelines are not provided for this subarea.

2.2 COMMUNITY CHARACTER PRINCIPLES

The physical city comprises both the public and the private realms. The public realm includes streets, sidewalks, and areas between the street and the sidewalk, as well as civic buildings, public plazas, parks, and greenways. Private property, including public gathering spaces not owned and operated as part of the public rights-of-way, are considered the private realm. Community character principles are the overarching principles that will define the character of the subareas. These principles are the basis for the specific guidelines pertinent to both the public and private realm that make up a neighborhood character.

2.2.1 Development Principles

- Promote dense and mixed-use development
- Promote development that is mixed both horizontally and vertically
- Promote development that is consisted to current market demand and flexible to accommodate future demand
- Promote reuse of existing historic buildings
- Promote context-sensitive infill projects and site planning
- Promote live building edges and articulated building façades emphasizing human-scale design
- Apply Crime Prevention through Environmental Design principles (CPTED) to development projects

Mixed Use Development Example. SOURCE: Brett VA



Placemaking Element Example. SOURCE: Sixflashphoto



2.2.2 Placemaking Principles

- Provide public places that are memorable and interesting
- Promote placemaking elements that pay tribute to history and tell a story of the City of Coachella
- Design public spaces to accommodate activity

2.2.3 Circulation, Mobility, and Connectivity Principles

- Integrate land use and transportation
- Utilize Complete Streets principles to create a multimodal environment
- Enhance connectivity to transit
- Create a comfortable pedestrian environment
- Create a safe environment for pedestrians and bicyclists
- Incorporate environmental design standards and green development techniques in street design
- Provide an adequate amount of on-street parking while maintaining block integrity.

Complete Streets Cater to All Users and Promote Activity. SOURCE: PeopleForBikes Foundation



3 PRIVATE REALM DESIGN GUIDELINES

3

3.1 INTERPRETATION AND APPLICATION

The guidelines are general and may be interpreted with some flexibility in their application to specific projects. The guidelines will be used during the City's design review process to ensure new development implements General Plan goals and objectives and becomes a compatible part of the total community environment.

These guidelines shall apply to all new development within the Pueblo Viejo District. They shall also be used to review existing development for any reuse, additions, remodel, reconfiguration or other construction requiring a building permit. In the review of modifications or additions to existing development, the provisions of the guidelines will be imposed to the extent that they are applicable and practical to impose in the situation.

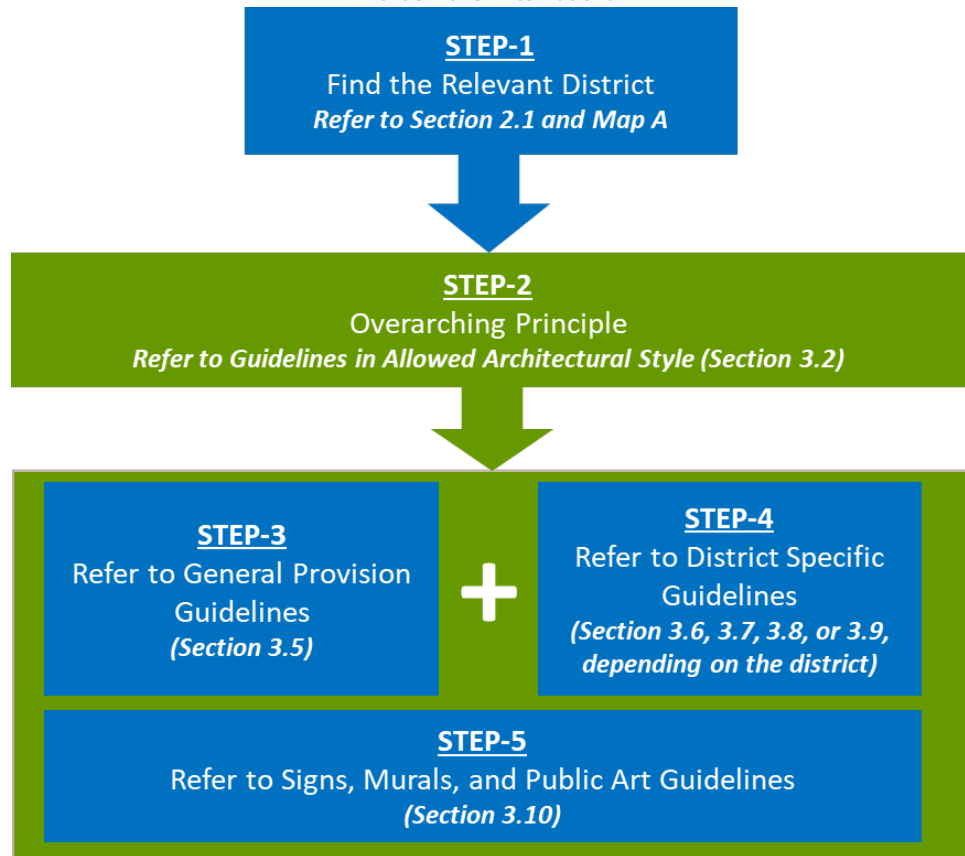
3.2 HOW TO USE GUIDELINES DOCUMENT

The Private Realm Guidelines correspond to development occurring on private property. The City's Zoning Code should always be consulted as the first step of any development project. The guidelines are organized in various sections and the diagram below shows the process of referring to various sections of the guidelines.

- **Step 1:** First step in the usage of the guidelines is to find the relevant district of the project site based on Map A provided in Section 2.1. The section also provides an overall philosophy of the district.

- **Step 2: Overarching Principle:** Finalize the architectural style for the project and refer to Section 3.3 *Allowed and Encouraged Architectural Styles*. These guidelines are referred to in the general provisions as well as subarea guidelines.
- **Step 3:** The applicant should refer to the Section 3.5 *General Provisions*. This section applies to all subareas.
- **Step 4:** Refer to either Section 3.6, 3.7, 3.8 or 3.9 depending on the subareas that the project site is located. These sections give additional guidelines specific to the subarea in addition to general provisions.
- **Step 5:** The Section 3.10 *Signs, Murals, and Public Art*, should be consulted for all projects irrespective of the subarea. The section provides guidelines for each type of signage that is allowed in Pueblo Viejo District. Table A in this section provides a list of various signs and the suggested subareas in which they may be used. The Mural and Public Art guidelines are applicable to all subareas.

Guideline Document Usage Diagram. SOURCE: Michael Baker International



3.3 ALLOWED AND ENCOURAGED ARCHITECTURAL STYLES

A mix of architectural styles and details can create an authentic and timeless downtown. The allowed and encouraged architectural styles will support the Pueblo Viejo’s historic charm, locally owned businesses, and vibrant civic center. A vertical and horizontal mix of uses would set Coachella apart from other cities in the Coachella Valley. In addition, residents prefer a mix of architectural styles that reflect the city’s past and preserve its eclectic style and sense of place to further set the Pueblo Viejo District apart from other areas in the Coachella Valley.

Architectural styles incorporating elements that are responsive to the environmental needs of the desert, including sun-shading, are strongly encouraged. Features such as arcades and colonnades will create livable outdoor spaces that will encourage a vibrant street scene. The architecture of Pueblo Viejo District should support the vision of the district as a vibrant downtown with opportunities for fun, dining, conveniences, and livable residential communities that are close to and integrated with these uses and account for variable economic pressures of changing market conditions.

Pueblo Viejo District Architecture

The existing Pueblo Viejo District architecture in Coachella has been influenced by various architectural styles. A review of historic images captures structures that include elements of the following architectural styles: Classic Revival, Neo-Classical, Western False Front, Spanish Colonial Revival, International Style, and Art Deco.

More recent development is postmodern or international architectural styles. These styles create a platform for experimentation and playfulness often abstractly incorporating historic elements that make a place unique in one respect. It helps break the monotony of traditional styles and enables the creation of unique and iconic structures.

Classic Revival Brick Style



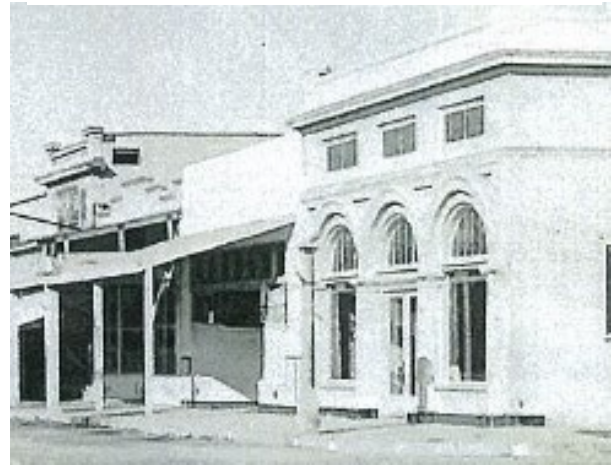
Classic Old West Architecture (Use of Colonnade)



Spanish Colonial Revival



Neoclassical Style



Vernacular Architecture (Use of Shaded Structures) International Style influenced by Mission Revival Style



Historic Images Source: Historic Society of Coachella

Allowed Architectural Styles

To preserve the architectural heritage of the Pueblo Viejo District, but also encourage its eclectic nature, four primary architectural styles are explicitly allowed.

- Spanish Colonial Revival
- Mission Revival
- Neoclassical
- Postmodern
- Architectural Innovation*

**In addition, applicants can depart from these styles with bold new contemporary design, but the design should be in context with the surrounding uses and pay homage to the heritage of the Pueblo Viejo District, the City of Coachella and the Coachella Valley. The detailed guidelines pertaining to each style and applicable to the Pueblo Viejo District are explained in the following sections.*

3.3.1 Spanish Colonial Revival Architecture

The Spanish Colonial Revival style revisits the traditional Spanish architectural themes seen in Spain’s early western colonial settlements. Within the U.S., this style gained national exposure at the 1915 Panama-California Exposition in San Diego. Many of the buildings designed and built for the exposition were in this style. The style is embraced principally in California and Florida and is characterized by a combination of detail from several eras of Spanish and Mexican architecture.

In the Pueblo Viejo District, architecture in the Spanish Colonial Revival style can be found along Sixth Street. Elements of this style that can be identified in Pueblo Viejo District are porch arcades with columns, round arches at entryways, and stucco exterior walls. The Coachella Public Library is designed in this style and bookends the Civic Center subarea.

This style was strongly preferred by stakeholders throughout the development of the 2010 Pueblo Viejo Revitalization Plan and these guidelines. The style blends Coachella’s Mexican heritage with a slightly modern twist and an elegance desired for Pueblo Viejo. Arcades and other attached shade structures are common features of this style and are appropriate to the local desert climate.

3.3.1.1 Overall Building Design

- Spanish Colonial Revival buildings are typically rectangular or L-shaped with horizontal massing and often include interior or exterior courtyards.

Courtyard. SOURCE: the 216



3.3.1.2 Walls

- Smooth or sand-float finish concrete stucco with tastefully placed adornments.

Smooth Stucco Walls. SOURCE: Camaclark



3.3.1.3 Roofs

- Slate or concrete shingles or half-cylindrical/ Spanish (S-shaped) modern concrete tiles should be used for durability.
- Lightly sloped or flat roofs are common to this style and are hidden behind domes or other false parapets.
- Shaped dormers/parapets.

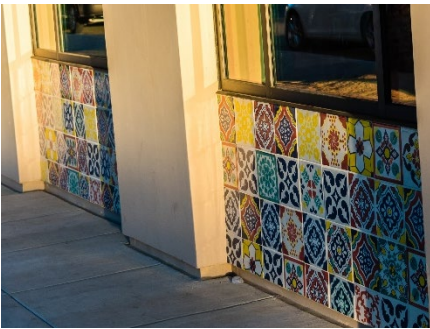
3.3.1.4 Articulation and Decorative Elements

- Patterned cutouts in smooth stucco used to decorate walls, chimneys, and vents.
- Terra-cotta or cast concrete ornaments.

Tower, Arched entry and Windows.



Decorative Tiles SOURCE: Michael Baker International



- Decorative tiles used as a wall surface or as ornament.
- Circular or square columns can be used as vertical elements.
- Rafters and supports may be exposed on the building's exterior.
- Decorative wrought iron and wood brackets or railings.
- Towers are used as decorative elements and are encouraged. Generally round, square, or polygonal towers are used to mark entrances and stairwells.

3.3.1.5 Windows and Doors

- Deeply inset arched or rectangular windows can occur on any side of the building.
- Double-hung and French or wood casement windows are common to this style. Such windows have grilles and usually divide the upper sash into individual panes that are square or close to square, giving the appearance of divided-lite windows.
- Arched entry doorways that are oversized and imposing are a common element of this style.
- French doors are usually used to provide access to porches and verandas, creating a visual connection between the interior and exterior. These are usually made to look like the building's windows.
- The framing for doors and windows is preferred to be wood, though decorative powder-coated metal frames can be used.
- The window, grilles, and doors are typically earth tones such as dark browns, terra-cotta reds, and light greens.
- Windows should be clear glass. Decorative glass is also appropriate in some cases.

3.3.1.6 Porches and Arcades

- Long exterior arcades and galleries that wrap the building are common to this style. Semi-circular arches usually spring from square pillars to form outdoor corridors.
- Small balconies with transparent railings, typically wrought iron.
- Projecting rectangular porticos, porches, or outdoor rooms created with arched openings are prevalent, especially in residential architecture.

Square Pillars for Arcade. SOURCE: Ken Lund



3.3.1.7 Columns and Pillars

- Large square pillars are commonly used with arcades/ exterior corridors of the buildings.

3.3.1.8 Materials and Colors

- The suggested exterior material is smooth, or sand-float finish concrete stucco with a minimum thickness of 7/8 inch. Accent materials can include, but should not be limited to:
 - Natural stone
 - Wood
 - Wrought iron
 - Textured or glazed concrete masonry units
 - High-quality, pre-stressed concrete systems
 - Structural or Corten steel (if a drip area is provided)
 - Hand-painted or natural tile (decorative and otherwise)
- The preferred colors for this architectural style are earthy tone colors used as base color of the buildings. Use of different colors belonging to same family of earth colors is encouraged. Earthy tone colors come from natural things around us: brown soil, green leaf, cloudy sky, as well as the red and yellow sun. These palettes can create a warm, nature-friendly atmosphere.
- Accent colors such as light blues and greens and vibrant blues, greens, reds, and yellows are encouraged. These colors can often be bold or vivid and are used sparingly, to emphasize, contrast or create rhythm and are encouraged.
- Doors usually have a dark wood finish.

Earthy Tone Finishes. SOURCE: Alan



3.3.1.9 Lighting

- Spanish Colonial Revival–style outdoor lighting fixtures with the features listed below include:
 - Wrought iron lighting fixtures, including lanterns of many different shapes
 - Clear glass shades to accent candle-shaped bulbs or amber shaded glass
 - Grandly ornamented fixtures with scrolled metal accents
 - Fixtures are either mounted to the wall or hung using chains

3.3.2 Mission Revival Architecture

Mission Revival architecture in California was inspired by the original Spanish missions. These missions were established in the late eighteenth and early nineteenth century. Mission Revival gained popularity with the train depots of the Santa Fe and Southern Pacific rail companies. By the late nineteenth century, the style began influencing the architecture of residential, commercial, and other institutional buildings. Defining characteristics include roof parapets, simple stucco or plaster siding and

Mission Revival Architecture. SOURCE: Lordkinbote



exposed beams. Below are the suggested guidelines for buildings designed in the Mission Revival architectural style.

3.3.2.1 Overall Building Design

- Enclosed courtyards are common to Mission Revival architecture.

3.3.2.2 Walls

- Unadorned smooth or light-float stucco walls
- Reinforced masonry walls mimicking adobe mud brick

3.3.2.3 Roofs

- Half-cylindrical concrete tiles are common typically of terra cotta or earth tone.
- Low-pitched hipped roofs/ shallow roof lines with wide overhanging eaves and exposed rafters are preferred for the roof structure.
- Shaped dormers/parapets are often used.
- Bell gables can be incorporated as a façade element, along with shaped dormers.

Shaped Dormers. SOURCE: Chris English



3.3.2.4 Articulation and Decorative Elements

- Building designs are usually accompanied with bell gables and rose windows.
- Restrained decorative elements of tile, iron, and wood create articulation on the façade are used, but not to the extent of Spanish Colonial Revival.
- Circular, twisted, or square columns can be used as vertical elements and help break the monotony of the façade.
- While demarcation of floors is preferred, it is not necessary.

Rectangular and Arched Windows, Double Hung Windows and Framing. SOURCE: Jeffrey Beall



3.3.2.5 Windows and Doors

- Deeply inset arched or rectangular windows can occur on any side of the building.
- Double-hung and French or wood casement windows are common to this style. Such windows have grilles and usually divide the upper sash into individual panes that are square or close to square, giving the appearance of divided-lite windows.
- Round or quatrefoil windows are integrated with the building design and create decorative elements on the façade and are encouraged.

- Arched entry doorways are a common element of this style and are encouraged.
- French doors are usually used to provide access to porches and verandas, creating a visual connection between the interior and exterior. These are usually constructed of carved wood and appear heavy. Such doors are encouraged.
- The framing for doors and windows is preferred to be wood, though decorative powder-coated metal frames can be used.
- The windows, grilles, and doors should be of earth tone but can be in a contrasting color to the wall color.
- Simple articulation is preferred in the frames in order to create play of light and shadow.
- Windows should be clear glass. Decorative glass is also appropriate in some cases.

Quatrefoil Windows. SOURCE: Title Insurance and Trust Company



3.3.2.6 *Porches and Arcades*

- Long exterior arcades and galleries that wrap the buildings are common to this style and are encouraged. Arches usually spring from square pillars to form outdoor corridors.
- Projecting porticos, porches, or outdoor rooms created with arched openings are prevalent to this style and are encouraged, especially in residential architecture.

Porches, Pillars. SOURCE: Wolfgang Beckers and Peng



3.3.2.7 *Columns and Pillars*

- Twisted or circular columns are used as decorative elements. These also appear to frame windows in some buildings.
- Large square pillars are commonly used with arcades/ exterior corridors of the buildings.

Arcade. SOURCE: Pretzelpaws



3.3.2.8 *Materials and Colors*

- The suggested exterior material is concrete stucco with a minimum thickness of 7/8 inch. Accent materials can include, but should not be limited to:
 - Natural stone
 - Reinforced brick
 - Textured or glazed concrete masonry units
 - High-quality, pre-stressed concrete systems
 - Other high-quality metals
- The preferred colors for this architectural style are earthy tone colors used as base color of the buildings. Use of different colors belonging to same family of earth colors is encouraged. Earthy tone colors come from natural things around us: brown soil, green leaf, cloudy sky, as well as the red and yellow sun. These palettes can create a warm, nature-friendly atmosphere.

- Accent colors such as light blues and greens and vibrant blues, greens, reds, and yellows are encouraged. These colors can often be bold or vivid and are used sparingly, to emphasize, contrast or create rhythm.

3.3.2.9 Lighting

- Mission Revival–style lighting outdoor fixtures with features listed below:
 - Wrought iron or hammered copper
 - Opaque shades, especially made of amber glass that covers the bare bulbs
 - Use of chains for hanging fixtures is prevalent

3.3.3 Neoclassical Architecture

Neoclassical architecture originated in the eighteenth and early nineteenth centuries. The movement used Greek and Roman details to create structures that are characterized by grandeur of scale through the use of simple geometric forms and columns, as well as blank walls.

Some historic buildings in Coachella bear elements of neoclassical architectural style. However, unlike historic neoclassical buildings seen in other parts of the world that are highly ornate in nature, these buildings are a utilitarian version of the same. The shape and form of buildings, use of columns and other features suggest neoclassical style. Such architectural style with lesser ornamentation compared to traditional neoclassical buildings complements the other styles such as Mission Revival and Spanish Colonial Revival. Hence, this style is suitable for the Pueblo Viejo District.

Rhythm, Order, Geometry and Grandeur in Simplified Form. It is important to note that the overall building height of this example is not appropriate to the Pueblo Viejo.
 SOURCE: Jörg Zügel



3.3.3.1 Overall Building Design

- The building architecture should have order and harmony. Building elements should be symmetrical with a clean geometry, and all parts should fit together to create a cohesive and balanced design.
- Neoclassical buildings usually use the Greek rule of proportion drawn from the golden ratio to design the architectural elements as well as for the overall composition of the building. Such consideration is encouraged.
- Neoclassical architecture style had no domes or towers, so they are discouraged in buildings using this style.
- The building façade is to be flat and long. A screen of freestanding columns, or windows or other elements that mimic columns is usually used. Historic examples within the Coachella valley did

not typically utilize true Greek or Roman columns, but simpler vernacular style.

3.3.3.2 Walls

- The façades are characterized by restrained ornamentation.

3.3.3.3 Roofs

- Roofs are usually flat and horizontal or low-pitched gable.

3.3.3.4 Articulation and Decorative Elements

- While the buildings can have restrained decoration and minimal ornamentation, blank façades are not permitted.
- Corner stones, or quoins, are encouraged to give a sturdy appearance.
- Trims are encouraged to be used around windows and doors.
- Use of a balustrade on upper levels is encouraged.
- Use of dentil molding—a series of closely spaced, rectangular blocks that form a molding—is encouraged below the cornice, along the roofline of a building.
- The decorative dentil molding band is encouraged to be used anywhere on a structure.
- Pilasters—a rectangular support or decorative protrusion that resembles a flat column—are encouraged to be used on the façade. Pilasters should project slightly from the wall and have a base, a shaft, and a capital like a column.

Balustrade and Use of Pilasters, Corner Treatment of Buildings. SOURCE: Jörg Zügel



Vertical Proportioned Windows, Decorative Entryways, Pediment. SOURCE: A.Savin

3.3.3.5 Windows and Doors

- Doorways should have decorative surrounds and pediments (the triangular section found above the entranceway).
- Windows should have vertical proportions with double-hung sashes. Division into six or eight panes is encouraged.
- Windows should be evenly spaced across the building's façade.
- Windows are encouraged to be flanked by shutters.
- Fanlight windows can be used in gables or above doors on the upper floors.

3.3.3.6 Porches and Arcades

- A temple-like pediment over the entry porch is usually seen as a characteristic of neoclassical architecture. However, these architectural features do not reflect the local neoclassical style in Coachella and hence, are discouraged.



- Buildings should have a full-height front porch supported by a row of columns.
- A balustrade along the second-story porch is encouraged.

3.3.3.7 Columns and Pillars

- More vernacular columns or pilasters are preferred over Greek or Roman columns.
- Columns should be even in number.
- Repetition of columns in porches and along the façade is encouraged.

3.3.3.8 Materials and Colors

- Heavier materials such as exposed reinforced brick/brick cladding are to be used in the lower portion of the structure, with lighter materials such as wood used in the upper portions of the façade.
- Exterior materials include:
 - Exposed reinforced brick, stone, and plaster on walls.
 - Trims should be made of wood or materials that are like wood and are durable. The use of foam molding is not permitted.
- If plastered, the buildings are usually in shades of white.

3.3.3.9 Lighting

- Neoclassical outdoor lighting fixtures integrating Greek and Roman ornamentation with the features listed below are encouraged:
 - Wrought iron lighting fixtures, including lanterns of many different shapes
 - Clear glass shades to accent candle-shaped bulbs or amber shaded glass
 - Grandly ornamented fixtures with scrolled metal accents
 - Fixtures are either mounted to the wall or hung using chains

3.3.4 Postmodern Architecture

Postmodernism emerged in the latter half of the twentieth century. It was a reaction against the rigid doctrines of the modernist movement that included austerity, simplicity, functionalism, uniformity, and a lack of ornamentation, color, and human scale, as well as lack of tribute to the

history and culture of the community. Modernist architectural style is a movement based on certain characteristics and features of the building as explained previously (and is not to be confused with contemporary architecture that means architecture of today). Complexity and contradiction of form, style, color, and material is the underlying theme of all postmodern buildings. Postmodernism is a broad movement that draws from a variety of architectural styles, history, culture and heritage and molds them together to create new typically more abstract design.

It is not the intention of this style to limit expression of architectural elements such as roofs, doors, windows, walls, and so on based on strict rules. Rather, the style allows for free expression and leaves room for diverse implementation and is represented by the major characteristics described below. The structures developed in the postmodern style are encouraged to draw from the other architectural styles described in this section: Spanish Colonial Revival, Mission Revival, and neoclassical. The architecture should also draw from local heritage, history and culture, as well as Mexican heritage. Ornamentation, color, and articulation drawn from these styles will allow for continuity while fostering unique architectural expression and respecting the history of the community. Buildings range from using ornamentation on the façade to being works of sculpture themselves.

3.3.4.1 Overall Building Design

- Postmodern buildings often combine astonishing new forms and features with seemingly contradictory elements of earlier periods and architectural styles.
- Contradiction of form with function is also a common element. Using traditional elements with connotations of very modern technology is a prevalent feature of postmodern architecture.
- Fragmentation of one structure into several smaller structures and forms is common with the use of different materials and styles. The concept aligns well with the overall vision of encouraging a building that respects human scale.
- Newer postmodernist compositions are rarely symmetrical, balanced, and orderly. Asymmetrical forms and curved and oblique buildings that tilt, lean, and give a sculptural quality are common. These buildings lend a whimsical character to the street and break the uniformity and order of modernist and international architectural styles.
- Trompe-l'œil, the art of making a two-dimensional object look like it is three-dimensional, makes buildings appear to be more spacious and bigger than they are. This is prevalent in postmodernism.

Use of Architectural Elements and Ornamentation from Different Styles.



Fragmentation of Buildings and Unsymmetrical Structure. SOURCE: Maksim.



Use Colors, Texture and Shapes. SOURCE: Andrew Bossi Laurel,



3.3.4.2 Ornamentation and Colors

- Articulation and ornamentation range from abstraction to monumental and informal forms, as well as traditional and high-tech representations.
- The accumulation of elements freely borrowed from past styles is encouraged to derive ornamentation from Spanish Colonial Revival, Mission Revival, and neoclassical styles.
- Colors and textures are important elements. It is common to use bright vibrant colors as well as different hues and contrasts to make the shapes stand out. Colored glass, ceramic tiles, or stone are also used to add texture.
- Exaggeration of forms is common and is encouraged.

3.3.5 Architectural Innovation

While repeating or reinterpreting the above-mentioned styles of architecture is encouraged, the design guidelines are not intended to curb innovation and the exploration of the evolution of architecture and the built environment. The use of innovative architecture that provides a building superior for its intended use and a benefit to the community can contemporary and international style architecture. The following must be considered if innovative architecture is proposed:

- A narrative explaining the connection of the proposed architecture to the vision, heritage, and/or history of the Pueblo Viejo District
- The use of materials, colors, and design elements that do not clash or disrupt with the surrounding context of structures and uses
- The building's ability to create a positive impact on its surroundings
- The building's ability to create a vibrant, inviting environment for its users

Buildings in an innovative style can push the barriers and create developments that are exceptional in their functionality while being unique in their aesthetics but will also be scrutinized during architectural review more closely based upon the criteria as described above. Proposed developments in an innovative style will be reviewed on a case-by-case basis. As part of the approval process, developers may be asked to provide additional architectural drawings and details, research, graphics, visualizations, and narrative to explain the building's inspiration.

3.4 PROXIMITY OF SIMILAR STYLES

To avoid the creation of mono-architecture that departs from the authenticity and current character and eclectic mix of architecture within the Pueblo Viejo District, buildings of the same architectural style should not be placed directly adjacent to each other. It is recommended that buildings of the same style are placed with at least one building of another style in between them. If buildings of the same architectural style are placed next to one another, the two buildings should use different materials, colors and articulation to avoid a monotonous façade. Buildings of the same style located near one another should also incorporate varied features of the architectural style and varied coloring to ensure the preservation of the downtown charm that currently exists in Pueblo Viejo District. Proximity to similar styles will be considered during the architectural review process.

Breaking Monotony- Use of Variety in Architectural Styles, Materials, Colors and Articulation. SOURCE: Loseto



3.5 GENERAL PROVISIONS

3.5.1 Existing Buildings

Certain existing buildings in the Pueblo Viejo District tell a story of the City's development through years and significantly contribute towards Coachella's heritage. These buildings have distinctive features, finishes, materials, spaces, construction techniques that render a unique character to the buildings and hence the corridor. These buildings and its unique elements should be preserved, restored and/or adapted in creative way for newer use.

- If a building or site has been designated as having historic significance by the Coachella City Council, any development on the site or work to the building should comply with Chapter 15.48, Historic Districts and Site, of the Coachella Municipal Code.
- Applicants/ owners are encouraged to refer to the older pictures of the City and apply preservation, rehabilitation, restoration or reconstruction standards as suggested in the "Secretary of the Interior's Standards for the Treatment of Historic Properties" (<https://www.nps.gov/tps/standards/treatment-guidelines-2017.pdf>).
- Development in out-lots, additions/ alterations to historic structures and adaptive reuse should be done in a manner that they maintain the character of the primary structure and are compatible with the mass, scale, and form of original structure. Such changes should not hinder the ability to interpret the design character of the original building. Compatibility does not necessarily mean imitating the architectural style of the structure but the ability of different components, whether similar or dissimilar, to function together and stand together in harmony.
- Expansions to existing buildings should provide for continuity between the old building and the new addition. It is not necessary to match the existing building but should include prominent design elements of the old building to provide architectural compatibility between old and new.
- New structures built in the outlots shall be treated as new construction and shall comply with the Design Guidelines for new structures.
- Additions to original structures should not interfere with the distinctive or character defining features of the structure and should be limited in the size to preserve the relationship with the existing building
- When adapting the use of a historic building, they should be designed to have the least impact on the historic character of the building. Character defining, and distinctive architectural features should be preserved.

- Original details and materials should be preserved as much as possible. When it is not possible to do so due to extreme financial burden, or safety reasons the replaced materials and details should be as close as possible to the original.
- Regular maintenance and repair are preferred over the replacement of any historic materials or features.
- Energy efficiency during adaptive reuse is encouraged but should be done while being true to the historic character of the building.
- A building's original orientation should not be changed, and the original primary entrance should be used as the building's primary entrance. If an original storefront/ façade has been altered over the years, the preferred treatment is to restore them to their original condition based on historic photos or other evidence.

Uncovering Original Details of Historic Structures. SOURCE: Michael Baker International



3.5.2 Energy Efficiency and Conservation Design

Building construction, operation and maintenance can have a significant impact on the environment through use of energy, material resources, water usage and stormwater runoff. The applicant is encouraged to employ efficiency and conservation principles from established assessment systems such as LEED, BREEAM, BEES, or other rating systems during the design and construction of the project in order to create sustainable built environment.

- **Energy Efficiency:** Use of Passive Solar Design principles and energy efficient systems can greatly help with energy conservation.
 - Consider use of renewable energy sources such as small solar or small wind energy, which can increase energy security and reduce greenhouse gas emissions associated with energy use.

Solar Panels in the Parking- Provides Shade and Renewable Energy. SOURCE: Carol M. Highsmith



- Window orientation should have a good balance between efficiency and other benefits such as bright indoor spaces and views south and should be shaded during the heating season by other buildings or trees. North windows lose significant heat energy and gain very little useful sunlight in the winter. East and west windows are likely to increase air conditioning needs unless and hence should be designed with careful attention to shading.
- The South-facing window should include enough solar glazing for good performance in winter, but not so much that cooling performance in summer will be compromised.
- As much as possible windows should be designed to be operable for easy natural ventilation.
- While building entrances are oriented towards primary streets, daylighting opportunities should be fully investigated, and effort should be made to incorporate daylighting in the design of the building.
- Landscape design should strive to provide trees for summer shading that will help reduce heat load to the building and/or outdoor spaces.
- Use efficient building shapes keep this exposed surface area to a minimum there by saving energy.
- Use efficient insulation systems to reduce heat flow in and out of the building from conduction through attic, sidewalls, basement walls and doors.
- **Materials:** Choices of materials and construction methods are important to reduce energy consumption of a building through reduced solar heat gain or loss, lower maintenance and increase life of the building.
 - Design for long life and adaptability for a variety of uses
 - Use durable low maintenance materials with low embodied energy that are locally sourced (including materials salvaged on site). Low embodied energy materials such as concrete, bricks, and timber will help to reduce energy consumed through mining, processing, manufacturing and transporting the materials as opposed to high embodied energy materials stainless steel, aluminum, and plastics.
 - Select materials that can be re-used or recycled easily at the end of their lives using existing recycling systems.
 - Use efficient and right sized building design to minimize materials

■ **Water Conservation**

- Consider employing system for collecting and using rainwater and grey water for irrigation. This will reduce the consumption of treated water.
- Use low-demand native plants and xeriscaping techniques to reduce water consumption on site.
- Avoid over watering and consider using underground drip irrigation systems, which reduces water loss caused by evaporation of surface water during watering.
- Employ design and construction strategies that reduce storm water runoff and polluted site water runoff.

■ **LID and BMPs**

- Low impact development (LID) best management practices (BMPs) are encouraged in public as well as private landscaped areas including parking areas to encourage infiltration and natural water quality treatment. Traditional retention/detention should be used only if LID options are shown not to work in a particular site due to topography, soil conditions, etc. Examples include:

- Infiltration basin
- Infiltration trench
- Permeable pavement
- Harvest and use BMPs
- Bioretention and/or biofiltration facilities
- Sand filter basin
- Extended detention basin

- Pervious paving and other permeable surface finishes appropriate for the soil conditions and hydrology are encouraged in hardscaped areas. These paving surfaces add texture and facilitate stormwater management. Particular care should be given to the installation, maintenance, and life cycle of such materials.
- Consult the Riverside County Watershed Protection “Whitewater BMP Design Handbook for Low Impact Development” and City of Coachella Municipal Code Section 13.16.047 for more information.

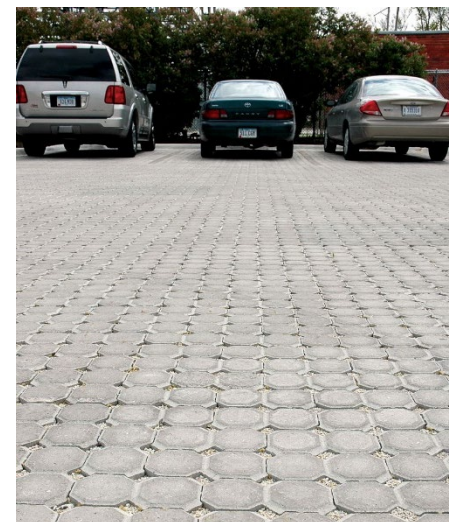
Xeriscaping. SOURCE: Potomacpalms



Low Impact Development. SOURCE: Chris Hamby



Pervious Paving. SOURCE: Jason Johnson



3.5.3 Exterior Building Materials

- Exterior building materials should be durable to avoid rapid wear and tear, economically maintainable, and of a quality that will retain their appearance or age well over time. For example, the use of foam molding of any kind is not permitted due to its lack of durability.
- In selecting exterior building materials, consideration should be given to the appropriateness of the materials based on the building's architectural style.
- Exterior building materials and colors should be compatible with the surrounding neighborhood setting and should be in keeping with the geographic and climatic conditions specific to the Pueblo Viejo District.
- Materials should be authentic whenever possible, although more durable contemporary materials are acceptable if the quality and appearance truly mimics the original authentic material.
- New inventions and established synthetic materials, if used, should age, in a similar manner to the natural materials they replace (i.e. patina).
- Materials should discolor naturally and only if intended as for historic purposes. Materials that unintentionally discolor should be avoided. Architectural styles that do not depend upon a historical context should be maintained in a manner to avoid discoloration and utilize proper maintenance and replacement.
- Reflective materials should be avoided.
- The combination of materials and colors on a building façade should be appropriate to its style and design.
- A variety of materials or colors should be used that emphasize differentiation between the various components of the building.
- A color palette with a minimum of five colors (with a minimum of three-color families) should be provided unless justification is provided for variation from this guideline.
- Adjacent buildings should have a differing color palette.

Changes in Materials, Colors, and Textures. SOURCE: Christopher L. Riley

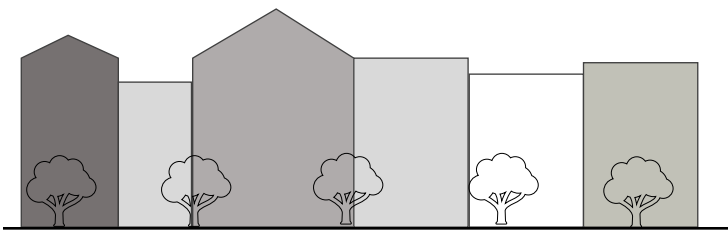


- Larger developments are encouraged to break the monotony of the façades using color and material differentiations in addition to the roofline variations to reflect the small-scale street frontage.

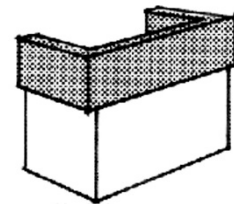
3.5.4 Roofs

- Rooflines should be designed to create architectural interest and to “break” large structures into smaller forms. Rooflines should employ distinctive profiles depending on the building’s architectural style as described in Section 3.3.

Changes in Roofline to Break Monotony. SOURCE: Michael Baker International



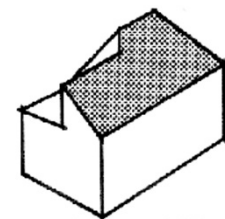
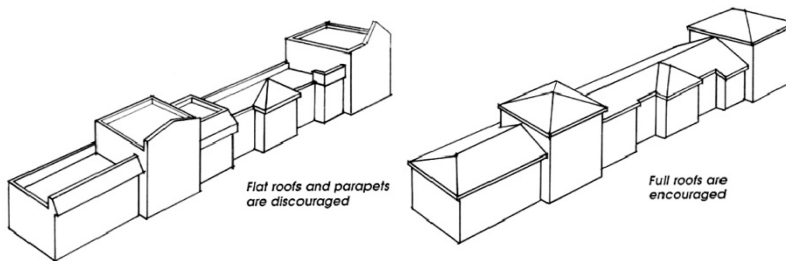
Roofline to Screen the Mechanical Equipment. SOURCE: Michael Baker International



Partial mansard roof discouraged

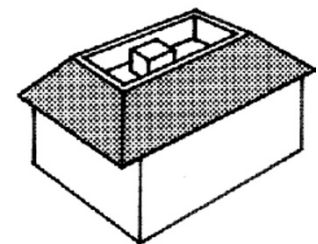
- Parapets facing the street should be subdivided into recognizable segments with shifts in height and architectural treatments.
- Use of flat parapets is discouraged and use of full roofs are encouraged

Flat Parapet vs, Full Roofs. SOURCE: Michael Baker International



Clipped roof to hide rooftop equipment

- The form, color, and texture of the roof should be an integral part of the building design and compatible with both the natural and built settings.
- Rooflines should be designed to screen roof-mounted mechanical equipment. The screening should reflect the architectural style.
- Roof materials should be durable, energy efficient, and relate to the building’s design and architectural style. Roofs should be detailed to be consistent with an appropriate use of the chosen material.



Full mansard roof will hide rooftop equipment

3.5.5 Shade Structures- Awnings

- Refer to subarea guidelines for allowability of awnings on first floor. Awnings are allowed on upper floors of all sub areas.
- Awnings if allowed on first floor should be no longer than a single storefront. Upper-floor awnings should be no longer than the window width.
- Awnings should not be internally illuminated nor used for signage except for lettering on variance.
- Hard material, fabric and canvas awnings with a matte finish are encouraged.
- Awnings with a high gloss finish and plastic awnings are prohibited.
- Awnings should be made of a durable material that is not susceptible to sun-bleaching. If the awnings become unsightly, they should be replaced immediately.
- Awning colors will be compatible with the overall color scheme of the façade from which it projects. Solid colors or subtle striped patterns are preferred.
- Awnings for principal structures if allowed on first floor should be mounted at a consistent height of 9 feet.

*Outdoor Furniture Examples.
SOURCES: Top- K.Артём.1; Middle-
Lee Cannon; Bottom- lienyuan lee.*



3.5.6 Outdoor Furniture

- Areas open to public at all times are encouraged to have a combination of movable as well as fixed furniture. Movable chairs give flexibility and are encourage public gathering in plaza area.
- All furniture used in outdoor dining area and outdoor gartering spaces shall be situated in a manner to maintain adequate and safe pedestrian and consistent with provisions of the Americans with Disabilities Act and State of California accessibly standards.
- Furniture used in outdoor dining area and outdoor gartering spaces shall be of substantial quality to withstand outdoor use, and maintained in good visual appearance, condition, quality and repair at all times.
- All furniture in outdoor dining area and outdoor gartering spaces should be compatible with the building’s architectural style and consider the climatic conditions. For example, the area receiving direct sunlight should refrain from using metal furniture unless there is enough shade to keep the furniture cool.
- The use of the following materials is encouraged for furniture, in or adjacent to sidewalk areas, while still maintaining quality and structure:
 - Metal, Aluminum and wrought iron

- Wood treated for weather and sun- example “French Bistro” dining sets
- Outdoor furniture including but not limited to tables, chairs and umbrellas, used in an Outdoor Dining Area shall not encroach on the property line or frontage of an adjacent business or property.

3.5.7 Perimeter Fences and Walls

- While security fences are allowed, chain link, barbed wire and razor wire, fences should not be used.
- The design of fences, walls, and other structural landscape features should be compatible with and complementary to the architecture of the building and the surrounding setting.
- Fences and walls should be constructed of durable materials and compatible with the architectural style of the building.
- All fences, walls, and other related features should be accompanied by landscaping to better integrate the structure within the site and reduce its visual impact.
- Walls and fences are generally discouraged along the public realm. But if necessary for safety and aesthetic reasons, wrought iron fences with landscaping are preferred over solid walls unless the walls also double as seating and are combined with landscaping.

Perimeter Fence Examples. SOURCES: Top- Thomas1313; Bottom- C&CGabrielle



3.5.8 Exterior Building Lighting

- Exterior building lighting should be used to light building architecture and site areas so as to accentuate design features and promote security in an attractive and understated manner that minimizes off-site impacts.
- Lighting fixtures should complement the façade and be of a style that suits the architectural style of the building.
- Shielded fixtures that minimize sky glow, glare and light trespass should be used. Site lighting should not be directed off-site.
- Direct façade lighting should be provided at all building entrances and along walkways.
- White light that interferes with the circadian rhythms of wildlife and people, and also creates safety issues due to its impact on the retina, should be avoided. Outdoor lighting should be no greater than 3000 degrees Kelvin color temperature as recommended by the American Medical Association.
- Lighting reduction and energy-efficient timer systems should be required after normal business hours except for lighting that is mandated for general safety and security.
- While up-lighting may be allowed to accentuate certain architectural features, care should be taken to make sure that such features minimize sky glow,

- Exterior building lighting should remain in operation till the general peak hours of the Subarea as determined by the City Staff in order to keep the consistency of lighting in the subareas and to encourage pedestrian activity. The lighting may be reduced during non-peak hours, but lighting should not be completely eliminated in parking lots or structures at any time.

Examples of Acceptable and Unacceptable Lighting Features. SOURCE: International Darksky Association (IDA) (<https://www.darksky.org/our-work/lighting/lighting-for-citizens/lighting-basics/>)

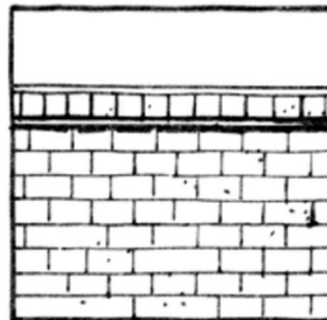
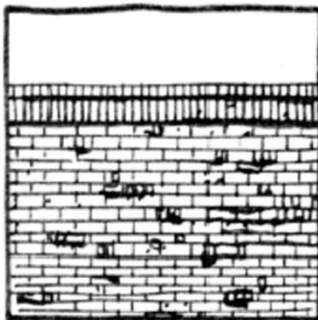
DISCOURAGED	ENCOURAGED
<p style="text-align: center;">Fixtures that produce glare and light trespass</p> <p style="text-align: center;">Unshielded Floodlights or Poorly-shielded Floodlights</p> <p style="text-align: center;">Unshielded Wallpacks & Unshielded or Poorly-shielded Wall Mount Fixtures</p> <p style="text-align: center;">Drop-Lens & Sag-Lens Fixtures w/ exposed bulb / refractor lens</p> <p style="text-align: center;">Unshielded Streetlight</p> <p style="text-align: center;">Unshielded Bollards</p> <p style="text-align: center;">Unshielded Barn Light</p> <p style="text-align: center;">Louvered 'Marine' style Fixtures</p> <p style="text-align: center;">Unshielded 'Period' Style Fixtures</p> <p style="text-align: center;">Unshielded PAR Floodlights</p> <p style="text-align: center;">Drop-Lens Canopy Fixtures</p>	<p style="text-align: center;">Fixtures that shield the light source to minimize glare and light trespass and to facilitate better vision at night</p> <p style="text-align: center;">Full Cutoff Fixtures</p> <p style="text-align: center;">Fully Shielded Wallpack & Wall Mount Fixtures</p> <p style="text-align: center;">Fully Shielded Fixtures</p> <p style="text-align: center;">Full Cutoff Streetlight</p> <p style="text-align: center;">Fully Shielded Barn Light</p> <p style="text-align: center;">Fully Shielded Walkway Bollards</p> <p style="text-align: center;">Fully Shielded Decorative Fixtures</p> <p style="text-align: center;">Fully Shielded 'Period' Style Fixtures</p> <p style="text-align: center;">Shielded / Properly-aimed PAR Floodlights</p> <p style="text-align: center;">Flush Mounted or Side Shielded Under Canopy Fixtures</p>

Illustrations by Bob Crelin© 2005. Rendered for the Town of Southampton, NY. Used with permission.

3.5.9 Trash and Recycling Enclosure

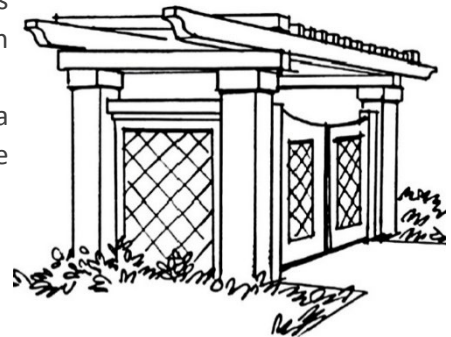
- Trash collection, trash compaction, outdoor storage, utility meters, HVAC equipment, and other service functions should take place behind buildings or on the side of structures and should be visually screened.
- Trash enclosures should be located away from adjacent parcels to minimize noise and odor impacts typically associated with garbage collection and storage.
- The enclosure shall be placed on a concrete pad and have a concrete apron with a minimum depth of 4 inches. Adequate drainage shall be provided around the pad area.

Examples of Solid Masonry or Concrete Tilt-up. SOURCE: Michael Baker International



- All refuse containers shall be screened with a six-foot high (minimum) enclosure of solid masonry or concrete tilt-up with an exterior finish compatible to the main structure.
- Gates shall be solid, heavy-gauge metal or of a heavy-gauge metal frame with a covering of wood or other suitable, opaque material compatible to the main structure.
- The perimeter of the recycling and trash enclosure shall be planted where practical with drought-resistant landscaping, including a combination of shrubs and/or climbing evergreen vines. Attractive xeriscaping may be used as a landscaping option.
- Safety boulders made of durable materials such as steel or concrete should be used to protect recycling and trash enclosure from moving traffic especially in parking and loading area.

Use of Trellis to Screen Trash Receptacle Area. SOURCE: Michael Baker International



Use of Landscaping Around Trash Receptacle Area. SOURCE: Michael Baker International

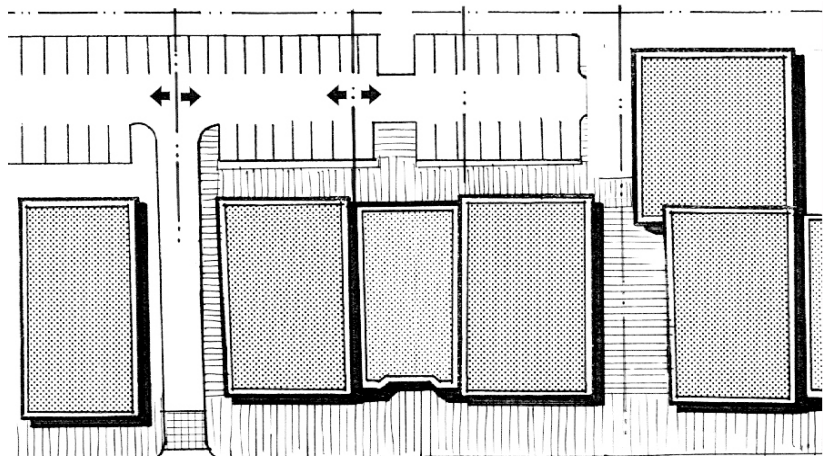


3.5.10 Off-Street Parking Design

3.5.10.1 Parking Lots/Surface Parking

- Landscaping and other architectural features should be used to create attractive buffer and screening between pedestrian pathway and the parking area.
- To avoid large expanses of paved areas, large parking lots should be divided into smaller parking areas. However, Multiple access points to parking areas should be avoided in order to minimize driveways along the street. This in turn minimizes traffic safety conflicts, street congestion, and unnecessarily disrupted street frontage.
- Adjoining properties are encouraged to share access driveways to minimize the number of driveways along public streets.

Shared Access and Shared Parking are Encouraged. SOURCE: Michael Baker International



- Side streets are encouraged to be used for ingress and egress to allow for smoother flow of traffic
- The driveways should be located as far from intersection as possible. However, the location of the driveway is based on available street frontage, site design, expected use of driveways, and traffic safety.
- Traffic calming features within parking lots, including but not limited to speed humps, are encouraged to enhance pedestrian safety in parking areas.
- Connections between parking facilities and building entrances should be considered and enhanced to provide a pleasurable experience and transition from parking lot to building entrance. This includes the use of canopies or other shade structures, landscaping and protected pedestrian facilities.
- Pedestrian walkways should connect parking lots to main building entrances. Enhanced colors and texture materials should

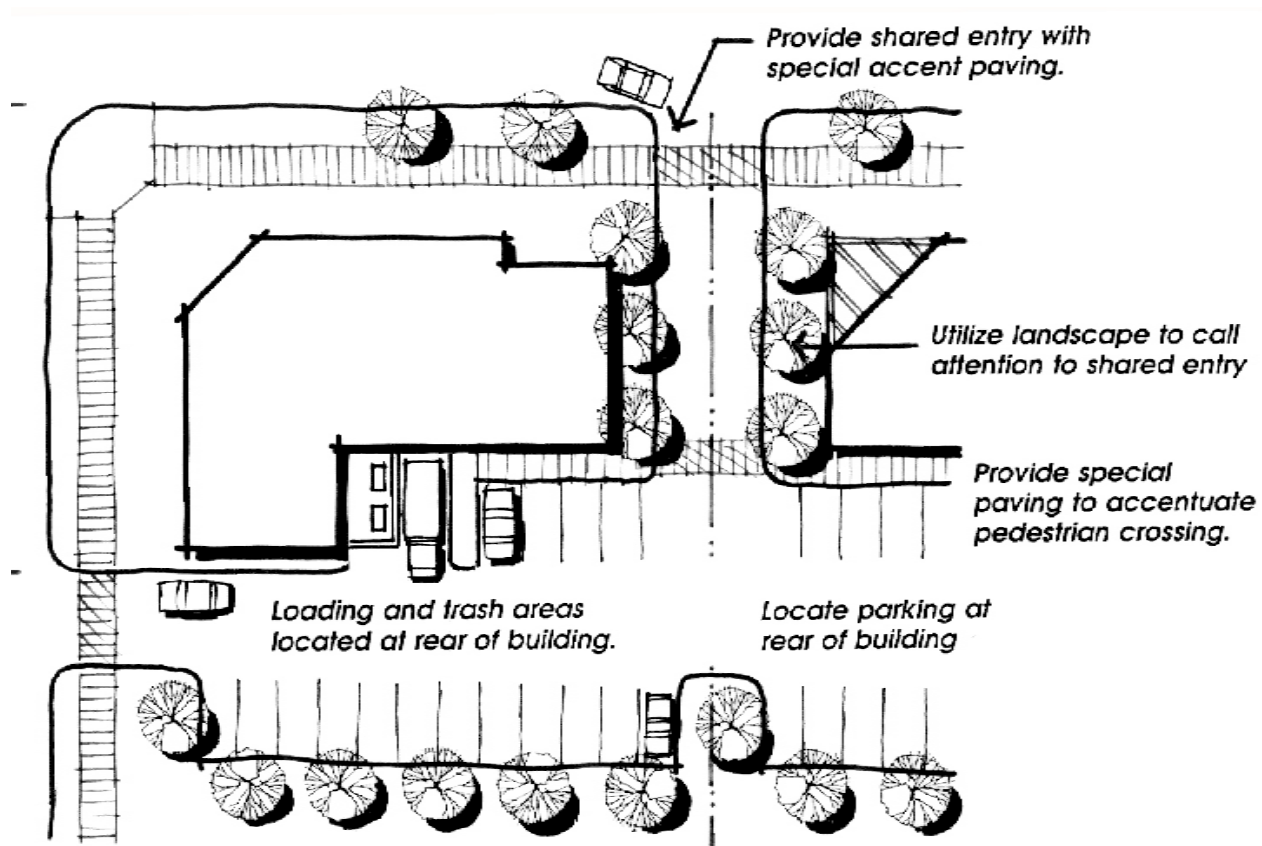
Well-designed Pedestrian Access in Parking Areas. SOURCE: Pi.1415926535



be incorporated into walkway or sidewalks within parking areas. Landscaping should be used to make the pedestrian experience pleasurable.

- Patterned pavement and integral colored concrete are encouraged, especially in areas that will have multiple uses, access from parking lots, and in other areas visible to the public.

Best Practices in Design of Parking Areas. SOURCE: Michael Baker International



3.5.10.2 Parking Garages

- Parking structure design must consider architectural compatibility size, scale, and bulk as it relates to adjacent properties and the overall design of the project.
- The first floor of parking garage should be programable if the garage abuts the primary street in the subarea so as to encourage pedestrian activity on the street.
- If the first floor cannot be programable the walls of first floor should be articulated per the architectural style of the primary structure and/or screened and landscaped to create a pedestrian-friendly environment.

Using Articulation and Landscaping of Non-Programmable First Floor. SOURCE: Top-MTA of the State of New York; Bottom-Nyttend



- Exterior elevations of parking garage shall draw architectural design cues from primary building and incorporate design components and materials utilized and compatible with the primary building(s). Long expanses of shear walls are not permitted.
- The exterior elevations of the parking structure should exhibit horizontal rather than sloping design elements.
- Landscaped planters are encouraged to be incorporated with the garage design
- Landscaping guidelines as described in Section 4.4 should be used to provide screening.

Programmable First Floor Space of Parking Garage. SOURCE: Michael Rivera



3.5.10.3 Bicycle Parking

- Bicycle parking may be placed as close to the entrance of a building as possible or may be organized into larger racks in an easily accessible location.
- It is highly encouraged that bicycle racks contribute to art installations. If unique bike racks that contribute to art installations are not provided, bicycle parking racks should be of the following types: Inverted “U”, “A”, and Post and Loop.
- The amount of bicycle parking facilities that are provided should be consistent with Section 12.40.050 of the City of Coachella Municipal Code.

Artistic Bike Racks- Creates District Identity. SOURCES: Left- Agremon; Right- Jim.henderson



3.5.10.4 Landscaping, Buffers, and Aesthetics

- Landscape buffering should be included as preferred in the landscaping guidelines in Section 4.4.
- Shade trees and drought-resistant planting should be planted in and around parking lots to reduce the heat island effect and offer visual relief in accordance with landscaping guidelines in Section 4.4.
- The landscaped islands are encouraged to use LID techniques/ bioswales to reduce storm runoff.

Shade Trees and Landscaping in Parking Areas. SOURCE: FASTILY



3.5.10.5 Parking Area Lighting

- Parking areas should be done in a consistent, attractive and unobtrusive manner that minimizes off-site impacts. The lighting fixtures should complement the street lighting fixtures.
- Lighting should use LED luminaires to reduce energy consumption and greenhouse gas emissions.
- Lighting should be dark skies compliant and use full or partial cutoffs to reduce light pollution.
- Adequate light should be provided for the safe movement of traffic and pedestrian but avoid glare on adjacent uses.

3.5.10.6 Pedestrian Safety

- Safe and highly visible pedestrian access should be provided from parking lots and garages to structures.
- The walkways should be minimum 6 feet wide and comply with ADA [Americans with Disabilities Act] regulations.
- Traffic calming features, including but not limited to speed humps and high visibility crosswalks may be used to enhance pedestrian safety in parking areas.

Painted Walkways and High Visibility Crosswalks. SOURCE: thisisbossi

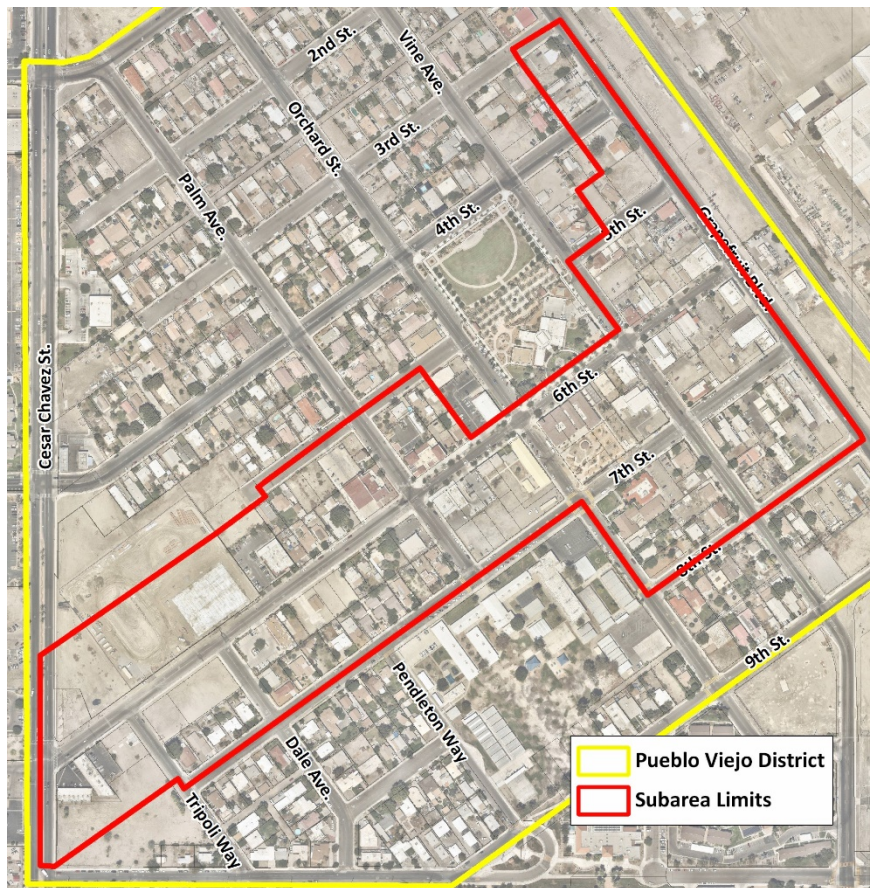


3.6 SIXTH STREET MIXED USE SUBAREA

The Sixth Street Subarea comprises parcels along Fifth, Sixth, and Seventh Streets and includes the Post Office area as well as the area surrounding intersections with Grapefruit Boulevard and Cesar Chavez Street (Refer to Map B).

Sixth Street is envisioned as a pedestrian-friendly horizontal and vertical mixed-use street with pedestrian-scale architecture. The interactive commercial uses on the ground floor and the articulation of buildings will make walking a pleasurable experience. Colonnades and arcades are encouraged as an extension of the building while maintaining harmony with adjacent buildings. These structures also provide much-needed shade. Plaza spaces that create mini gathering areas boost pedestrian activity and are encouraged. Buildings are expected to follow the architectural styles explained in Section 3.3. Architectural styles other than those described in the Section 3.3 may be allowed but are subject to more scrutiny. The City will require visualizations.

Map B Sixth Street Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



The buildings on Fifth and Seventh Streets also promote horizontal and vertical mixed-use development, though multifamily residential structures with no ground-floor commercial are also allowed.

Unless specified, the Sixth Street mixed use guidelines apply to structures oriented on Fifth, Sixth, and Seventh Streets.

3.6.1 Existing Buildings

Certain existing buildings along Sixth street enforce the feeling of a downtown district with small setbacks and pedestrian-oriented entrances. Preservation, restoration and adaptive reuse of these buildings is especially encouraged, where existing buildings. Refer to Section 3.5.1 for guidelines related to existing buildings

Preserve Existing Structures that Enforce a Feeling of Historic Downtown. SOURCE: Michael Baker International

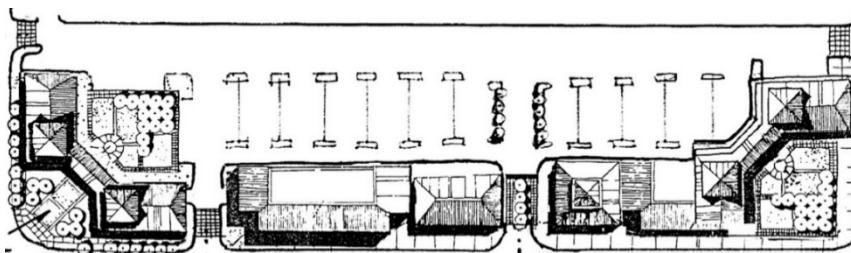


3.6.2 Site Design

3.6.2.1 Building Orientation and Setting

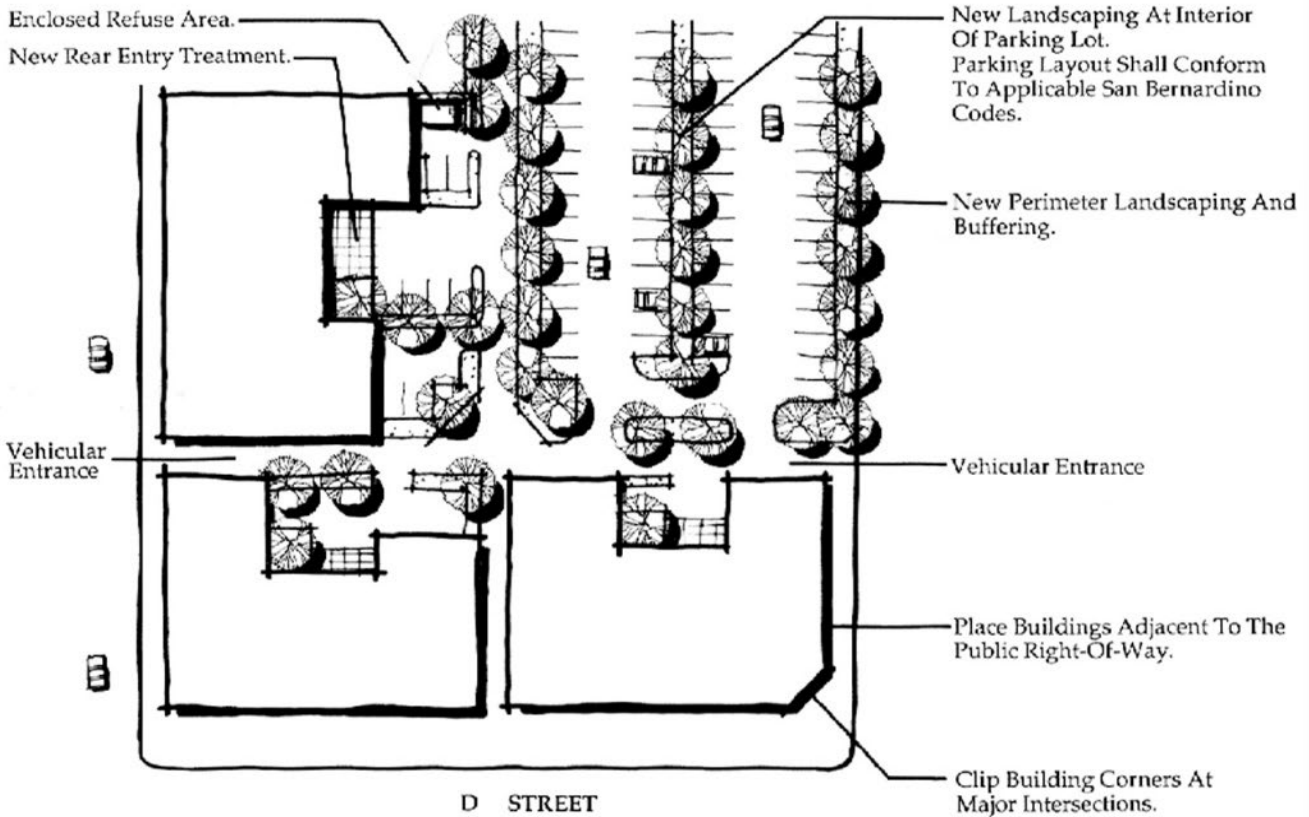
- Building orientation facing the primary street is important for creating street appeal, enhancing aesthetics, and drainage considerations and encouraged.

Building Orientation Towards the Street. SOURCE: Michael Baker International

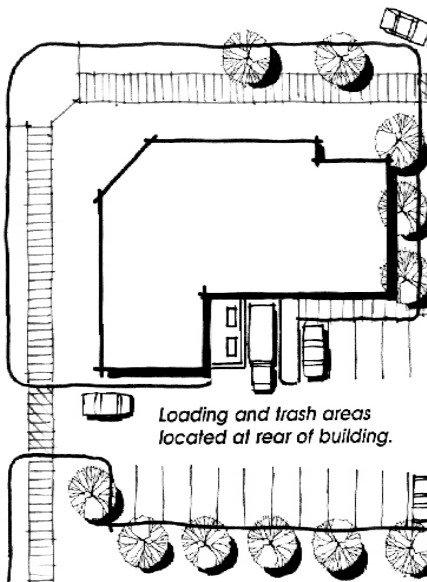


- When possible, the entry to parking and loading areas should be from the side streets. This orientation will help reduce the number of driveways on the primary street and create a pedestrian-friendly environment.

Typical Building Site Setting for Sixth Street Subarea. SOURCE: Michael Baker International

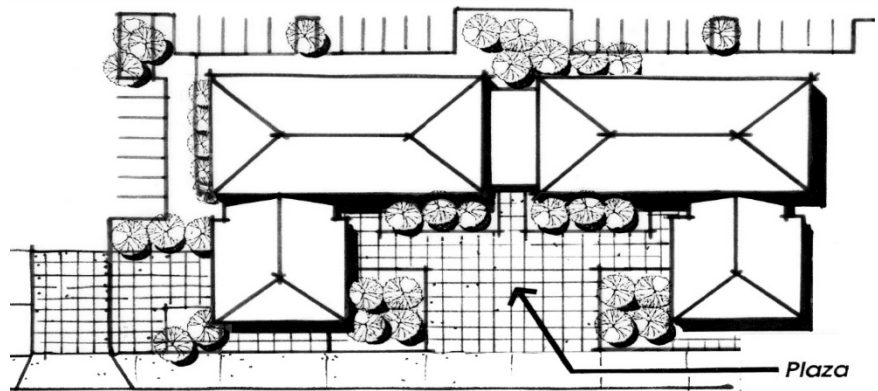


Loading Areas Bend the Buildings. SOURCE: Michael Baker International



- Parking and loading areas should be located at the rear of the buildings.
- Plazas, outdoor dining, or other open spaces should be oriented toward the street (see Section 3.6.4.1, Plazas, Open Space, Paseos, and Outdoor Dining).

Public Gathering Areas Orientation Towards the Street. SOURCE: Michael Baker International



3.6.3 Architecture

3.6.3.1 Massing and Articulation

- Buildings should use four-sided architecture, meaning all visible sides of the building should have façade treatment. However, the primary entry will be on the primary street.

Use of four-sided Architecture. SOURCE: M.O. Stevens

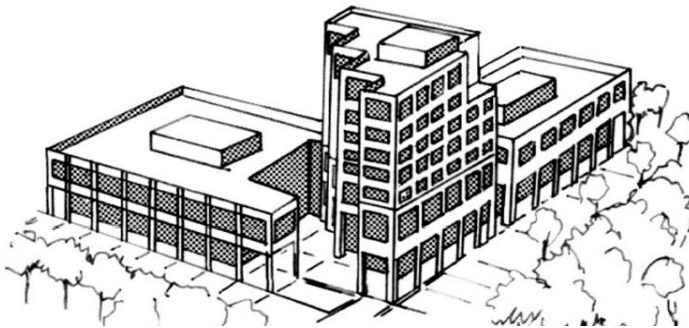


- Where the side walls are required to be blank for architectural or structural reasons, and are visible from the street, the developer should partner with the City for mural installation in accordance with the City’s mural program. See Section 3.10, Signs, Murals, and Public Art.
- Changes in height, materials, articulation style, massing, rooflines, and so on break the monotony of the façade and create interest for pedestrians. These changes should occur often (every 25 to 35 feet) along a building façade to retain pedestrian interest. Larger projects should be designed to reflect a small-scale street frontage.

Painted murals on blank walls help with placemaking. SOURCE: Thomas R Machnitzki



Changes in height. SOURCE: Michael Baker International



Changes in articulation. SOURCE: Michael Baker International



- Any articulation should comply with the maximum setback limits provided in the Zoning Ordinance.

Pedestrians Observe Details. SOURCES: Top-Tim Kiser; Bottom- Stanley Walker



3.6.3.2 Articulation and Detailing

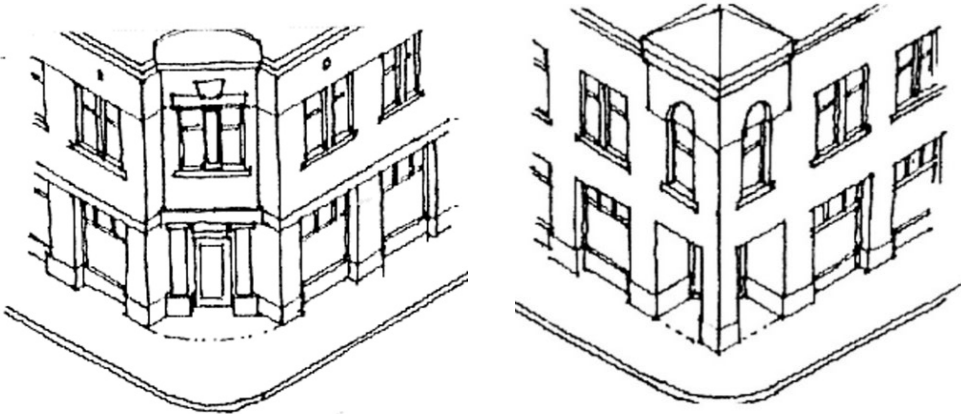
Pedestrians move slower than vehicular traffic and are more likely to observe articulation and smaller details. In order to retain pedestrians’ interest and make walking a pleasurable experience, buildings should have architectural elements that are detailed to be appreciated by the pedestrians and authentic representations of architectural styles. Variation in materials and colors and/or use of cornices, decorative columns, and other features are different ways of providing articulation.

- Depending on the architectural style, articulation or framing around the windows and doors using varied materials or colors can create interest in building façades.
- On façades that do not include entrances, windows, or display windows, articulation using vertical and/or horizontal elements, should be used on the façade to add visual interest. Applicant should refer to the Articulation and Decorative Elements and Windows and Doors subsection under Allowed and Encouraged Architectural Styles in Section 3.3 depending on the architectural style of the project
- All facades should emphasize three-dimensional detailing depending on the architectural style used. Such detailing provides depth and play of shadows creating visual interest on the façade.

3.6.3.3 Entry Orientation

- Primary entries should be on the primary street. The entry should be accentuated to make it visible from the street, depending on the architectural style.
- Entries oriented toward Sixth Street should be integrated with the colonnade or arcade structures.
- Entries providing access to uses on the upper levels should clearly visible from the street and integrated with the ground-floor façade to highlight the access location.
- Corner lots at the intersection of Streets are encouraged to have corner entries thus enabling entry from both streets while maintaining single point of entry.
- Development on corner lots should make a strong architectural statement and place more emphasis on the building corner.

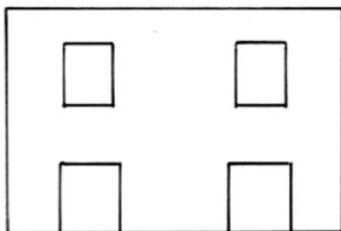
Corner Buildings, Strong Architectural Statement, Corner Entries. SOURCE: Michael Baker International



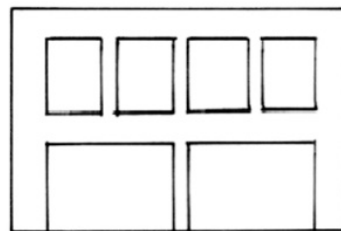
3.6.3.4 Window Design

- Windows should be used as architectural elements that add relief to the façade and wall surface while being true to the architectural style.
- The window design can be modified on upper levels to allow for the functionality of the space as intended. However, it should follow the rhythm established by the ground floor articulation.

Windows as Architectural Element. SOURCE: Michael Baker International



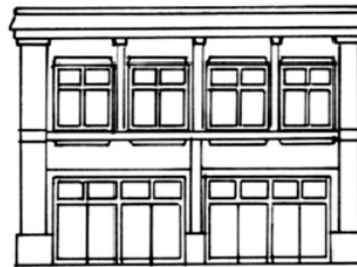
Proportion of opening sizes to building mass is too small



Increase opening sizes

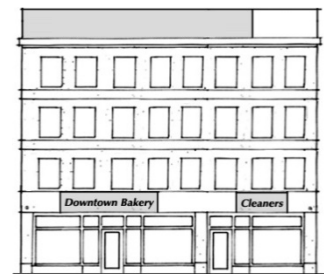


Articulate openings

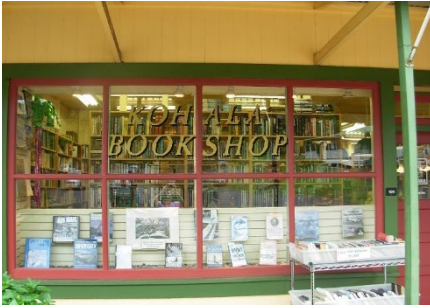


Break up building mass

Modified Upper Level Windows. SOURCE: Michael Baker International



Display Window- Providing Visual Access to the Interior. SOURCE: Brewbooks from near Seattle



Display Window- Providing No Visual Access to Interior. SOURCE: Editor5807



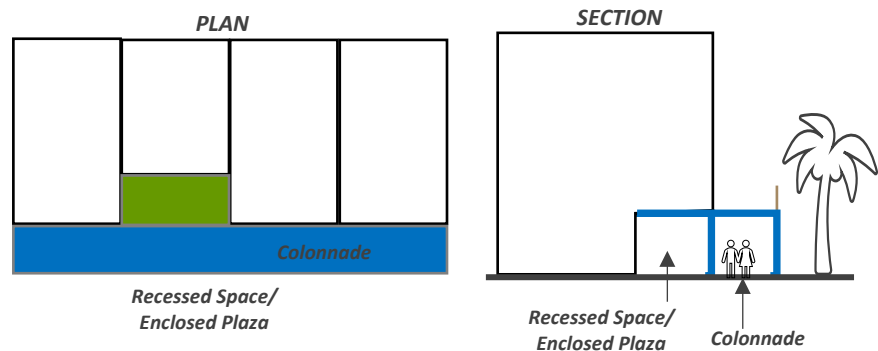
- Commercial storefronts should include street-oriented display windows in order to engage pedestrians. These windows should provide visual access to the building interior while also serving as an area for merchandise display.
- Enclosed display windows that provide minimal to no visual access to storefront interiors should be included where actual windows cannot be provided due to the nature of the building’s use.
- Exceptions to window display requirements can be made where display windows (open and enclosed) are not feasible. In this case, exterior walls should be designed to offer architectural relief or be screened by landscaping and designed to provide pedestrian amenities such as wider sidewalks or benches.
- Clear glass windows are encouraged especially for retail use to promote interaction between pedestrians and the businesses and to provide light and views into store interiors. The tinting of glass should be no more than 20% on all windows. Highly reflective window film is prohibited as the application can interfere with moving traffic.
- For both commercial and mixed-use buildings, real windows that face the street should be provided on the second floor. In mixed-use buildings, the second-floor windows should be fully functional and able to be opened.

3.6.3.5 Porches, Balconies, and Decks

3.6.3.5.1 Porches

- Porches are not conducive to the Main Street style of the Sixth Street Subarea. Instead, a continuous colonnade structure spanning entire blocks is desired. See Section 3.6.3.6 for Shade Structures. However, enclosed plaza spaces or recessed porches are allowed.

Commercial Uses Oriented Toward Sixth Street. SOURCE: Michael Baker International



- Recessed spaces are encouraged for buildings on Fifth and Seventh Streets as well as for buildings oriented on minor streets. Such a space should be designed to be consistent with the architectural style of the building to which they are attached and have sufficient depth and width to be occupiable spaces.

3.6.3.5.2 Balconies

- Window balconies if compatible with architectural style are encouraged on the upper floors if they do not interfere with street uses or detract from the common building line or patterns.

Window Balconies on Upper Floor. SOURCE: Pixie



- Faux balconies with windows are acceptable provided they integrate features from the architectural style of the building.
- Refer to Zoning Ordinance for setback considerations related to Balconies.
- When balconies on the upper floor run the entire length of the façade, they should be integrated with a colonnade structure on the ground floor. The depth of such balconies should not be more than the depth of the colonnade structure and should be made of lighter materials to give a feel of openness and maintain the visual continuity of the main building structure.
- Where possible, balconies should be oriented toward the street and not toward internal block parking areas.

*Lower and Upper Level Deck Examples.
SOURCES: Top- Jonathan Billinger;
Bottom- Aleniromswit*



*Examples of shade structure for decks.
SOURCE: Top- MB298; Bottom- Dan
Rademacher*



3.6.3.5.3 Decks and Patios

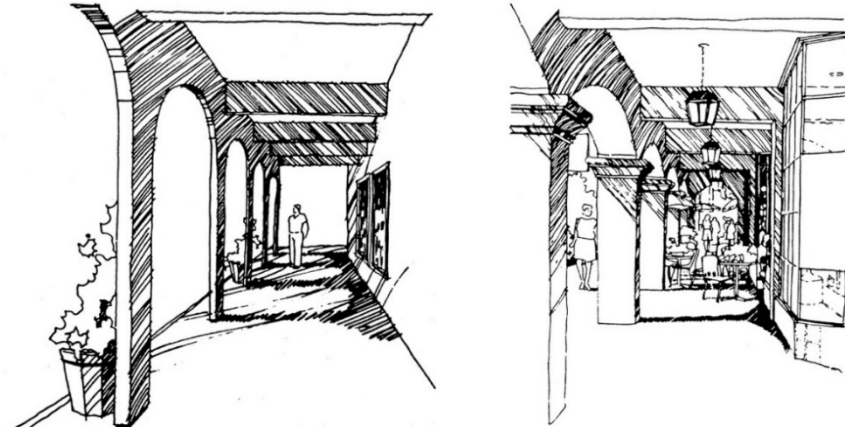
- Decks and patios are encouraged as they provide outdoor gathering/ dining spaces and activate the street life. Refer to Zoning Ordinance for allowable size, setback, and location of patios.
- Decks and patios should be ADA accessible
- Decks should have sufficient depth and width to be occupiable spaces.
- Upper level decks should have a minimum parapet of 36 inches. Though encouraged, parapets are not required if decks and patios are 30 inches above grade or lower.
- The parapet and other articulation of the deck structure should draw from the architectural style of the building or should be designed in away so as to not interfere with the character of the building.
- Decks should be made with durable materials with low heat absorption and should take climatic conditions into consideration. Some suggested materials are natural wood such as redwood, cedar and pressure-treated wood, Composites composed primarily of wood fibers and recycled plastic and Aluminum.
- Decks should be maintained in usable condition free from chipping, cracks, and tripping hazards. Staining and coloring should be done per manufacturer’s instructions on a regular basis.
- Decks and patios are encouraged to use shade structures in order to provide relief from harsh desert climate and rain. These structures can be such as Pergolas, Trellis, Tension membrane structures (TMS) or shade sails, retractable devices, such as canvas awnings or louvres.
- The shade structures should be maintained regularly. If such structures become unsightly, they should be replaced immediately.

3.6.3.6 Shade Structures

3.6.3.6.1 Colonnades and Arcades

- Shade structures such as colonnades and arcades are encouraged, especially on Sixth Street. Such structures should incorporate architectural features from the building’s style while ensuring the visual continuity of the walkway.

Colonnade Should Complement Architectural Style. SOURCE: Michael Baker International



These columns are much too thin. Columns with mass and integrity.

- On corners, colonnades or arcades may wrap around the side of the building facing the side street.
- The space above colonnades can be used as occupiable space.

3.6.3.6.2 Awnings

Awnings are not permitted on the first floor of buildings on Sixth Street Subarea. Awnings may be used on the upper floors to provide shade. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.6.4 Outdoor Gathering Areas

The outdoor gathering areas include paseos plazas/open spaces, courtyards, and outdoor dining areas. These areas give vitality and vibrancy to the place.

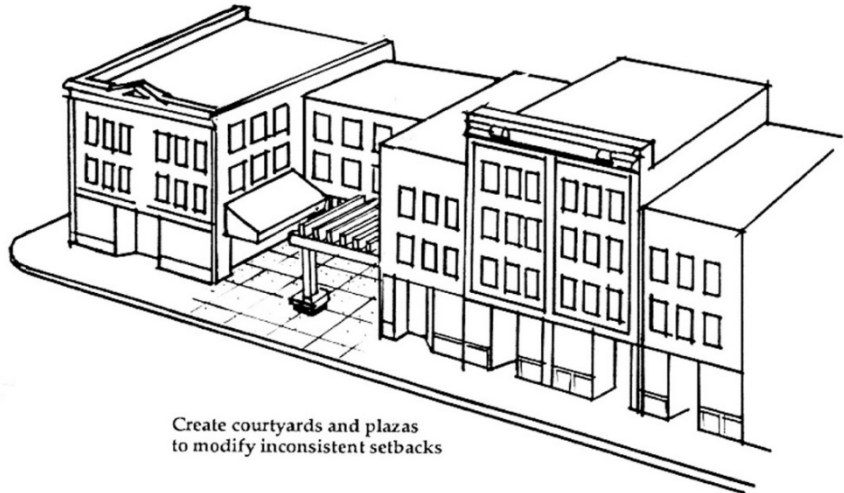
- **Paseos** are outdoor passages dedicated to pedestrian use. They establish connection between streets, parking areas, plazas and courtyards, and building entrances. These areas can be used for spillover dining and gathering spaces.
- **Plazas** are common open spaces that are readily accessible from the street. They provide for large gathering spaces. They can be integrated within single building design or can be created with the placement of 3 or more buildings to form a shared open space.
- **Courtyards** are similar to plazas but at smaller scale.
- **Outdoor dining area** are generally within the setback of the building and separated from sidewalks using a divider such as a

fence. They are generally used by people patronizing the business.

3.6.4.1 Plazas, Open Spaces, Gathering Areas

- Plazas, Open Spaces, Gathering Areas usable by the general public should be physically and visually connected to the public sidewalk.

Plaza Oriented Towards the Street. SOURCE: Michael Baker International



Placemaking Elements Example for Plaza Area. SOURCE: Charvex



Shade Structure Example for Plaza Area. SOURCE: Sekretärin



- Plazas, Open Spaces, Gathering Areas are should incorporate attractive landscaping.
- Plazas are encouraged to be designed to give an inviting feel using high-quality hardscape and focal elements such as water features public art, and murals.
- Use of Interesting shade structures is encouraged but such structures should not prohibit the penetration of sunlight to the ground floor or obstruct view of the storefronts.
- Escalators or elevators should not dominate the function and appearance of the plaza.
- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Plazas developed as a part of residential development on the ground floor can remain private.

3.6.4.2 Paseos

- Paseos should connect interior spaces such as courtyards and parking to the primary street also in order to provide a pedestrian the ability to pass through the block without walking around it.

- Vehicular access, loading, or parking uses are prohibited within the paseo except during restricted hours as determined by the City.
- Paseos should be wide enough to feel safe for a pedestrian. Narrow spaces, even when lit, will tend to not be utilized. Increasing a Paseo's size can also allow for multiple uses such as dining and gathering spaces.
Paseos may be used as outdoor dining areas. However, they should be wide enough to accommodate a clear walking path for pedestrians and provide for ADA accessibility.
- Entries to the paseo, and storefront entries within the paseo, shall be designed and lighted so they do not create hiding places.
- Lighting should be provided from overhead fixtures, either mounted on the building facade or suspended from cables spanning the paseo so they do not obstruct the free-flow of pedestrians.
- Murals, sculptures, artwork, archways, water features, landscaping and other features that help with placemaking by creating interest for the users and thereby activating space in paseo areas are encouraged. These placemaking elements should be compatible with the architectural style of the building.

Active Paseo Space. SOURCE: Martin Falbisoner



Example of Active Paseo Area Connecting Two Streets. SOURCE: Comercio y Justicia



Example of Sidewalk Dining Areas in Frontage Zone. SOURCE:



Example of Sidewalk Dining Areas in Furnishings Zone, SOURCE: Runner1928



3.6.4.3 Sidewalk Dining Areas Fences and Barriers

- Fences and barriers are required to demarcate the outdoor gathering areas when they are in Frontage Zone (area between pedestrian walkway and the property line), so they do not encroach cause obstruction for pedestrians and provide a clear walkway for passersby.
- Fences may not be needed on pedestrian side if the outdoor dining is in the Furnishings Zone (area between pedestrian walkway and street. However, fence will be required on the street side of dining area.
- Fences and barriers shall be removable at any time if required; however, they shall be properly secured when the corresponding business is closed or not in use.
- This fencing or barrier should be semitransparent and architecturally compatible with the building’s architectural style. Refer to Zoning Ordinance for maximum allowable height of the fence or barrier.
- Fencing materials should be of good quality and durable, such as wrought iron, steel, or wood and be compatible with the architectural style of the building.
- No fence or barrier shall have any sharp or pointed pickets that could cause injury. All finishes should be clean and free of any exposed screws or other fasteners.
- Fences are encouraged to incorporate planter boxes that are easily removable. The planters should be and should be maintained at all free of dead plants. The plants should not exceed the height of the fence. Artificial plants are discouraged.

Example of Sidewalk Dining Areas in Frontage Zone. SOURCE: Will Buckner

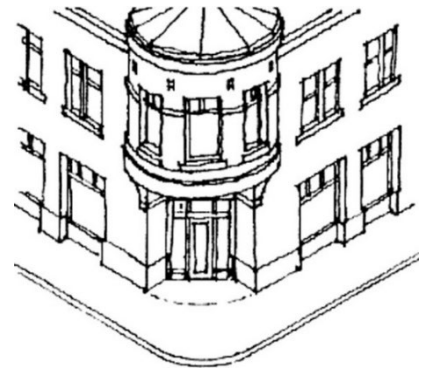


3.6.5 Buildings at Intersections

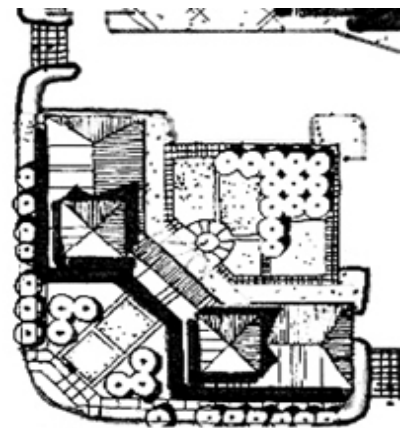
The buildings at the intersections of two streets are the most visible and have a greater effect on passersby. They provide entry to the street; thus, more articulation is desired. The architectural and other guidelines above are also applicable to the buildings at the intersections of Sixth Street with other major or minor streets. However, it is expected that these buildings will be treated as gateway buildings, especially at major intersections—Cesar Chavez Street and Grapefruit Boulevard.

- Several ideas are listed below for the corner buildings that will help create the gateway effect.
 - Buildings can be higher than the rest of the buildings on the street/ block.
 - Buildings can be recessed to create a small plaza space and include art, a fountain, or a focused landscaping feature.
 - Buildings can have a corner element appropriate to the architectural style, such as a bell tower.
 - Buildings can creatively use projections on the façade at the corners to emphasize the effect of the entryway.
 - Materials or colors could be varied at the corner, depending on the architectural style.
- A greater degree of ornamentation or articulation is desired appropriate to the architectural style compared to the rest of the structure.
- The corner buildings are encouraged to have corner entries to enable entering the building from both streets.

Greater Degree of Ornamentation and Projection of Corner Element. SOURCE: Michael Baker International



Create a Small Plaza Space at the Corner Entrance. SOURCE: Michael Baker International



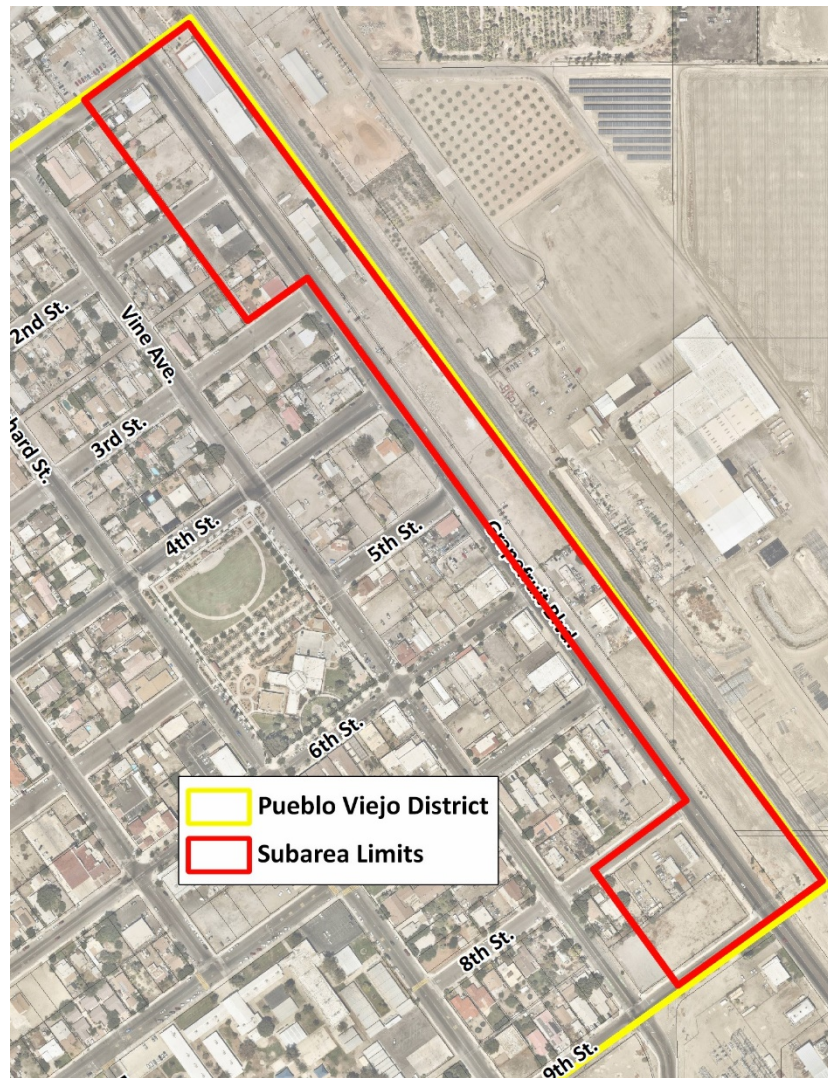
Greater Degree of Ornamentation and Projection of Corner Element. SOURCE: M.O. Stevens



3.7 GRAPEFRUIT BOULEVARD SUBAREA

Grapefruit Boulevard Subarea includes the parcels along highway 111 in the Pueblo Viejo District. These parcels have highway oriented light industrial businesses ranging from automobile repair, auto sales, gas stations, small stores and offices. Most of these businesses are expected to remain in place. The new buildings are also envisioned to be industrial or highway commercial making Grapefruit Boulevard a vehicle-oriented commercial corridor with automobile-scale buildings. Hence, the guidelines related to scale, massing, articulation and signage will be suited to an auto-centric setting. However, it is also expected that the development of out-lots, vacant parcels and adaptive reuse of the structures could lead to development of uses such as bars and restaurants and hence, pedestrian-friendliness is a necessary part of the

Map C Grapefruit Boulevard Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



guidelines. The goal of design guideless for this area focus on creating a cohesive appearance using certain common elements and a unique look by incorporating colloquial architectural elements and art and history that relates to Coachella.

Grapefruit Boulevard between First and Ninth Streets should stand out from the majority of this thoroughfare and encourage passing automobiles to stop and enjoy all that Pueblo Viejo District has to offer. The intersection of Sixth Street and Grapefruit Boulevard is one of the three Gateways to Pueblo Viejo District and hence should be designed to reflect that. Refer Section 3.6 for Sixth Street Subarea guidelines. Buildings are expected to follow the architectural style guidance as described below.

3.7.1 Existing Buildings

Certain buildings on Grapefruit Boulevard though automobile oriented, have unique historic industrial characteristics and are encouraged to be restored and/or adapted to newer uses and/or additional uses. Some of the existing buildings if adapted to newer uses can result in outlots that can be developed with additional compatible uses. Preservation, restoration and adaptive reuse of these buildings is encouraged. Refer to Section 3.5.1 for guidelines related to existing buildings.

Unique Architectural Features. SOURCE: Michael Baker International



3.7.2 Site Design

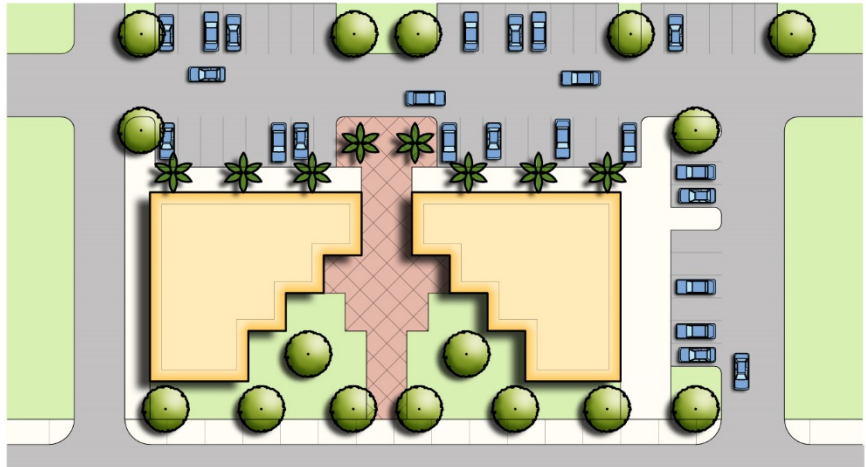
3.7.2.1 Building Orientation and Setting

The larger lot sizes and automobile-oriented uses along Grapefruit Boulevard should be designed in a manner that fits in with the surrounding development pattern and context. However, ensuring that the new buildings establish a more appropriate development pattern is also necessary.

- Primary façade of buildings should be oriented toward Grapefruit Boulevard; Developers are encouraged to treat side facades visible from the street with similar design principles as they treat primary facade.
- Buildings should not be located in a manner that make them appear like “islands” surrounded by paved areas.
- As much as possible the office portions or pedestrian oriented uses should be oriented toward Grapefruit boulevard to create a pedestrian friendly environment.
- Developments should include usable outdoor open space (courtyards, plazas, shaded arcades and functional landscaped areas) whether located in setbacks or other areas. These could also be used as outdoor employee break/lunch areas.

- Mature trees and tree groupings, and significant vegetation should be preserved and incorporated into development's usable open space where possible. Such features should be considered as strong site design determinants.
- Where industrial uses are adjacent to non-industrial uses, appropriate buffering techniques such as setbacks, screening, effects of industrial operations.

Pedestrian Friendly Building with Parking Behind the Building. SOURCE: Michael Baker International



Pedestrian oriented building parking in the front



Examples of Pedestrian Friendly Automobile Oriented Commercial/ Light Industrial Buildings. SOURCE: Michael Baker International



3.7.3 Architecture

3.7.3.1 Massing and Articulation

The automobile-oriented commercial uses and light industrial structures along Grapefruit Boulevard are expected to have larger building footprints. The larger the mass of a building with unbroken building walls and rooflines, the larger and bulkier it will appear. While faster moving traffic might not need building mass to be broken to the extent that a pedestrian oriented corridor such as Sixth Street might need, it should

not be forgotten that the Grapefruit Boulevard is also envisioned to house pedestrian oriented uses such as bars and restaurants and hence, there should be a balance between automobile-centric and pedestrian centric design with respected to massing and articulation. Bulkiness of buildings can be reduced through several methods that break massing and incorporating articulation. Guidelines include, but are not limited to:

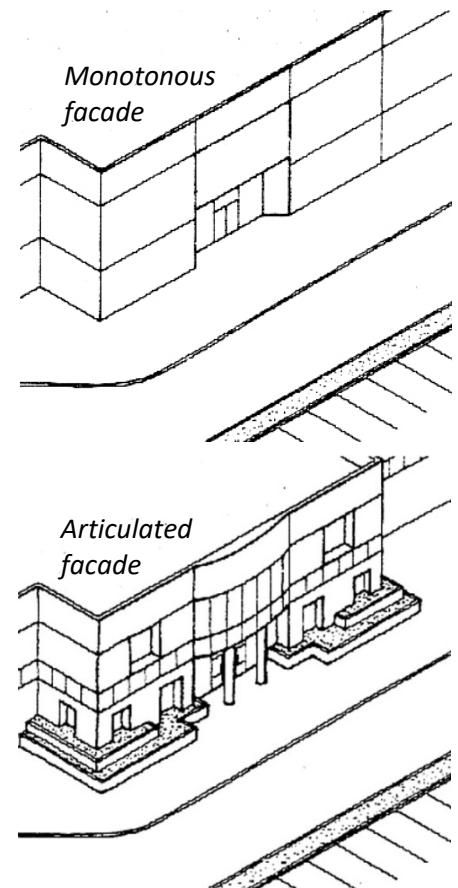
- At an intersection with a minor street, the side of the building facing the minor street should have a façade treatment.
- Developers are encouraged to uses techniques such a recessing building floors and changing rooflines to break the mass of the building
- Breaks in building mass should be used to provide visual relief for long building facades. Vertical or horizontal offsets in the wall surfaces at regular intervals, including columns, projections, and recesses, depending on the architectural style of the building helps with breaking the monotony as well as mass of the building. Such treatments are encouraged every 30 to 50 feet
- Applicants are encouraged to incorporate other structures on the site instead on a single dominant building
- Articulating details around doors, windows, balconies and other openings help with breaking the monotony of the façade and are encouraged
- Building façade design oriented towards Grapefruit Boulevard should avoid long, repetitive, monotonous facades – particularly those that repeat the same design element several times along the same elevation is
- Façade design should incorporate play of colors and materials depending on the architectural style of the building to break the monotony of façade

The collective architecture of buildings along a street defines the character of the corridor and gives it an identity. Developers are encouraged to draw cues from existing light industrial buildings, automobile repair shops and other existing structures along Grapefruit Boulevard that have distinctive architectural elements and style and in cohesion can create a unique industrial architecture inspired corridor.

3.7.3.2 Entry Orientation

- Primary entrances should be oriented toward Grapefruit Boulevard. Warehousing and industrial uses may have secondary entrances on the sides or rear of the building.
- Primary building entries should be readily identifiable by the moving traffic and well defined through the use of projections, recesses, columns, roof structures, or other design elements

Changes in Materials/ Colors and Using Articulation Interest and Breaks the Monotony of Façade. SOURCE: Michael Baker International



Prominent Entry Feature. SOURCE: Dfadden



based on the architectural style. The single massing break provided by the entry in long blank façade is not acceptable.

- Entries to the upper floors if used by different business offering should be readily visible by the moving traffic.

3.7.3.3 Door and Window Design

- Windows and doors are key elements of any structure's form and should relate to the scale of the elevation on which they appear and the Architectural style of the building. Windows and doors can establish character by their rhythm and variety.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- The tinting of glass should be no more than 20% on all doors and windows. Highly reflective film is prohibited as the application can interfere with moving traffic.

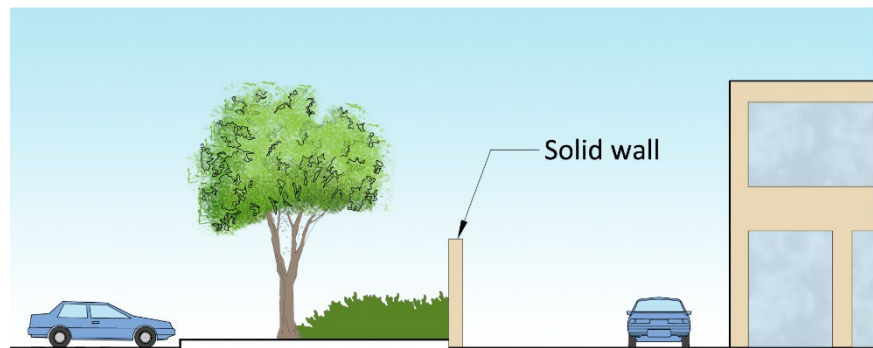
3.7.3.4 Awnings

- Awnings are permitted both on first as well as upper floors. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.7.3.5 On-site Screening

- Less visually aesthetic components necessary for industrial/commercial development such as loading areas, trash enclosures, mechanical equipment, and noise and odor producing functions should be located at the sides and/or rear of

Screening- Solid wall and Landscaping. SOURCE: Michael Baker International



main buildings, and screened with compatible architectural features and walls, and/or landscaping.

- Solid walls if necessary for screening should be no more than 6 feet high and made of solid masonry or concrete tilt-up with an

exterior finish compatible to the main structure. Landscaping should be used to reduce the starkness of such walls

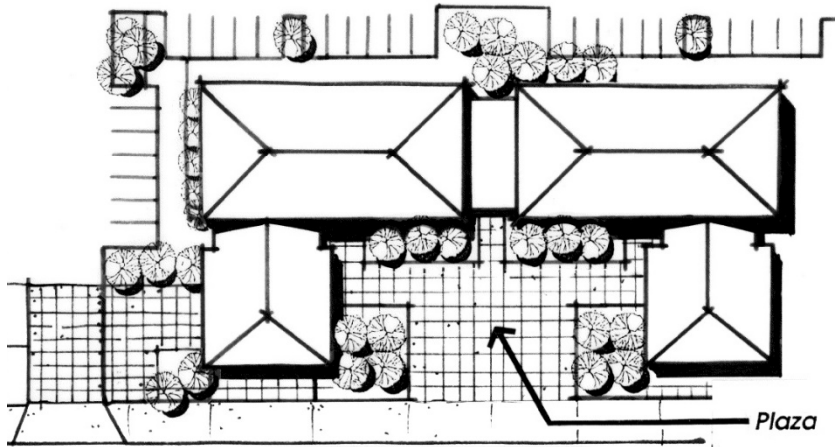
3.7.4 Off-Street Parking

- Parking lots should not be the dominant visual element of the site. Refer to Section 3.5.11 Off Street Parking guidelines in the General Provisions
- Developers are encouraged to locate parking on the sides or rear of the buildings.

3.7.5 Outdoor Gathering Areas

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- The plazas are encouraged to be integrated with building design and to give an inviting feel using elements such as public art, landscaping, interesting shade structures, murals, arched walls creating gateways, and so on.

Plaza Integrated with Building. SOURCE: Michael Baker International



Plaza Area Seating. SOURCE: Andy C



- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Pedestrian walkways should be provided to connect building entrances with parking areas. It is strongly encouraged that these walkway areas provide pedestrian improvements including but not limited to shade structures, benches and furniture, and dining areas, if appropriate.
- Plazas, open spaces, and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.

Pedestrian Walkways Connecting Parking to Entrance. SOURCE: Øyvind Holmstad



- Fences or barriers are encouraged to separate dining areas from open space and plazas where there is pedestrian traffic.
- Fences should be of good quality and made of wrought iron, steel, or wood. Other materials may be permitted if they are compatible with the architectural style of the adjacent building and the style of the dining furniture.

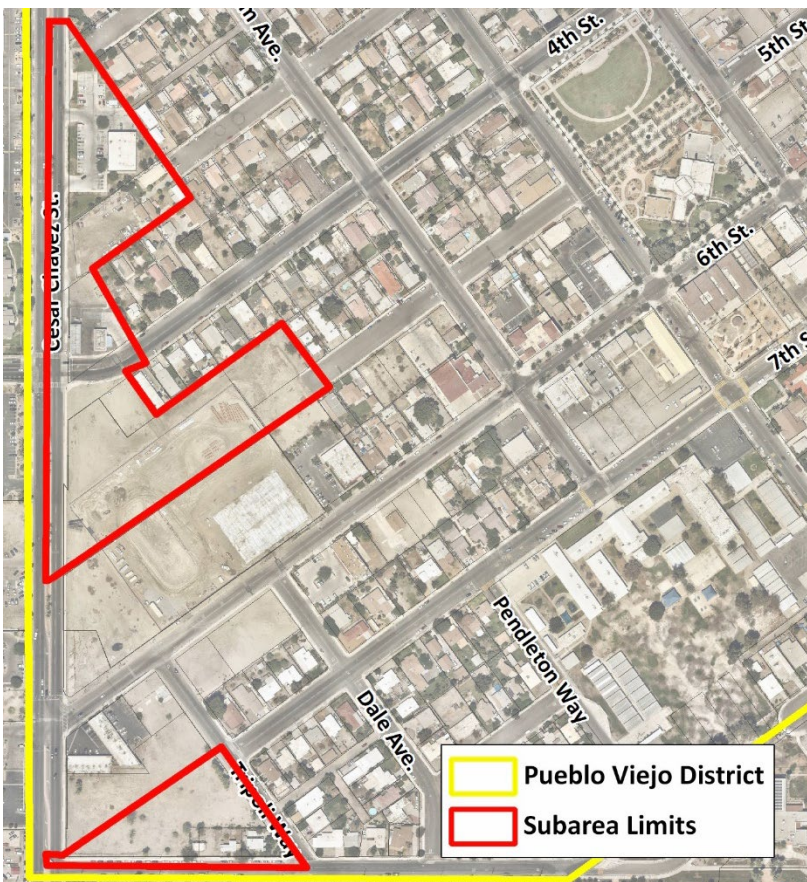
Plaza Space in Commercial Area. SOURCE: Payton Chung



3.8 CESAR CHAVEZ STREET SUBAREA

Cesar Chavez Street Subarea consist of parcels on the east side of Cesar Chavez Street as shown on Map D. The parcels on the southern side of the street are currently vacant while northern side houses auto-related uses such as gas station, AutoZone and so on. The rest of the corridor though not in the study area is developed in a conventional strip commercial style with single storied franchise stores and large expanse of parking. If unchecked the development of vacant parcels in this subarea as well as any redevelopment opportunity of existing built areas could follow the similar development patterns that is not conducive to pedestrian and bike traffic and will cater largely to automobile users. While uses along this fast-moving corridor are expected to remain primarily auto-centric, the areas are in immediate vicinity of Sixth Street and is positions for spillover effects and hence should be planned as a pedestrian and bike friendly corridor. Site design, massing, articulation, open space development and other placemaking elements play a role in creating such an environment that encourages pedestrian activity while catering to automobile users.

Map D Cesar Chavez Street Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



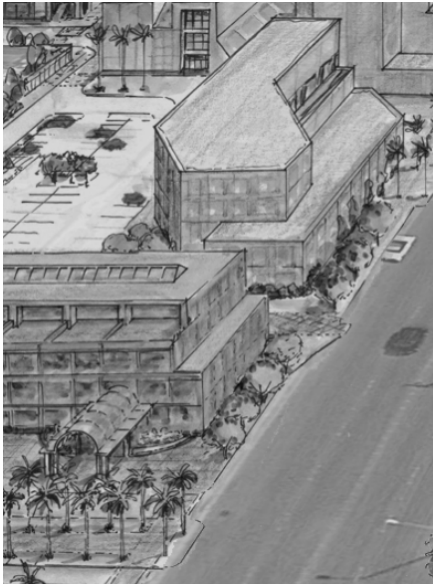
3.8.1 Existing Buildings

- Renovations and additions to existing structures in order to introduce new uses should be done appear as an originally conceived part of the design. New additions should match the current scale and proportions of the existing buildings. However, they need not replicate the existing structures. construction. Refer to Section 3.5.1 for guidelines related to existing buildings.

3.8.2 Site Design

3.8.2.1 Building Orientation and Setting

Buildings Oriented Towards Primary Street.
 SOURCE: Michael Baker International



- Primary façade of buildings should be oriented toward Cesar Chavez Street; Developers are encouraged to treat side facades visible from the street with similar design principles as they treat primary facade.
- Development design should not lead to creation of superblocks. A finer pedestrian friendly block design no greater than 400 feet in length is preferred. These internal routes should be designed to connect to existing street grid. Such blocks also provide increased street frontages on the internal through-routes of these centers create more walkable setting for new buildings and public open spaces.
- Internal streets should contain no more than two through-lanes and sidewalks leading to parking areas
- Buildings should not be located in a manner that make them appear like “islands” surrounded by paved areas. Shared parking concepts and developments encouraging “park-once-and-walk” experience is encouraged.

Orientation and Articulation. SOURCE Michael Baker International



- Organize buildings and the spaces between them to cultivate street life. The arrangement of the buildings should entice customers out into the public spaces between the buildings.
- Where surface parking is required for development feasibility, surface lots should be to the rear of primary building frontages

and configured to allow their conversion to structured parking as land values rise and development intensity increases. Walkways connecting these parking spaces to front of buildings can be doubled as open spaces or paseos that are strategically located to be visible from large portions of development as well as street.

- Developments should include usable and attractive outdoor open space (courtyards, plazas, shaded arcades and functional landscaped areas) located in strategic locations to attract and promote pedestrian activity. These locations can that connect parking to the front of the buildings and can also be used as outdoor dining areas, mini playground spaces and other activity generating uses.
- Mature trees and tree groupings, and significant vegetation should be preserved and incorporated into development’s usable open space where possible. Such features should be considered as strong site design determinants.

3.8.3 Architecture

3.8.3.1 Massing and Articulation

- At an intersection with a minor street, the side of the building facing the minor street should have a façade treatment.
- Other facades directly visible from the street should have façade treatment.
- The central portion of development as well as bookends should be designed to be visibly taller and denser than rest of the development as these are most visible from the street.

Dominant Central Portion and Bookends. SOURCE: Michael Baker International

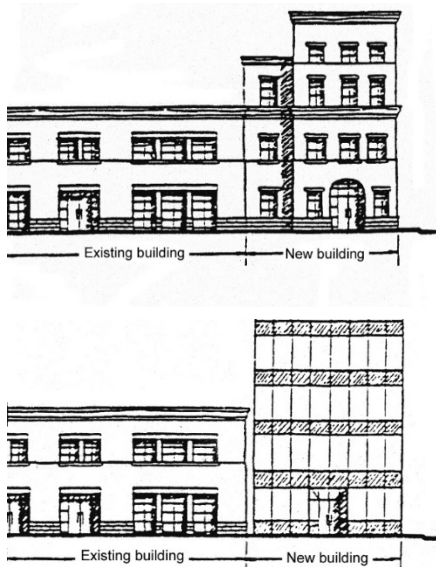


- Building design should avoid blank façades. A single, dominant building mass should not be used for larger buildings. Breaks in building mass should be used to provide visual relief for long building facades.
- In order to break the mass of a large store, consider expressing internal functions (i.e. bank, deli, and florist) as minor storefronts.

Articulation and Breaking the Facade. SOURCE: Michael Baker International



*New Expansion to Compliment Old Structure. (Image on the top shows addition using same architectural style. Bottom image shows different style but follows the banding thus complementing the existing building architecture).
SOURCE: Michael Baker International*



Franchise store Architecture Using Prescribed Architectural Guidelines.



- Incorporate two (or more) entrances along the front of large stores. If two entrances are not possible, consider partially wrapping the front of a large store with smaller stores. Wall planes should not run in one continuous direction for more than 30 feet without an offset or some form of articulation dependent on architectural style of the building.
- Substantial variations at massing breaks should include changes in height and the horizontal plane.
- Changes in materials, textures and the utilization of other architectural enhancements by use of vertical or horizontal architectural elements based on architectural style of the building that help break massing and create interest for passersby are also encouraged.

Frequent Changes in Façade. SOURCE: Michael Baker International



- Murals and landscape screening are greatly encouraged as they help in breaking mass of the building.
- Expansions to existing buildings should provide for continuity between the old building and the new addition. It is not necessary to match the existing building but should include prominent design elements of the old building to provide architectural compatibility between old and new.
- When using architecture of a franchise business such as fast food, it should be compatible with the neighboring buildings and should integrate features from architectural styles prescribed in Section 3.3. These franchise businesses should not be standalone structures but a part of overall development and should incorporate the architectural style of the overall development in their design.
- Buildings that derive their image primarily from applied treatments that express corporate identity are discouraged.

3.8.3.2 Entry Orientation

- Primary building entries should be readily identifiable by the moving traffic and well defined through the use of projections, recesses, columns, roof structures, or other design elements based on the architectural style. The single massing break provided by the entry in long blank façade is not acceptable.
- Entries to the upper floors if used by different business offering should be placed on the front of the building.

3.8.3.3 Door and Window Design

- Windows and doors are key elements of any structure's form and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety.
- Windows set back from the exterior wall surface to create variety and shadows in the wall planes are encouraged though the architectural style used will dominate the appearance, rhythm and design of these elements.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- Reflective glazing is prohibited.

3.8.3.4 Shade Structures

3.8.3.4.1 Colonnades and Arcades

- Shade structures such as colonnades and arcades are encouraged as part of internal site design to provide shaded walkways within the development or from parking lots or structures to building entrances.
- Developers should explore the possibility of connecting different buildings in large developments using colonnades and arcades and integrate them with open spaces such as courtyards, plazas, outdoor dining spaces and so on.

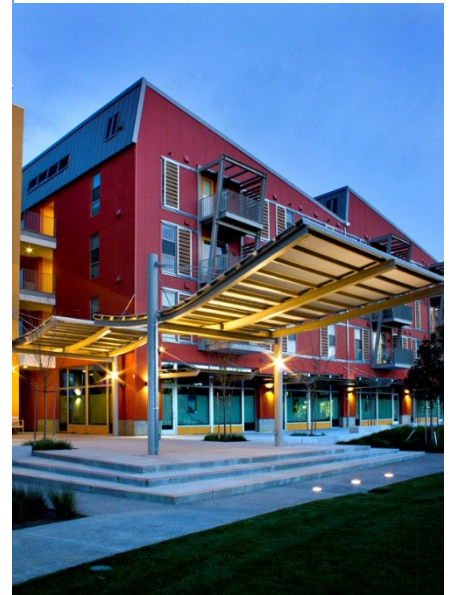
3.8.3.4.2 Awnings

- Awnings are permitted both on first as well as upper floors. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.8.3.5 On-site Screening

- Less visually aesthetic components necessary for large commercial developments such as loading areas, trash

Identifiable Entry. SOURCE: Michael Baker International



Awnings. SOURCE: Michael Baker International



enclosures, mechanical equipment, and noise and odor producing functions should be located at the sides and/or rear of main buildings, and screened with compatible architectural features and walls, and/or landscaping.

- Solid walls if necessary for screening should be no more than 6 feet high and made of solid masonry or concrete tilt-up with an exterior finish compatible to the main structure. Landscaping should be used to reduce the starkness of such walls

3.8.4 Off-Street Parking

- Parking lots should not be the dominant visual element of the site. Refer to Section 3.5.11 Off Street Parking Guidelines in the General Provisions
- Developers are encouraged to locate parking on the sides or rear of the buildings.

3.8.5 Outdoor Gathering Areas

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- The plazas are encouraged to be integrated with building design and to give an inviting feel using elements such as public art, landscaping, interesting shade structures, murals, arched walls creating gateways, and so on.
- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Pedestrian walkways should be provided to connect building entrances with parking areas. It is strongly encouraged that these walkway areas provide pedestrian improvements including but not limited to shade structures, benches and furniture, and dining areas, if appropriate.
- Plazas, open spaces, and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.
- Fences or barriers are encouraged to separate dining areas from open space and plazas where there is pedestrian traffic.
- Fences should be of good quality and made of wrought iron, steel, or wood. Other materials may be permitted if they are

Buffering Plaza Space. SOURCE Michael Baker International



compatible with the architectural style of the adjacent building and the style of the dining furniture.

Examples of Outdoor Gathering Areas. SOURCE: Michael Baker International



3.9 TRANSITION AREA

The Transition area comprises of parcels surrounding the Veterans’ park areas a block south of Seventh Street and east of Orchard Street. As the name suggest this is an areas in between residential and commercial areas. Design of site and structures are expected to integrate the surrounding characteristics of the land and the residential development pattern though used for commercial land uses. Most existing structures in this area are expected to remain with infill development occurring in the vacant parcels. The existing structures are expected to be converted to low traffic producing commercial uses as directed by zoning. The area is expected to have spillover effect from Sixth Street development with uses that are geared towards providing accommodations for tourists as well as locals and lighter commercial uses. The existing and new developments will have a view of newly redesigned Veterans’ Park and streetscapes. Similar to the Sixth Street Subarea, this area is envisioned as a pedestrian and bike friendly area. Horizontal and vertical mix of uses- residential, institutional, commercial, offices- is encouraged. However, building design should consider pedestrian scaled buildings and balancing it with need for privacy especially if there are residential uses on the ground floor. New buildings are expected to follow the architectural styles explained in Section 3.3. Other architectural styles other than the ones stated in the Section 3.3 may be allowed but subject more scrutiny and will require visualizations.

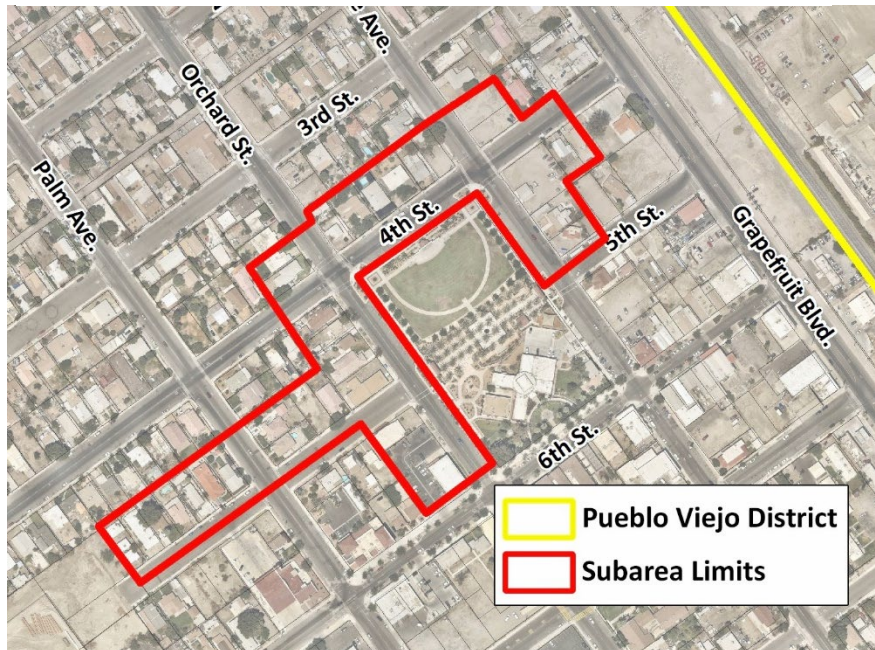
Existing Buildings Converted to Medical Office. SOURCE: Sardaka



Existing Buildings converted to artist's studio. SOURCE: Ildar Sagdejev



Map E Transition Area Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



Existing Buildings Converted to Medical Office. SOURCE: Rept0n1x



Existing Building Converted to an Inn. SOURCE: Roger Kidd



3.9.1 Existing Buildings

- Any additions/ renovations to existing structures, conversion of uses to commercial, changes in parking area, and addition of signage should be done in a manner that it does not change the residential character of the area and compatibility with the surrounding neighborhood.
- Additions should respect the scale and massing of existing structure and should not become dominant part of the development.

Existing Buildings. SOURCE: Michael Baker International



- The architectural and articulation of new addition should draw from style of the existing building. However, they need not replicate the existing structures.
- Refer to Section 3.5.1 for additional guidelines related to existing buildings.

3.9.2 Site Design

3.9.2.1 Building Orientation, and Setting

- Building orientation facing the primary street is important for creating street appeal, enhancing aesthetics, and drainage considerations and encouraged.
- Parcels surrounding the park should be oriented to take maximum advantage of visual and physical access to the park.
- Buildings should use four-sided architecture, meaning all visible sides of the building should have façade treatment. However, the primary entry will be on the primary street.
- Open spaces should be oriented toward the street (see Section 3.6.4.1 and 3.6.4.2 in Outdoor Gathering Areas Section).

3.9.3 Architecture

3.9.3.1 Massing and Articulation

Due to spillover effects of Sixth Street development and existence of Veterans’ Park, pedestrian activity is expected in this area. Hence, the massing and articulation should be designed to appeal to the slower moving pedestrian traffic rather than vehicular traffic. Emphasis on detailing and use of quality materials will be important since pedestrians move slower than vehicular traffic and are more likely to observe articulation and smaller details.

- In order to retain pedestrians’ interest and make walking a pleasurable experience, buildings should incorporate changes in massing as well as articulation and detailing that are authentic representations of architectural styles. Changes in the architectural style, variation in materials and colors materials, horizontal and vertical articulation elements, framing around the windows and doors, changes rooflines and wall planes and so on break the monotony of the façade and create interest for pedestrians.
- All facades should emphasize three-dimensional detailing depending on the architectural style used. Some examples are use of elements such as cornices, window moldings and reveals to cast shadows and create visual interest on the facade.

- Tile artwork, plaques, decorative glass, and lighting fixtures are encouraged to provide visual relief to façades. These elements should reflect the building’s architectural style.
- Artwork and architectural relief integrated into building design is encouraged and should be appropriate to the setting.

3.9.3.2 *Entry Orientation*

- Primary entries should be on primary street. The entry should be accentuated to make it visible from the street depending on the architectural style.
- Direct and visible ground floor access to uses located in upper levels should be noticeable from the street and integrated with ground floor façade to highlight their location. Entries should be further emphasized if the uses are located on the front facade.

3.9.3.3 *Door and Window Design*

- Doors and Windows should be used as architectural elements that add relief to the facade and wall surface while being true to the architectural style.
- Commercial storefronts (if on ground floor) should include street-oriented display windows in order to engage pedestrians.
- Windows for residential uses on ground floor should be designed to provide privacy to the residents while creating a pedestrian scale environment. An example would be to provide windows above pedestrian sight level with articulation is provided at the pedestrian level.
- Exceptions to window covering requirements can be made where display windows (open and enclosed) are not feasible. In this case, exterior walls should be designed to provide architectural relief or be screened by landscaping and designed to provide pedestrian amenities such as wider sidewalks or benches.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- Clear glass windows are encouraged especially for retail use to promote interaction between the pedestrians and the businesses. Highly reflective window film is prohibited as they can interfere with moving traffic.

3.9.3.4 *Porches, Balconies, and Decks*

3.9.3.4.1 Porches

- Porches are encouraged and should be designed to be consistent with the architectural style of the building to which they are attached and provide sufficient depth and width to be occupiable spaces.

Porch Example. SOURCE: Stalane



3.9.3.4.2 Balconies

- Window balconies if compatible with architectural style are encouraged at the upper floors. Faux balconies with windows are acceptable provided they integrate features from the architectural style of the building.
- Where possible, balconies should be oriented towards the street/ park area and not towards internal block or parking areas.

Deck Example. SOURCE: Acabashi



3.9.3.4.3 Decks

- Decks should be designed to be aesthetically unobtrusive and should provide sufficient depth and width to be occupiable spaces.
- The parapet and other articulation of the deck structure should draw from the architectural style of the building.

3.9.3.5 Awnings and Canopies

- Awnings and canopies may be used on first floor as well as upper floors of the building. Refer to Section 3.5.6 Shade Structure-Awnings in General Provisions for materials, design and maintenance guidelines.

3.9.4 Outdoor Gathering Areas

3.9.4.1 Plazas, Open Spaces, Gathering Areas

Due to transitional and residential nature of the area large plazas or paseos are not expected in this area. Rather front yard space converted to small plaza, open space or gathering areas is envisioned.

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- Open spaces, gathering area and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.
- Pedestrian walkways should be provided to connect building entrances with sidewalks. These should be separate from vehicular access.
- Attractive landscaping and use of elements such as water fountains, art work, playground equipment and so on make smaller gathering spaces lively and are encouraged. However, installation of such elements should respect the scale of the building.
- Perimeter fencing separating sidewalks from gathering areas is generally discouraged unless used for outdoor dining areas.

3.9.4.2 Sidewalk Dining Areas Fences and Barriers

- If open spaces oriented towards sidewalks are used for dining, fencing or barriers should be provided to separate dining areas from sidewalks. Refer to Section 3.6.4.3 under Sixth Street Subarea for guidelines related to Sidewalk Dining Area Fences and Barriers

3.10 SIGNS AND PUBLIC ART

3.10.1 Signs

Signs are an important element of commercial/ mixed use corridors and play an important role in establishing the visual character of a place. They are a vital component for many businesses because they serve as the primary identification tool for an establishment and often advertise goods or services that businesses may provide. However, a sign that has a cluttered look will undermine the character of the building and overall street appearance, while clean, attractive, and aesthetically pleasing signage can give a unique identity to the street and contribute to the overall visual appeal of the area. The general guidelines in this section provide guidelines that are applicable to all sign types that are further detailed in this section. A matrix is also provided that suggests permitted sign types by subarea.

3.10.1.1 General Sign Guidelines

- Repetitious signage should be avoided.
- No more than three signs are allowed for each building.
- The signs should not be placed closer together than 5 feet. A-frame signs can be placed closer than 5 feet from another sign for the same business.
- The design of each sign should incorporate a maximum of four colors. All signs of a single business should be of a similar color palette.

3.10.1.1.1 Sign Message

- The primary sign should only display the business's name. Secondary signs may include information regarding products provided.
- Sign message should be aesthetically pleasing and should not give a cluttered appearance.

3.10.1.1.2 Sign Lighting

- Any external spot or flood lighting should be directed at the sign and away from pedestrian walkways and/or roadways. Lighting should not create a glare for motorists or pedestrians.
- External spot or flood lighting fixtures should be simple in design, should blend into the appearance of the sign, and should not obscure the sign's graphics.

- Back-lit signs are discouraged unless back lighting is used for individually cut letters
- Halo-lit illumination or reverse channel letters are encouraged.
- Cabinet lit signs are prohibited.
- Blinking or flashing lights are prohibited.
- All mechanical equipment for the lighting of a sign should be hidden from view.
- Where signs face residential areas, lighting should be minimal.

Backlit Lighting for Individual Letters.
SOURCE: Nina Stössinger



3.10.1.2 Sign Types by Subarea

The following text describes the various types of signs that can be used in each subarea. However, the choice of sign type should be based on the subarea’s character, land uses, traffic, and the audience the sign attracts. For example, Sixth Street is expected to be transformed into a pedestrian-friendly environment with restaurants and ground-floor retail. Signs in this subarea should cater to slower-moving traffic and even slower pedestrians that will appreciate detailing and articulation. As such, a pedestrian scale is appropriate for projecting signs, hanging signs, and other sign types. In contrast, motorists in the faster-moving vehicles on Grapefruit Boulevard or Cesar Chavez Street appreciate larger signs with illumination and larger lettering that makes signs easy to read while driving. While other pedestrian-level signs will give character to these two subareas, the signs may not be as frequent as those seen on Sixth Street. Table AA shows various signs and the suggested subareas in which they may be used.

Table AA: Sign type by subarea

Sign Type	Suggested Subarea (Frequent Use)	Remarks
Projecting	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard 	These signs are to be used frequently in the Sixth Street Subarea and in the Transition Area, both of which are expected to attract pedestrians. Projecting signs can be used by businesses along Grapefruit Boulevard that may cater to pedestrian traffic.
Hanging	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard 	These signs are to be used frequently in the Sixth Street Subarea and in the Transition Area, both of which are expected to attract pedestrians and have shade structures.

Window	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>Window signs cater to pedestrian traffic and are expected to be used in pedestrian-oriented areas such as the Sixth Street Subarea and the Transition Area. They are expected to be used less frequently along Grapefruit Boulevard and Cesar Chavez Street.</p>
A-Frame Sign	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area 	<p>Window signs cater to pedestrian traffic and are expected to be used in pedestrian-oriented areas such as the Sixth Street Subarea and the Transition Area. They are not expected to be used on Grapefruit Boulevard and Cesar Chavez Street.</p>
Wall Sign	<ul style="list-style-type: none"> ■ Transition Area ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>These signs are effective in areas with faster-moving traffic where the signs are to be read in a relatively shorter amount of time. Larger signs with bigger text facilitate quick reading and are to be used along Grapefruit Boulevard and Cesar Chavez Street. They can be used in the Transition Area though are not encouraged. Wall signs are not to be used in the Sixth Street Subarea.</p>
Monument Sign	<ul style="list-style-type: none"> ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>These signs are effective in areas with faster-moving traffic where the signs are to be read in a relatively shorter amount of time. Larger signs with bigger text facilitate quick reading and are to be used along Grapefruit Boulevard and Cesar Chavez Street. Monument signs are not to be used in the Sixth Street Subarea and the Transition Area.</p>

3.10.1.3 Sign Types

This section describes various sign types and includes guidelines for their use.

3.10.1.3.1 Projecting Signs

Projecting signs are attached to a building façade and project at a 90-degree angle from the face of the building. Projecting signs should be oriented toward pedestrians and should only be used when a shade structure is not located on the building's façade. Along Grapefruit Boulevard, projecting signs should only be used along pedestrian walkways that connect parking areas with building entrances and on storefronts adjacent to plazas or open space.

- Projecting signs should be flat in nature, with signage details embossed into or projecting slightly from the sign material.
- Sign design, supports, and brackets should be compatible with the architectural style of the building from which the sign projects.
- Projecting signs should be mounted to the ground-floor façade, with a vertical clearance of 8 feet.
- Signs should be of a visually interesting design that may include irregular outlines, internal cutouts, and/or two- or three-dimensional symbols or icons.
- Projecting signs (excluding supports or brackets) should fit within a rectangle with a maximum area of 6 square feet.
- A minimum clearance of 12 inches is required between the building façade and the beginning of the sign, with a minimum projection of 36 inches.

3.10.1.3.2 Hanging Signs

Hanging signs are attached to the underside of a shade structure (for example, a colonnade, arcade, or canopy) and should be placed at a 90-degree angle from the building façade. Hanging signs should be oriented toward pedestrians and should only be used under shade structures. If shade structures are included along pedestrian walkways between parking areas and building entrances, hanging signs may be appropriate for businesses along Grapefruit Boulevard.

- Hanging signs should not project farther than the shade structure to which they are attached.
- Hanging signs should not be attached to shade structures whose height is greater than that of the ground floor.
- A minimum vertical clearance of 8 feet is required.

Projecting Signs. SOURCE: Michael Baker International



Hanging Signs. SOURCE: Michael Baker International



- Hanging signs (excluding supports or hangers) should fit within a rectangle with a maximum size of 4 square feet.
- A minimum clearance of 12 inches is required between the building façade and the beginning of the sign, with a minimum projection of 36 inches.
- Signs should be of a visually interesting design that may include irregular outlines, internal cutouts, and/or two- or three-dimensional symbols or icons.

3.10.1.3.3 Window Signs

Window signs are signs that are painted, etched, or adhered to windows or doors within the façade of a building. These signs can be only text or a combination of text with graphic items.

Window Sign. SOURCES: Top- Michael Baker International; Bottom- Brewbooks from near Seattle



- Window signs should not occupy more than 15 percent of the available window area of a building façade.
- Sign text should not be more than 8 inches in height.
- Window signs should be applied directly to the interior of the window or hung on the inside of the window as close to the glass as possible. Any blank space should be transparent.
- Window signs should be made of high-quality materials, including but not limited to paint, gold leaf, and/or neon. Etching or sandblasting are also appropriate methods of window sign installation.
- Window signs and associated graphics should complement the architectural style of the building and the design of the storefront interior.
- Neon signs should only be hung in windows.

3.10.1.3.4 A-Frame Signs

A-frame signs are temporary signs that stand on their own and are usually placed on the sidewalk in front of the business they advertise.

- A-frame signs should be made of durable, sturdy materials, including but not limited to wood or metal, with open bases that can withstand a variety of weather conditions and will not be easily sun-bleached.
- Glass, breakable materials, paper or laminated paper, PVC pipes, or illumination are prohibited for A-frame signs.
- Signs should be designed to look organized, with minimal text and large graphic elements.

- A-frame signs should be easily movable and should not be installed permanently.
- A-frame signs should have a maximum sign area of 8 square feet and can have a maximum of two sides of this size.
- All text on A-frame signs should be between 2 and 4 inches high. Text associated with the business’s logo should be exempted from this guideline.
- Rectangular A-frame signs should be no more than 4 feet in height and 2 feet in width. These signs should have an open base.
- Shaped silhouette A-frame signs should be no more than 5 feet in height and 3 feet in width. These signs should not exceed the maximum sign area of 8 square feet.
- A-frame signs should be placed on the sidewalk in such a way that 5 feet of sidewalk area is still available and doorway access is not impeded.
- A-frame signs should be placed only along the business’s frontage.
- A-frame signs should be placed on the sidewalk no sooner than half an hour before the business’s posted opening time and should be taken in from the sidewalk no later than half an hour after the business’s posted closing time.
- A-frame signs should be placed along the curb on the street side of the sidewalk.
- Each business should only have one A-frame sign.

A-frame Signs. SOURCE: Infoqmaton



3.10.1.3.5 Wall Signs

Wall signs are attached directly to the façade of a building and are of a three-dimensional shape.

Wall Signs. SOURCE: Michael Baker International

- Wall signs should project no more than 12 inches from the wall of the façade to which they are attached.
- Wall signs should not extend beyond the edges of the building façade to which they are attached and should not cover doorways or windows.
- Wall signs should not cover more than 15 percent of the building façade.
- Wall signs should complement the architectural style of the building, in style and proportion.
- Signs should be designed to provide shadow relief and a well-designed substantial appearance.



3.10.1.3.6 Monument Signs

Monument signs are freestanding signs that primarily draw the attention of motorists. These signs are often used for developments in which the main building is set back from the street right-of-way.

Monumental Sign. SOURCE: Dj1997



Monumental Sign. SOURCE: ReptOn1



- Monument signs should be installed on two posts or a substantial base.
- Monument signs should be designed to be architecturally compatible with the building for which they provide signage.
- The base should consist of stone, brick, stucco, or other sturdy, permanent, and durable material and should be at least 18 inches in height.
- The sign face should consist of durable materials, including but not limited to metal, aluminum, and/or wood.
- Landscaping should be integrated into the sign and should surround the entirety of the base. The landscaping should be designed to ensure sign legibility for the life of the sign.
- Monument signs should avoid identifying multiple tenants. However, monument signs should be no closer than 100 feet from each other. If lot width and building design do not permit multiple monument signs on the property, the sign should be designed to incorporate the development's anchor tenants.
- Monument signs should be externally lit. Internally lit monument signs are prohibited. In the case of neon, exceptions may be made.
- Site addresses should appear on the monument signs and will not count as part of the sign area. Addresses should be no less than 9 inches in height.
- Monument signs should be perpendicular to the adjacent street and sidewalk and should have not more than two parallel sign faces.
- Monument sign face should be no more than 50 square feet. The height of the sign's side should be no more than 6 feet.

3.10.2 Public Art

Public art refers to murals, statuary, and installations in public realm and areas of private realm accessible to general public. Public art projects come in many forms and can be permanent or temporary art installations with varied viewpoints. They create a vibrant environment and give a uniquely identity and placemaking aspect to an area. However, establishing clearly defined criteria for art installations is necessary to avoid conflicts and tensions between various sects of people using a place

and providing a cohesive look. Along with artwork itself, its siting and maintenance should as be clearly defined.

Murals are categorized as public art and are an important part of the urban fabric of Pueblo Viejo. They represent not only the residents of Coachella but also the zeitgeist of the time at which they were installed. New development offers an opportunity to plan for mural locations in a way that will complement the intended architectural style of the buildings. Murals should be consistent with Chapter 5.70, Art Murals, of the City of Coachella’s Municipal Code.

3.10.2.1 Site Selection and Placement

- Public art is encouraged to be installed in areas experiencing or expected to experience high levels of pedestrian traffic such as sidewalks and plaza areas or places that create entrance to the area such as medians, and circles.
- Public art should be easily visible and accessible to the public and serve as gathering places for people
- Public art should be part of the City's circulation system and enhance the overall public environment and pedestrian streetscape experience
- Public art should establish landmarks and neighborhood gateways.
- The placement of public art should not block entrances, windows, signage, and pedestrian circulation.
- The placement of public art should not interfere with the line of sight of the oncoming traffic

3.10.2.2 Content

- Public art should not include commercial signage, logos, or advertising. However, they may include dedications.
- Public art projects are encouraged to be diverse and derive content from local history, community, geography, and environment.
- Public art should be expected to integrate into the overall design of the larger
- context in which it is placed. It should not be conceived as an afterthought.

Public Art in Sidewalk Area. SOURCE: Géza Stremeny (sculpture) / Yoav Dothan (photo)



Temporary Art Installation. SOURCE: Martin Falbisoner



Public Art in Plaza Area. SOURCE: Beyond My Ken



3.10.2.3 Materials and Construction

- Public Art should be constructed with durable materials that will withstand the elements and can be washed to remove dirt and graffiti.
- Public art projects are encouraged to be made of environmentally sustainable materials.
- Temporary installations may be allowed for no more than two weeks, using non-durable materials based on with City's approval. However, such installations should be firmly anchored to ground or structure depending on the type of installation. The City may ask for its removal prior to time limit if it suffers significant damage/ deterioration.

3.10.2.4 Maintenance

- The maintenance of public art on private property will be the responsibility of property owner.
- The artwork should be presentable at all times and devoid of unpleasant look due to conditions such as discoloration, rust, and so on.

Existing Mural in Pueblo Viejo District. SOURCE: Michael Baker International



4 PUBLIC REALM DESIGN GUIDELINES

4

The public realm refers to the street right-of-way and includes sidewalk space and roadway space. Planners and designers are focusing on a variety of ways to integrate multiple modes of travel—not just motorized vehicles—with the land uses they serve. Alongside traditional transportation by car, new developments are rediscovering multimodal transportation by improving transit, pedestrian, and bike connectivity that lead to amenity-rich, walkable, bikeable, and sustainable communities. These approaches are designed to provide transportation choice, reduce air pollution and other environmental impacts, enhance public health, and support amenities. The public realm guidelines concentrate on the commercial/mixed-use streets in the Pueblo Viejo District although traffic calming features are suggested for the residential streets. The chapter begins with explanation of streetscape concepts for each of the commercial street along with an illustrative section. This section is followed with General Guidelines that are applicable to all commercial streets. Traffic Calming section lays out various ways of calming traffic in both commercial and residential areas of the District and Landscaping Guidelines provides plant palette for various streets.

4.1 STREETScape CONCEPTS

In 2015, the City implemented new streetscape design along three blocks of Sixth Street from Palm Avenue to Grapefruit Boulevard. This streetscape will inspire the streetscape design on the remaining commercial streets. However, all streets are different and should be designed per the space available, context, pedestrian activity, and land uses to which they cater. Each street should have its own distinct character, though some elements such as street furnishings, lighting, and signage act as tying elements that give uniformity and order to the whole District. Streetscapes should continue the design principles, materials, planting, lighting, furnishings, and general streetscape design per the newly built streetscape on Sixth Street.

While detailed streetscapes are to be designed as and when the City is ready to implement these projects, shown below are the typical streetscapes that are suggested to create a multimodal public realm along the commercial/mixed-use streets of Pueblo Viejo.

- Sixth Street, from Grapefruit Boulevard to Cesar Chavez Street
- Fifth Street, from Grapefruit Boulevard to Cesar Chavez Street
- Seventh Street, from Grapefruit Boulevard to Tripoli Avenue

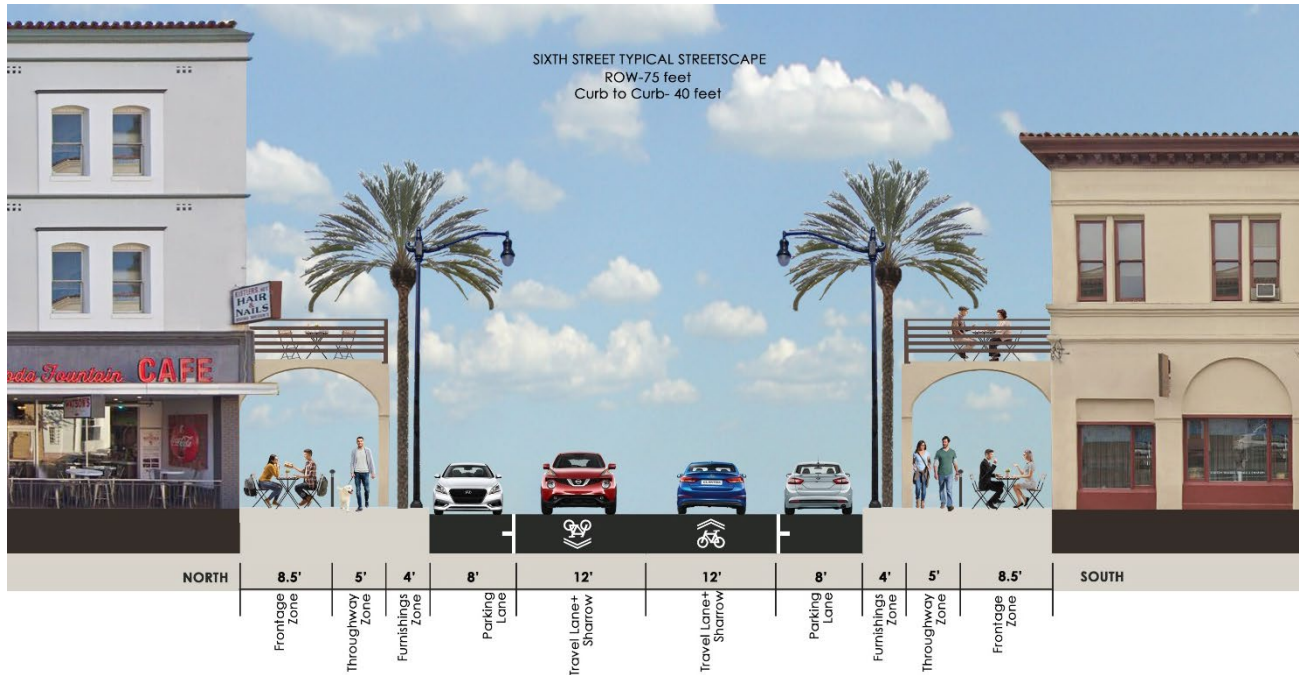
- Grapefruit Boulevard, from Bagdad Avenue to Park Lane
- Cesar Chavez Street, from Bagdad Avenue to Park Lane
- Orchard Street, from Sixth Street to Fourth Street
- Vine Avenue, from Sixth Street to Fourth Street

4.1.1 Sixth Street

From Grapefruit Boulevard to Cesar Chavez Street

The streetscape of Sixth Street will be a continuation of the recently implemented streetscape in the three blocks of Sixth Street from Grapefruit boulevard to Palm Avenue. As depicted in the street section below, it consists of one lane on each side of the street and parking lane. Due to restriction of space and need for enhanced pedestrian area, a shared bicycle facility is suggested on this street. The sidewalk area consists of a 4 feet Furnishings Zone that will accommodate plantings, light poles, bike racks, way finding maps, and other utilities. As an urban corridor the Furnishings Zone is preferred to be hard paved with tree grates and planter pots and can be used as an extension of sidewalk area (Throughway Zone). Dependent on City’s permission this area can also be for outdoor dining space. The Throughway Zone and the Frontage Zone (spillover space between 5 feet clear walking space and parcel line) are approximately 13.5 feet wide and shaded with a colonnade/ Arcade structure. Frontage Zone will house street furniture (sidewalk dining, benches and trash cans).

Sixth Street Streetscape. Source: Michael Baker International



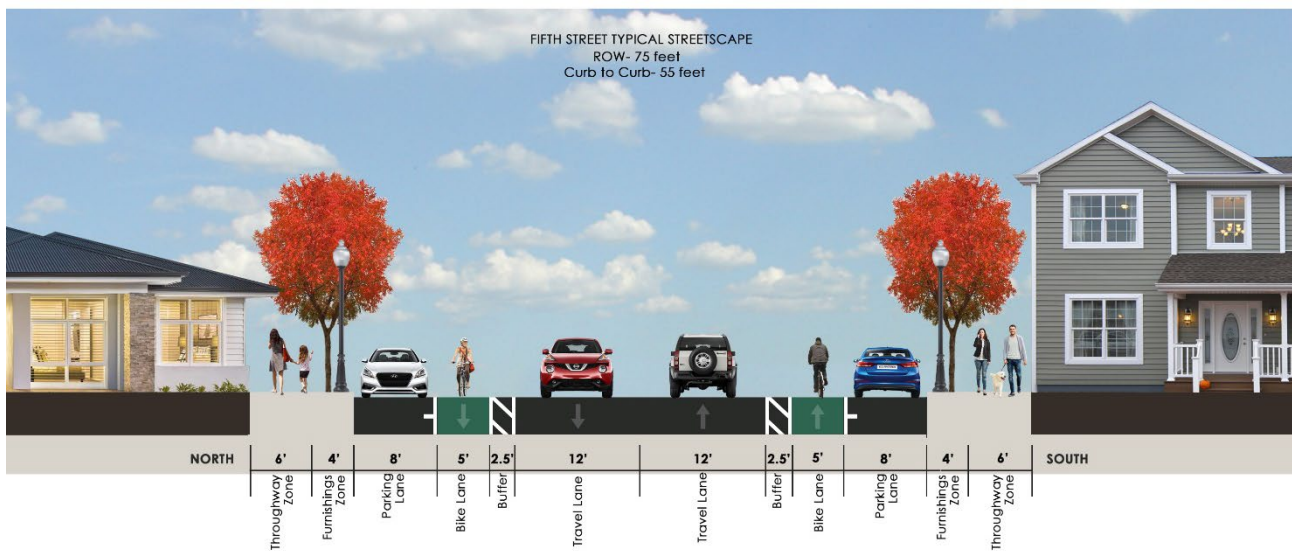
The private realm guidelines will help in making walking a pleasurable experience for pedestrians but details in public realm such as treatment of sidewalk with inlays, markings, and patterns in paving can add to this experience. The place making features such as plazas or gathering spaces that may a part of public realm will follow guidelines listed under this category in private realm as applicable (see Section 3.6.4). Traffic calming and landscaping on this pedestrian oriented corridor will play a large part in attracting foot traffic.

4.1.2 Fifth Street

From Grapefruit Boulevard to Cesar Chavez Street

Fifth Street is a part of Sixth Street Subarea and a parallel street to Sixth Street to the north. The street is expected to see spillover effects of development along Sixth Street on the south side of the Street and have transitional land uses on the north side. The street is also expected to cut through the development in Block XX and intersect with the Cesar Chavez Boulevard. While there will be pedestrian activity, it is not expected to be as intense as Sixth Street leading to minimum area (4 feet) for Furnishings Zone and a comfortable Throughway and Frontage Zone (approximately 6 feet). The proposed roadway has one travel lane in each direction, a buffered bike lane, and on-street parallel parking. Being a parallel street to Sixth Street and having ample right of way, this street is a good candidate for a bike facility. Hence a buffered bike lane is proposed.

Fifth Street Streetscape. Source: Michael Baker International

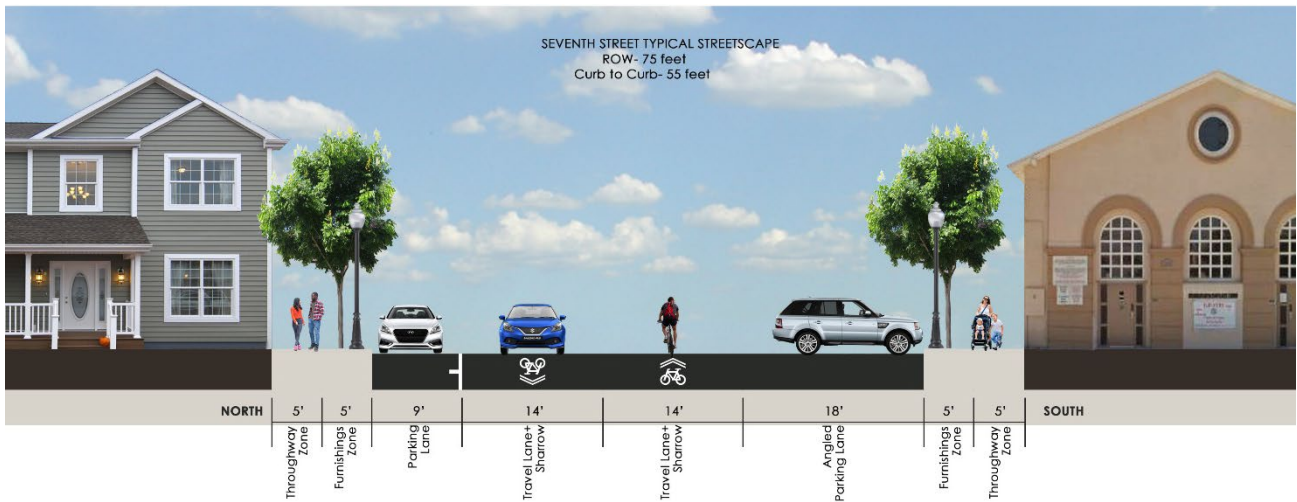


4.1.3 Seventh Street

From Grapefruit Boulevard to Tripoli Avenue

The Seventh Street is also a part of Sixth Street Subarea and a parallel street to Sixth Street towards the southside. The street is also expected to see spillover effects of development along Sixth Street in certain blocks. The school block and residential uses on the south side of the street are not expected to change. The configuration of Street includes 5 feet Furnishings Zone and 5 feet of Throughway and Frontage Zone. The ample right of way allows for accommodation of angled parking on south side and parallel parking on the north side of the street. In addition to parking, the proposed facility includes one travel lane with Sharrows to accommodate bikes.

Seventh Street Streetscape. Source: Michael Baker International



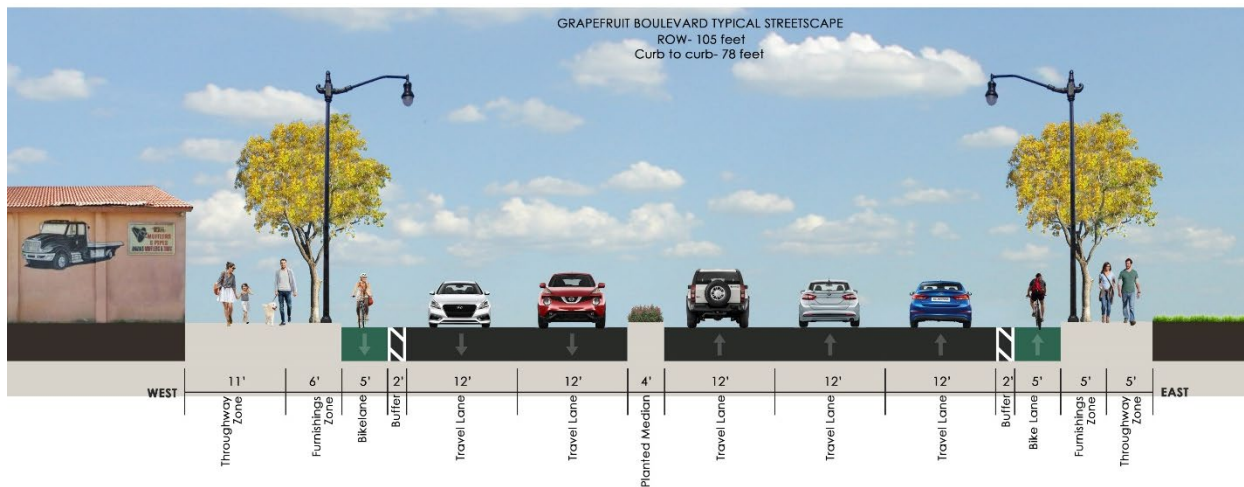
4.1.4 Grapefruit Boulevard

From Bagdad Avenue to Park Lane

Grapefruit Boulevard is a State Route 111, with a speed limit of 45 mph and provides connection to State Route 86. It caters to commercial and light industrial uses in the Pueblo Viejo District and it is expected to be that way in the future. The east side of the street has vacant land bordered by railway tracks. Currently, there is one travel lane in each direction, on street parking and a turn lane. Though being a gateway to the City and to the Pueblo Viejo District, the street does not have an appeal and does not look unique to Coachella. It has ample right of way and is in need of an upliftment.

Based on Cities current General Plan Update’s Mobility Element, Grapefruit Boulevard falls in “Primary Arterial with Enhanced Bicycle Facilities” category. The suggested configuration calls for two 12 feet lanes in north side of the street and three 12 feet lanes on the south side of the street. There is no street parking provided on this street. The proposed concept also calls for buffered bike lanes in both directions. A 4 feet planted median is also proposed. It will not only create an aesthetically pleasing corridor with a gateway effect but will also help with access management and traffic flow. The walkway area in the north is larger than the south side of the corridor as it is expected to get more

Grapefruit Boulevard Streetscape. Source: Michael Baker International



pedestrian activity.

4.1.5 Cesar Chavez Street

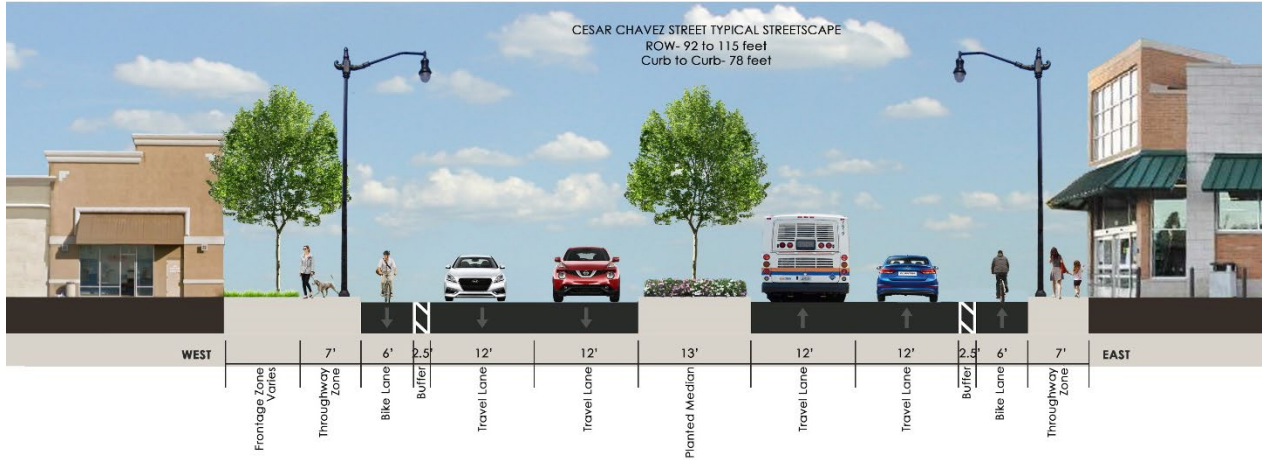
From Bagdad Avenue to Park Lane

Cesar Chavez Street is a commercial corridor with big box stores, strip malls, apartment complex, and other commercial uses. The vacant land at the intersection with Sixth Street is proposed to house a transit center. Currently, there are two lanes in each direction and a planted median in parts of the corridor and turn lane in the rest. While there are pedestrian facilities, the streetscape design does not encourage walking. Being a major gateway to the District the street will need upliftment and incorporation of multimodal facilities.

The proposed configuration has two 12 feet travel lanes in each direction, no parking, a 13 feet wide planted median, and a buffered bike lane. The Throughway/furnishing zone has a combined 7 feet of space based on the existing curb lines restrictions leaving no room for street trees. However, smaller potted plant arrangements can enhance the aesthetics of the

walkway. The Frontage Zone varies in the North based on the development and parcel lines. This space can be used for planting trees and other placemaking opportunities such as installation of gateway features, art and so on. The planted median will also give much needed gateway effect.

Cesar Chavez Street Streetscape. Source: Michael Baker International



4.1.6 Orchard Street

From Sixth Street to Fourth Street

The Orchard Street has recently been reconfigured with angled parking on the park side and parallel parking on the other side of the Street. It has one lane in each direction and enhanced pedestrian realm. However, the street lacks bicycle facility. The Street has right of way to accommodate bicycle lanes or can be marked as a share the road facility by sharrow markings on the street. This will provide bike connections from the Fifth street and to the Park.

4.1.7 Vine Avenue

From Sixth Street to Fourth Street

Vine Avenue has recently been reconfigured similar to the Orchard Street and has no bike facilities. It too has right of way to accommodate bicycle lanes or can be marked as a share the road facility by sharrow markings on the street to provide bike connections from the Fifth street and to the Park.

4.2 GENERAL GUIDELINES

4.2.1 Active Living Components

Active living components—pedestrian, bicycle, and transit facilities—should be integral to the streetscape design in Pueblo Viejo District.

4.2.1.1 Pedestrian Facilities

Walking is the most basic form of transportation. The creation of an attractive and safe pedestrian environment is a critical part of developing more livable communities. Pedestrian facilities should be safe and accessible to all types of users, connect to places where people want to go, encourage interaction, be attractive and of pedestrian scale, easy to use, economical to build, and easily maintainable. The main forms of pedestrian facilities are sidewalks and trails including street crossings and curb ramps. Enhancements include pedestrian signals (including countdown signals and lights embedded in crosswalks), raised crosswalks, and street furniture, lighting, and landscaping.

Sidewalks

Connectivity between buildings, land use areas, and adjacent development, both existing and new, can be achieved through the site-sensitive design of pedestrian areas such as sidewalks. These elements may be integrated into the street design and cohesively designed to connect residential, office and retail/commercial areas, and transit stops.

Sidewalks are divided into three zones:

1. **Furnishings Zone**
 2. **Throughway Zone**
 3. **Frontage Zone**
- The **Furnishings Zone** is defined as the area of the sidewalk appropriate for planting trees and the placement of street furniture, bike racks, trash receptacles, and light poles.
 - The width of Furnishings Zone is to be a minimum of 6 feet.
 - See related guidelines for landscaping, active transportation, lighting, and street furniture in this section.
 - The **Throughway Zone** is defined as the area of the sidewalk that is utilized for moving pedestrians along the corridor.
 - The width of Throughway Zone is to be a minimum of 5 feet.
 - Throughway Zone must be clear without any obstacles.

- The Throughway Zone should be covered with a colonnade or arcade structure on Sixth Street.
 - A shaded structure is an optional for the rest of the streets in Pueblo Viejo District.
- The **Frontage Zone** is defined as the area of the sidewalk between the property line and the Throughway Zone.
 - The width of this area varies and may be combined with setbacks to provide additional area for outdoor activities.
 - Frontage Zone may be used in a variety of ways such as outdoor dining areas, locations for public art, fountains, planter boxes, or for pedestrian space for window shopping, festive decorations, or small kiosk areas.

Sidewalk Zones. SOURCE Michael Baker International



- All sidewalks should be accessible pursuant to ADA standards.
- Ramps at corners, midblock, or at driveways should be pursuant to the California Department of Transportation’s (Caltrans) ADA standards.
- Truncated domes (dark gray color) should be used at the ramps per Caltrans’ standards.

- The transition between ramps and gutters to the road surface should be smooth.
- Curb ramps are required at every corner of the intersection.

Materials and Construction

- Sidewalks should be constructed of concrete that meet City standards.
- Any color addition to the concrete must approved by the City.
- Sidewalks should be constructed to promote the safety of the users, meaning they should be smooth and free of uneven surfaces that may cause a pedestrian to trip and fall.
- Elements such as engravings, inlays, embeddings, mosaic work, and other types of public art elements are encouraged to project the unique character of the community. These are placemaking elements and create interest for pedestrians. However, these elements should not cause obstructions to ADA accessibility or other safety concerns and should be approved by the City.
- Sidewalks should be built with appropriately spaced expansion and control joints so as to not cause cracking in the concrete.
- If used, tree grates should be flush with the sidewalks.

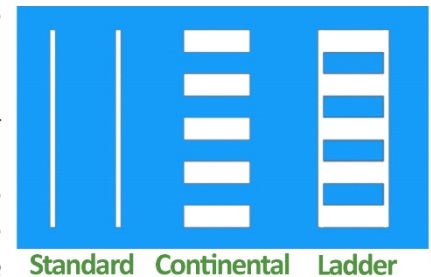
Public Art and Pavers. SOURCE: Jeangagnon



Pedestrian and Bicycle Crossings

- Marked crosswalks are most effective when they can be identified easily by motorists and pedestrians. They should present a visual contrast with the surface of the street. Marked crosswalks may be provided at all major-minor and minor-minor intersections as well as at midblock crossings.
- The use of longitudinal stripes in addition to or in place of the standard transverse markings can significantly increase the visibility of a marked crosswalk to oncoming traffic and are encouraged.
- Thermoplastic pavement marking material should be used for pavement markings including crosswalks.
- Decorative crosswalks typically made with bricks or other specialty paving are encouraged. They make the crosswalk more visible to oncoming traffic. These should be provided at all major intersections such as Sixth Street intersections with Grapefruit Boulevard and Cesar Chavez Street. Decorative crosswalks may also be used at other intersections and midblock crossings. The FHWA Memorandum “MUTCD - Official Ruling 3(09)-24(l) – Application of Colored Pavement” dated August 15, 2013 provides the following clarification on acceptable decorative treatment at marked crosswalks and should be followed in Pueblo Viejo District:

Marked Crosswalks. SOURCE: Michael Baker International



Decorative Crosswalk. SOURCE: Piotrus



“Examples of acceptable treatments include brick lattice patterns, paving bricks, paving stones, setts, cobbles, or other resources designed to simulate such paving. Acceptable colors for these materials would be red, rust, brown, burgundy, clay, tan or similar earth tone equivalents. All elements of pattern and color for these treatments are to be uniform, consistent, repetitive, and expected so as not to be a source of distraction. No element of the aesthetic interior treatment is to be random or unsystematic. No element of the aesthetic interior treatment can implement pictographs, symbols, multiple color arrangements, etc., or can otherwise attempt to communicate with any roadway user.”

Raised Crosswalk. SOURCE: Richard Drdul



- Raised crosswalks are encouraged to be used at locations where speeding is prevalent.
- Midblock crossings should be highly visible and installed in areas with large neighborhood blocks, typically more than 400 feet. Use of frequent midblock crossings can disrupt traffic flow and create safety concerns for pedestrians.
- A refuge island is a cut in a median island or median (refer to Section 4.3 Traffic Calming Features) to provide refuge for pedestrians and bicyclists. It is an effective tool in increasing overall comfort and facilitates pedestrian and bicycle crossing. Refuge islands offer a protected space for pedestrians and bicyclists to wait for an acceptable gap in traffic, especially at unsignalized crossings, and reduce overall crossing length.

4.2.1.2 Bicycle Facilities

Bicycle facilities are very important in creating a multimodal transportation Pueblo Viejo District. The Streetscape Concepts in Section 4.1 suggests various facilities that can be incorporated in the available right of way in the commercial areas of Pueblo Viejo District. It is suggested that a bicycle connectivity plan be created for the Pueblo Viejo District area to assess the feasibility of various bikeway types on the streets of the District and ensure connectivity to businesses in the commercial areas. Low speed and low traffic residential streets should be looked at to provide traffic calming features and cover them as bike boulevards. As far as possible, analysis should assess the feasibility of the bike facility from the safest to the least safe option in the order listed below.

- Separated trail
- Curb/median-separated cycle track
- Post-separated cycle track
- Elevated cycle track
- Buffered green bike lane
- Buffered bike lane
- Green bike lane
- Striped bike lane

- Bike boulevards
 - Sharrows with signs
 - Share the road signs
 - No facility
- Caltrans standards and National Association of City Transportation Officials (NACTO) guidelines should be followed in the design of these facilities along the roadway and at crossings.
 - The bike facilities should be connected to transit and pedestrian facilities.

Bicycle Parking

- Bicycle racks should be placed in the Furnishings Zone at strategic locations specified by the City. The racks should be installed between street trees, planter boxes, and other street furnishings.
- Racks should be located so that an average of two bicycles may be parked every 200 feet.
- Maximum distance between any two rack locations is not to exceed 600 feet.
- Bicycle stands should complement the streetscape design, materials, and color palette in the area.

Separated Bike Lane. SOURCE: Paul Krueger



Buffered Bike Lane. SOURCE: Paul Sableman



4.2.1.3 Transit Facilities

Bus Stop Placement

Bus stops should be placed in strategic locations as specified by the Sunline Transit Agency and as determined by a traffic/transit study of the roadway. Many factors influence the location of stops, such as site-specific safety considerations, traffic patterns, intersection geometry, passenger origins and destinations, pedestrian accessibility, route design, frequency of use, density and land use, service type (express vs. local), and available space. In general, pedestrian-oriented areas have closely placed bus stops (approximately 0.25 miles) compared to areas with less pedestrian focus.

The guidelines below apply to the placement of bus stops.

- It is preferred that bus stops be located on the far side of a roadway (right after an intersection) because that location is the safest for passengers exiting the bus and minimizes conflicts with other vehicles. Midblock or near-side stop (just before an intersection) may be used in some situations, depending on Bus pull-out bays are preferred over curbside stops and should be integrated into streetscape design where possible.
- The bus stop should be placed in the Furnishings Zone and should not spill into the Throughway Zone.
- Bus stops should be placed in such a way that they are visible to transit users.
- The bus stop boarding, and alighting area should have a firm, stable surface and should be handicap accessible pursuant to ADA standards.

Sunline Transit Agency Bus Stop. SOURCE: Michael Baker International



- It is advised that all bus stops in the Pueblo Viejo District incorporate a shelter per Sunline Transit Agency’s standards.

4.2.2 Parking

On street parking is encouraged in the commercial areas as it has several advantages. Besides providing convenient parking for businesses, on street parking can reduce the need for provision of off street parking depending on city’s zoning code and thereby reducing impervious surface required for development. It also acts as a traffic management tool by providing a safety barrier for pedestrians and reduces motor vehicle speeds.

- On-street parking is encouraged in the commercial areas.
- Multi-space parking meters are encouraged compared to individual pole mounted meters as they incorporate more customer-friendly features such as on-screen instructions and acceptance of credit cards for payment.
- Parallel or angle parking may be based on street configuration as determined by a traffic study and the streetscape design.
- The on-street parking stall dimensions should follow City’s parking standards.
- Each parking stall should be individually marked.

*Multi-space Parking Meter Example.
SOURCE: Zorro2212*



4.2.3 Access Management

Access management is the practice of properly locating and designing access to adjoining properties to reduce conflicts and improve safety while maintaining reasonable property access and traffic flow on the public street system.

- Minimize curb cuts to reduce conflicts between vehicles, pedestrians, and bicyclists; locate driveways and major entrances away from intersections and away from each other to minimize effects on traffic operations; minimize potential for crashes; provide for adequate storage lengths for turning vehicles; and reduce conflicts with pedestrians.
- Consider using curbed medians and locating median openings to manage access and minimize conflicts.
- Use turn lanes where medians are not possible to avoid delays in traffic flow.
- Use cross streets and alleys to provide access to parking and loading areas behind buildings.

4.2.4 Street Furnishings

Street furnishings have a significant aesthetic impact on the streetscape. Therefore, attention should be paid to all street furnishings to maintain

consistency along the corridor. Examples of street furniture include benches, trash bins, telephone booths, water fountains, and recycling/trash containers.

- Commercial area street furnishings should follow the palette established by the Sixth Street streetscape design. The City will create a developer contribution program to fund the street furnishings as properties are developed. The City will install all the street furnishings, which will ensure the consistency of the streetscape.
- The selected designs for street furnishings not described in the Sixth Street streetscape design should be compatible with the established palette and be simple, functional, easily maintained, sturdy, and of good commercial-grade quality.
- The materials should be nonreflective and be able to withstand weather conditions.
- The materials that come in contact with pedestrians should have a low heat absorption index.
- **Benches**
 - Benches in commercial areas should follow the palette established by the Sixth Street streetscape design: 96-inch Classic series bench, Model C-196, bronze powder-coat color with IPE wood seat, manufactured by Victor Stanley. Benches should be attached to the ground per the manufacturer's recommendation.
 - Benches should be placed in the Frontage Zone similar to the current Sixth Street design.
 - The frequency of benches will vary by subarea, with the pedestrian-friendly Sixth Street Subarea having at least two benches per block compared to Grapefruit

Bench Used in Sixth Street Streetscape. SOURCE: Victor Stanley



Boulevard or Cesar Chavez Street with one bench per block.

- The City will determine the location of the benches based on detailed streetscape plans.
 - Once installed, the benches are to be maintained by property owners.
- **Recycling and Trash Containers**

- Trash containers in commercial areas should follow palette established by the Sixth Street streetscape design: Dynasty Series litter receptacle, Model DYN-36, bronze powder-coat color with side opening, manufactured by Victor Stanley. Containers should be attached to the ground per the manufacturer’s recommendation.
- It is recommended that the new streetscape design use trash and recycling containers. Recommended model: Dynasty Series litter receptacle, Model DYN-242, bronze powder-coat color with side opening, manufactured by Victor Stanley. Containers should be attached to the ground per the manufacturer’s recommendation.
- Recycling and trash containers should be placed in the Frontage Zone, similar to the current Sixth Street design, and in line with the benches.
- The placement of the containers will follow the design of the streetscape. However, it is advised that one set of containers be placed every 200 feet in areas with heavy pedestrian traffic such as the Sixth Street Subarea.
- Containers should be placed so that they are easily visible.
- The containers should have clear and visible symbols for trash and recycling.
- Recycling and trash containers should be maintained by the property owner.

■ **Public Art**

- Apart from regular street design, streetscapes should provide for a periodic “surprise element” in the form of public art. These elements may be stand-alone structures or interactive art pieces, gateway elements, or artistic gathering places. Surprise elements not only play an important role in attracting the public, they also help create an identity within each subarea.

■ **Utilities**

- Utility connections and support should be located in the Furnishings Zone to avoid conflict with pedestrian movement in the rights-of-way.

Trash Can Used in Sixth Street Streetscape. SOURCE: Victor Stanley



Trash and Recycling Can Used in Sixth Street Streetscape. SOURCE: Victor Stanley



Use of Public Art as Surprise Element. SOURCE: Jeangagnon



Example of Tree Grate. SOURCE: David Schott from Redmond



- New utility upgrades and service to development parcels should be installed belowground and should be easily accessible. These should be approved by the City’s Public Works Department in coordination with other regulating agencies.
- Existing utility boxes such as telephone boxes could be converted into pieces of art to add character to the neighborhood.

■ **Tree Grates**

- Tree grates may be used in the commercial areas based on the streetscape design.
- Tree grate design and material should complement the street furnishings.
- Tree grates should be a minimum of 5 feet square.
- Tree grate should flush well with the sidewalk in order to prevent any injury to pedestrians related to uneven surface

4.2.5 Lighting

- The City has an established palette for lighting along Sixth Street. This palette should be continued in all commercial areas.
- The City will create a developer contribution program to fund the lighting as properties are developed. The City will install all the street lighting, which will ensure the consistency of the streetscape.

4.2.6 Gateways and Entries

According to the Vision Plan for Pueblo Viejo, major gateway features are suggested at three intersections.

- Cesar Chavez Street and Grapefruit Boulevard
- Sixth Street and Grapefruit Boulevard
- Sixth Street and Cesar Chavez Street

Following guidelines are suggested for the design of the Gateways and Entry structures

- To create a "sense of arrival" or entry, there must be a significant change in the spatial arrangement to the scale of which the pedestrian and the vehicle driver can perceive the change.
- The size and scale of an entry is important as it must be bold enough to capture the attention and memory of the user.
- To create a visual change, contrast of spatial elements and design features are required. To strengthen the entry statement, a

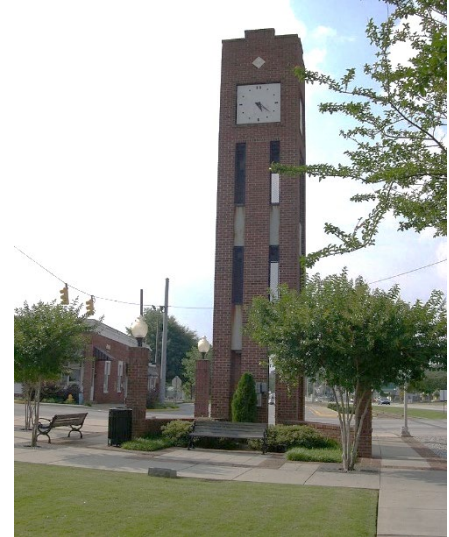
combination of a vertical support structure with an overhead horizontal structure is recommended.

- The Gateways should be designed in a way that its materials and design reflect the Spanish colonial revival or Mission Revival architectural style.
- Gateways and Entry structure should be placed in a way to not cause obstructions to the line of sight.

Example of a Gateway. SOURCE: Charvex



Gateway Tower Example: SOURCE: Jerry Stevens



4.2.7 Roadway Signage

- The City has an established palette for roadway signs along Sixth Street. This palette should be continued in all commercial areas.
- Signage location and installation should be per MUTCD standards or as specified by the City based on detailed streetscape design.
- Signage should be clearly visible to the intended traffic.
- The signage installation should be in the Furnishings Zone and should not create obstructions in the Throughway Zone.

4.2.8 Wayfinding

Exterior wayfinding signs help locals and visitors orient to a place and easily find shops or restaurants.

- Wayfinding signs may be placed near landscaped areas in the Furnishings Zones at major intersections. They can also be placed at strategic locations that can also double as plaza spaces in commercial areas.

- The design of wayfinding signs should complement the distinctive characteristics of the subareas and the street furniture in the commercial areas.
- Maps may be designed as interactive or static.
- Maps should be designed as easy to read, and graphics should be clear and easily understood.
- The maximum height permitted is 6 feet, or as approved by the City.
- The maximum width permitted is 4 feet, or as approved by the City.
- Illumination should be subdued and not garish.

Wayfinding Signage Example. SOURCE: Stevekeiretsu Wayfinding Signage Example. SOURCE: Seattle City Council from Seattle



4.3 TRAFFIC CALMING FEATURES

Traffic calming features are used to reduce vehicle speeds and make it safer and more comfortable for other modes to coexist with the automobiles. A variety of traffic calming features are available for use in streetscape design. Commercial streets are expected to attract pedestrians and bicyclists and hence streetscape design with strategically incorporated traffic calming features will help maintain speed limit while encouraging walkability.

Residential streets are well established in the Pueblo Viejo District. However, speeding and cut-throughs are an issue. Further, because of the lack of space on the commercial streets to accommodate facilities for bicyclists, the residential neighborhood streets are an option for creating alternative routes in the form of bike boulevards. These accommodations and general safety involve a reduction in the speed of motor vehicles moving along the residential neighborhood streets.

Every street is different, and further study is needed to implement one or more suitable methods for each street and purpose it serves. Described below are some methods that can calm traffic and increase the real as well as perceived sense of safety for pedestrians and bicyclists in the residential areas of Pueblo Viejo. The Table B contains a matrix that shows the applicability of these features by streets.

- **Medians.** A median is the portion of the roadway separating opposing directions of the roadway or separating local lanes from through travel lanes. Medians may be depressed, raised, or flush with the road surface. Medians are generally linear and continuous through a block and allow vehicles to travel efficiently. They can also encourage pedestrians to cross away from crosswalks. The presence of a median (especially a raised planted median) gives the perception of narrower lanes, in turn causing vehicles to slow down. Medians also reduce the length of the crossing if accompanied with a median island, making crossing safer for pedestrians and bicyclists.
- **Median Islands.** A median island is an island in the middle of the roadway, typically with landscaping for aesthetic reasons, in order to narrow the vehicle travel lanes. They are generally located midblock and can be used as a gateway to the street or community. A median island can be combined with a refuge island (a cut created to provide refuge for pedestrians) when midblock crossings are warranted.
- **Reduced Lane Widths.** According to the AASHTO Green Book, for rural and urban arterials, lane widths may vary from 10 to 12 feet.

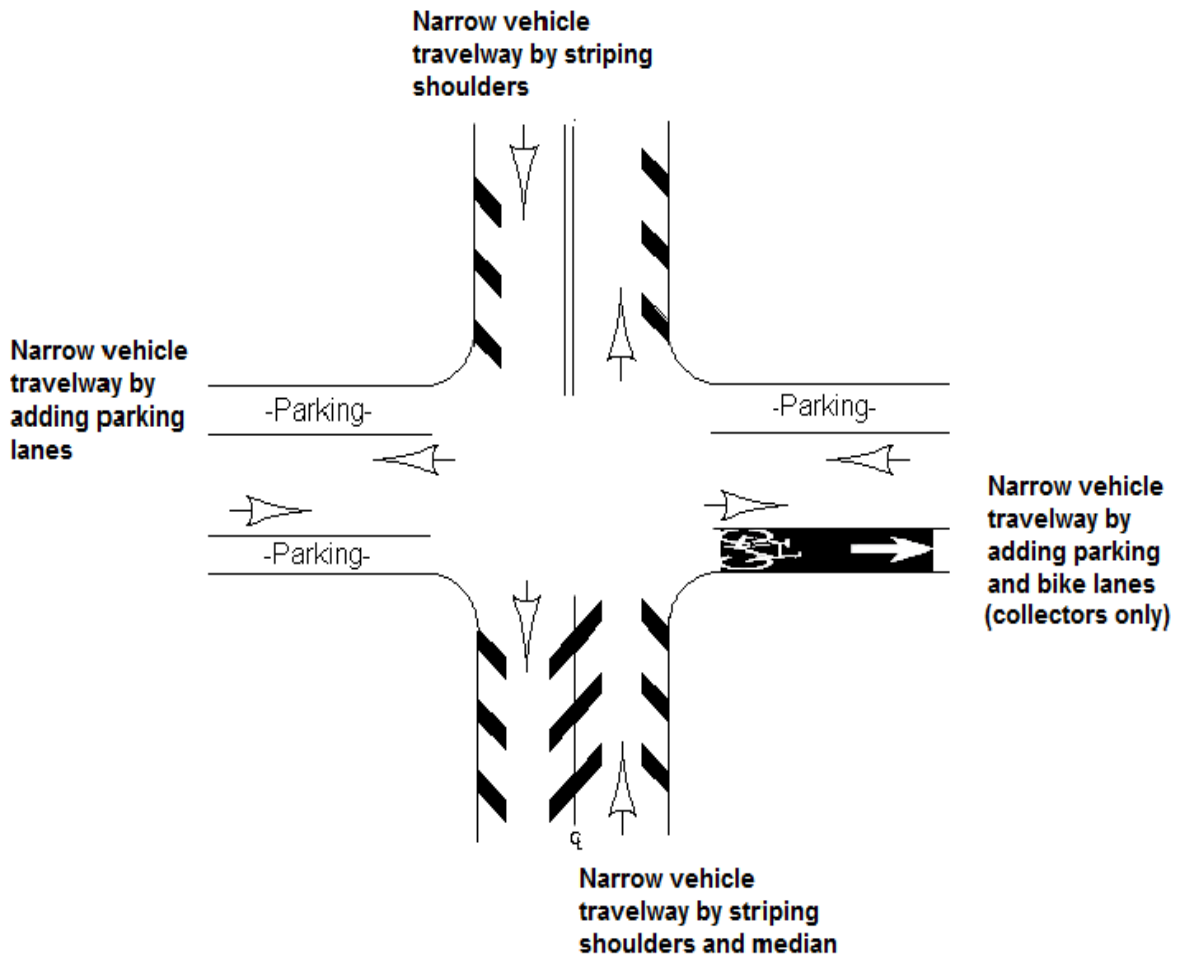
Median Island. SOURCE: Richard Drdul



For roadways at low speeds (45 mph or less) narrower lane widths are normally quite adequate and have some advantages. When there is excess space available, it can be repurposed to provide features such as bike lanes, enhanced pedestrian facilities, and on-street parking without the need to physically widen the roadway. All roadways in Pueblo Viejo District have speed limit of 45 mph or less and hence narrow roadways can be applied as a traffic calming feature.

- Pavement Markings.** Most streets in the Pueblo Viejo District have no pavement markings, leaving the street wide open for speeding vehicles. Pavement markings allocate limited space for moving traffic, keeping vehicles in their lanes and helping motorists slow down. The perception of narrower streets tends to make motorists drive slower. Various options for pavement markings are shown in figure below

Pavement Marking. SOURCE: Traffic Calming Guide for Neighborhood Streets, Virginia Department of Transportation, 2017



- Curb Extensions/ Bulb-outs.** Curb extensions/bulb-outs are provided at driveways and at intersections. They can be effective at intersections in reducing crossing length and increasing visibility, making crossing safer for pedestrians and bicyclists. The presence of curb extensions/bulb-outs gives the perception of narrower lanes, in turn causing vehicles to slow down. They may result in a loss of parking but do include space for landscaping, creating community character and a pedestrian-friendly environment. The curb extensions must be carefully planned and designed to work with the existing driveway locations and the function of existing drainage and street sweeping systems.

Landscaping in Curb Extensions. SOURCE: Richard Drdul

Curb Extensions in Commercial Area. SOURCE: Andrew Bossi



- Chokers.** Chokers are curb extensions that reduce the overall width of the roadway. They create a pinch point along the street. Chokers can be created by bringing both curbs in, or they can be done by more dramatically widening one side at a midblock location. They should be clearly visible to oncoming traffic and should incorporate landscaping.
- Chicanes.** Chicanes create a horizontal diversion of traffic and can be gentler or more restrictive depending on the design. Shifting a travel lane has an effect on speeds as long as the taper is not so gradual that motorists can maintain speeds. For traffic calming, the taper lengths may be as much as half of what is suggested in traditional highway engineering.
- Speed Humps.** Speed humps can serve to slow traffic and are especially effective on residential roadways and other low-volume roadways. However, they can be discouraged by fire departments due to potential reductions in response times. While speed humps may be effective at reducing speed at the location of the hump itself, vehicles may speed up between the humps depending on the distance between them and other

Speed Hump. SOURCE: Richard Drdul



Speed Lump. SOURCE: Whatlep



Raised Crosswalk. SOURCE: Scott Batson



PMSD Sign. SOURCE: Richard Drdul



factors such as stop signs and signalized intersections. Noise impacts should also be considered as a factor due to an increase in the noise level from vehicles passing over the humps, as well as the additional noise from vehicles braking in advance of the humps and accelerating after crossing them.

- **Speed Lumps.** A speed lump is a modified speed hump where openings are added to accommodate emergency or other large vehicles so that they can use the openings without traversing the raised portion to minimize speed reduction. However, the size of the speed lumps ensures that passenger vehicles cannot likewise avoid traveling over at least one set of lumps. Like speed humps, speed lumps should be clearly visible and are generally placed midblock.
- **Speed Tables.** Speed tables provide an overall gentler transition than speed humps because they incorporate a larger flat area. They improve noise levels compared to speed humps. Speed tables should be clearly visible and are generally placed midblock.
- **Raised Crosswalks.** In addition to marked or decorative treatments, raised crosswalks make pedestrians more visible to oncoming traffic, in addition to serving as a speed reduction facility for vehicles due to the horizontal deflection of the roadway. The elevation of the crosswalk serves to reduce speeds almost like a speed hump and textured materials. Raised crosswalks should be designed pursuant to ADA standards. Since they increase visibility, it is suggested that midblock crossings be treated as raised crosswalks.
- **Raised Intersections.** Raised intersections make for an overall gentler transition compared to speed humps and can provide visually attractive traffic calming on two or more streets at once.
- **High Visibility Crosswalks.** Refer to the discussion of crosswalks in Section 4.2.1.1, Pedestrian Facilities.
- **Pole-Mounted Speed Display (PMSD) Signs.** This sign combines a speed limit sign with a radar speed feedback sign that displays the real-time speed of an approaching vehicle, which tends to make motorists reduce their speed. The signs are highly effective on roadways identified for traffic calming and should be placed at the beginning of a street section. A 200-foot visibility distance should be provided.
- **Traffic Calming Circles and Mini-Roundabouts.** Traffic calming circles are smaller than roundabouts and can fit within the area available for area intersections. They are most appropriate for

residential neighborhoods where speeding might be an issue. When left-turning traffic is heavier, an alternative solution is a mini-roundabout. The primary difference between a traffic circle and a mini-roundabout is that with the mini-roundabout, the central area must be traversable by long vehicles and cannot be landscaped. Instead, it is often raised slightly and paved with a special treatment to discourage traffic. Automobiles will generally circulate properly around the mini-roundabout, while trucks will turn over the raised area slowly.

Mini Roundabout. SOURCE: Stephen Sweeney



- **On Street Parking.** On-street parking, in effect, reduces the width of the street, leading to slower driving. Parking also separates traveling cars from the sidewalk, helping to improve pedestrian safety.

On Street Parking and Lane Making Giving Perception of Reduced Lane Width. SOURCE: Andrew Bossi



Table B: Traffic calming feature by street

Type of Traffic Calming Feature	Suggested Streets for Application
Medians	<ul style="list-style-type: none"> ■ Grapefruit Boulevard ■ Cesar Chavez Street
Median Islands	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Reduced Lane Widths	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue
Pavement Markings	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Curb Extensions/Bulb-outs	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Chokers	<ul style="list-style-type: none"> ■ Residential Streets
Chicanes	<ul style="list-style-type: none"> ■ Residential Streets
Speed Humps	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets

Speed Lumps	<ul style="list-style-type: none"> ■ Residential Streets
Speed Tables	<ul style="list-style-type: none"> ■ Residential Streets
Raised Crosswalks	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street
Raised Intersections	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street
High Visibility Crosswalks	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Pole-Mounted Speed Display (PMSD) Signs	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Traffic Calming Circles and Mini-Roundabouts	<ul style="list-style-type: none"> ■ Residential Streets
On Street Parking	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets

4.4 LANDSCAPING GUIDELINES AND PLANTING PALETTE

4.4.1 Shade and Tree Canopy

- The palette of plant materials should create an aesthetically pleasing space through a mix of colors, heights, and types of plants.
 - Trees should provide vertical interest.
 - Shrubs should be used as a natural fence that shields vehicles from the pedestrian sidewalk.
 - Flowering plant material in a variety of colors at the ground level or in planters is encouraged.
- Creative design and the use of native and drought-tolerant plants are required. The latest editions of *Western Garden Book* as well as Coachella Valley Water District Publication *Lush and Efficient-Landscape Gardening in the Coachella Valley*, should be referred to when finalizing the tree palette.
- Landscaping can be incorporated into open tree wells, planter pots, and fences.
- Ample space should be provided for the growth of trees, so the roots do not damage sidewalks as they grow.

4.4.2 Planter Boxes/Containers

There are opportunities along some of the commercial corridors such as Sixth street to incorporate planter boxes in the design. The main aim of this planter boxes will be to provide physical as well as perceived separation from the moving traffic as well as to add color splash to the streetscape. Below are suggested guidelines.

- The City planter boxes should be located in the Furnishings Zone of the streetscape and be consistent for the entire Streetscape in order to provide continuity of design.
- The City planter boxes should be made of durable material that can be easily maintained and can withstand wear and tear due to rough handling. Some materials used for City provided planter boxes are concrete, cast stone, Glass fiber reinforced concrete ay.
- Planter boxes provided by private property owner in Frontage Zone can be of varying materials and shapes as long as they are complementary to building architecture and streetscape and well maintained at all times.
- The planter boxes both in Furnishings Zone and Frontage Zone should not encroach on clear space allocated for pedestrian walkway.

- Planter boxes should be placed in such a way to provide easy access to underground utilities lids
- Planter boxes should be placed in manner to not interfere with drop off zones, disabled parking and loading zones.
- Planter boxes should be made heavy enough to not be lifted by couple individuals in order to avoid theft or easy movement of the planter boxes.
- Planter boxes should be at least 30 inches in height.
- The shape and sized of planter boxes should be complementary to the streetscape design and function.



4.4.3 Planting Palette

4.4.3.1 *Street and Ornamental Trees*

The street and ornamental tree palette proposed here are suggestive purposes and may be changed based on the design of the streetscape. However, it is suggested that the trees should be chosen to thrive in the harsh desert climate of Coachella. The latest edition of Western Garden Book should be referred to when finalizing the tree palette for the streetscapes for Pueblo Viejo District.

Street trees and ornamental trees are suggested for each street to provide its own unique identity. There are a variety of ways to configure these trees and will depend on the design of streetscape. Below is the suggested matrix and pictures by the street.

Sixth Street

Street Tree- Date Palm - Phoenix dactylifera



Characteristics

- Height: 80 - 100 feet
- Width: 20 - 40 feet
- Tree Shape: Feather Palm
- Foliage: Evergreen; Gray Green
- Leaves Shape: Frond and Pinnatifid
- Flowers: Spring or Summer; Yellow
- Fruit: Fall; Brown Drupe
- Shading Capacity: Moderate

Ornamental Tree- Pink Dawn Chitalpa - x Chitalpa tashkentensis 'Pink Dawn'



Characteristics

- Height: 25 - 35 feet
- Tree Shape: Rounded, Umbrella or Vase Shape
- Foliage: Deciduous; Gray Green, Gold
- Leaves Shape: Lanceolate to Ovate
- Flowers: Spring, Summer or Fall; Showy and Pink
- Shading Capacity: Moderate

Seventh Street

Street Tree- Chinese Flame Tree - *Koelreuteria bipinnata*



Characteristics

- Height: 20 - 40 feet
- Width: 15- 30 feet
- Tree Shape: Rounded, Umbrella or Vase Shape
- Foliage: Deciduous
- Leaves Shape: Bipinnately Compound, Medium Green, Bronze or Gold
- Flowers: Summer or Fall; Yellow
- Fruit: Fall; Prolific, Orange, Red or Rose Capsule

Ornamental Tree (Option-1) - Palo Verde - *Cercidium x 'Desert Museum'*



Characteristics

- Height: 20 - 25 feet
- Width: 12 - 15 feet
- Tree Shape: Rounded or Umbrella
- Foliage: Deciduous; Blue Green or Medium Green
- Leaves Shape: Pinnately Compound Odd
- Flowers: Spring; Bright Yellow
- Shading Capacity: Moderately Low

Ornamental Tree (Option-2) - Crepe Myrtle - *Lagerstroemia indica*



Characteristics

- Height: 20 - 25 feet
- Width: 20 - 25 feet
- Tree Shape: Oval, Rounded, Umbrella or Vase Shape
- Foliage: Deciduous; Bronze, Dark Green, Red, Gold, Orange Multicolored
- Leaves Shape: Oval
- Flowers: Summer; Showy. Lavender, Pink, Red, Rose or White
- Fruit: Fall; Brown Capsule
- Shading Capacity: Moderately Dense

Fifth Street

Street Tree- Chinese Pistache - *Pistacia chinensis*



Characteristics

- Height: 25 - 35 feet
- Width: 25 - 35 feet
- Tree Shape: Oval, Rounded or Umbrella
- Foliage: Deciduous; Medium Green, Red, Gold, Orange or Multicolored
- Leaves Shape: Pinnately Compound Even with Oblong to Elliptical Leaflets
- Flowers: Spring; Inconspicuous
- Fruit: Summer or Fall; Prolific- Red or Mostly Blue Drupe
- Shading Capacity: Moderately Dense

Ornamental Tree (Option-1) - Crepe Myrtle - *Lagerstroemia indica*



Characteristics

- Height: 20 - 25 feet
- Width: 20 - 25 feet
- Tree Shape: Oval, Rounded, Umbrella or Vase Shape
- Foliage: Deciduous; Bronze, Dark Green, Red, Gold, Orange Multicolored
- Leaves Shape: Oval
- Flowers: Summer; Showy. Lavender, Pink, Red, Rose or White
- Fruit: Fall; Brown Capsule
- Shading Capacity: Moderately Dense

Ornamental Tree (Option-2) - Buddhist Bauhinia - *Bauhinia variegata 'Candida'*



Characteristics

- Height: 20 - 35feet
- Width: 20 - 30 feet
- Tree Shape: Rounded or Umbrella Shape
- Foliage: Deciduous to Partly Deciduous, Light Green
- Leaves Shape: Lobed and Palmate
- Flowers: Spring or Summer; Showy-Fragrant White
- Fruit: Summer; Brown Pod
- Shading Capacity: Moderate

Grapefruit Boulevard

Street Tree- Arizona Ash - *Fraxinus velutina* 'Bonita



Characteristics

- Height: 30 - 50 feet
- Width: 30 - 40 feet
- Tree Shape: Conical or Oval Shape
- Foliage: Deciduous ; Medium to Light Green, Gold
- Leaves Shape: Pinnately Compound Odd
- Flowers: Spring; Inconspicuous
- Fruit: Summer or Fall; Yellow or Mostly Green Winged Seed
- Shading Capacity: Moderate

Ornamental Tree - Honey Mesquite - *Prosopis glandulosa*



Characteristics

- Height: 25 - 35 feet
- Width: 25 - 35 feet
- Tree Shape: Rounded
- Foliage: Deciduous ; Green
- Leaves Shape: Bipinnately Compound
- Flowers: Spring or Summer; Inconspicuous- Yellow
- Fruit: Spring; Brown Pod
- Shading Capacity: Low to Moderately Low

Cesar Chavez Street

Street Tree- California Fan Palm - *Washingtonia filifera*



Characteristics

- Height: 50 - 70 feet
- Width: 10 - 20 feet
- Tree Shape: Fan Palm
- Foliage: Evergreen; Dark Green
- Leaves Shape: Palmate
- Flowers: Year Round; Inconspicuous
- Fruit: Year Round; Black Drupe
- Shading Capacity: Dense

Ornamental Tree (Option-1) - Hong Kong Orchid - *Bauhinia blakeana*



Characteristics

- Height: 20 - 40 feet
- Width: 20 - 25 feet
- Tree Shape: Umbrella
- Foliage: Partly Deciduous; Gray Green
- Leaves Shape: Lobed and Palmate
- Flowers: Fall or Winter; Fragrant Pink, Purple or Rose
- Fruit: Fruitless
- Shading Capacity: Moderate

Ornamental Tree (Option-2) - Chinese Elm- *Ulmus parvifolia*



Characteristics

- Height: 40 - 60 feet
- Width: 50 - 70 feet
- Tree Shape: Oval, Rounded or Umbrella
- Foliage: Evergreen to Partly Deciduous; Glossy Dark Green, Bronze or Gold
- Leaves Shape: Elliptic to Ovate
- Flowers: Summer or Fall; Inconspicuous
- Fruit: Fall; Brown or Mostly Green Winged Seed
- Shading Capacity: Moderate

Other Options

Ornamental Tree (Option-1) - Desert Willow - *Chilopsis linearis*



Characteristics

- Height: 15 - 30 feet
- Width: 10 - 20 feet
- Tree Shape: Rounded or Umbrella
Foliage: Deciduous; Blue Green or Medium to Dark Green, Gold
- Leaves Shape: Linear
- Flowers: Spring or Summer; Showy
Fragrant- Lavender, Pink, Rose or White
- Fruit: Fall; Brown Capsule
- Shading Capacity: Low

Ornamental Tree (Option- 2) - Ocotillo - *Fouquieria splendens*



Characteristics

- Height: 18 - 20 feet
- Width: 8 - 10 feet
- Tree Shape: Vase Shape
- Foliage: Type; Color
- Leaves Shape: Ovate Leaves on Spiny Stems
- Flowers: Spring or Summer;
Tubular- Red
- Shading Capacity: Low

Ornamental Tree (Option-3) - Pomegranate - *Punica granatum*






Characteristics

- Height: 18 - 20 feet
- Width: 12 - 15 feet
- Tree Shape: Oval, Umbrella or Vase Shape
- Foliage: Deciduous; Glossy Light to Medium Green, Gold
- Leaves Shape: Oblong
- Flowers: Spring or Summer;
Showy- Orange, Pink or Red
- Fruit: Fall; Prolific- Brown or Red
Berry Shading Capacity: Moderate to Dense

4.4.3.2 Container Plantings

Various varieties of grasses, dwarf shrubs, flowering perennials and succulents are suggested here planting in containers in the public realm. These plants are drought tolerant and well suited to Coachella region. The palette is suggested for Pueblo Viejo District as a whole. These plants can be mixed and matched to create unique combination along each streetscape that have planter boxes. The suggested plants can be used by private property owners for planting in Frontage zone of they choose. The latest edition of Western Garden Book should be consulted to determine any planting that is not suggested in the palette below for container planting in public realm as well as areas open to public in private realm.

Grasses and Grass-like Plants

Carex		
		<p><u>Blooming</u></p> <p>Early Summer</p>
Festuca		
		<p><u>Blooming</u></p> <p>Summer</p>
Stipa		
		<p><u>Blooming</u></p> <p>Non-flowering</p>

Dwarf Shrubs

<p>Nandina</p>		 <p>Blooming</p> <p>Late Spring, Early Summer</p>
<p>Calliandra</p>		 <p>Blooming</p> <p>Seasonal Bloomer</p>
<p>Dwarf Rose</p>		 <p>Blooming</p> <p>Spring to Frost</p>
<p>Convolvulus</p>		 <p>Blooming</p> <p>Late Summer</p>

Flowering Perennials

Teucrium		<u>Blooming</u> Summer
		
Origanum		<u>Blooming</u> Summer to Fall
		
Salvia		<u>Blooming</u> Summer to Fall
		

Succulents

Sedum	
	<p><u>Blooming</u></p> <p>Summer to Fall depending on the type on the species</p>
Echeveria	
	<p><u>Blooming</u></p> <p>Summer to Fall depending on the type on the species</p>
Aloe	
	<p><u>Blooming</u></p> <p>Seasonal Bloomer</p>
Agave	
	<p><u>Blooming</u></p> <p>Rarely Blooms</p>



STUDY REPORT
PARKING STUDY



Executive Summary

The City’s General Plan states that Coachella is at a cusp of growth that will significantly transform the City from a small town to a medium-sized city. The Pueblo Viejo district is poised to witness higher-intensity development and is already experiencing an influx of new development including the new County building and the new public library. Other proposals for development are being considered. The General Plan and Pueblo Viejo Vision Plan both envision this area for mixed-use development with a mix of commercial/retail and multifamily residential uses. This vision mirrors trends in retail development and real estate market realities that show more success for experiential retail with an increasing intensity of surrounding residential development. As the intensity of the Pueblo Viejo district increases, so will the demand for parking. This parking study’s objectives are to understand the current parking supply and future projected supply based upon differing development scenarios and regulations, and future projected parking demand based on expected development envisioned by the General Plan and the Pueblo Viejo Vision Plan. This study provides findings and recommendations to:

- Understand potential parking supply and demand scenarios.
- Right-size parking standards to ensure both workable development scenarios and effective parking operations.
- Determine contingencies for balancing future projected parking supply and demand through demand reduction strategies, thresholds to trigger creation of additional supply, and more efficient use of existing supply.

This study finds that the Pueblo Viejo district currently has a sufficient parking supply to meet the existing demand. It should be noted that the current distribution of available on- and off-street spaces and restricted on- and off-street spaces do create inefficiencies in the allocation and distribution of parking supply, which gives a perception of a smaller available supply. When examining each block of the district, the blocks with a deficient parking supply—namely the blocks on the east side of Sixth Street—are not within a 5-minute walking distance from the blocks that may have parking spaces. This mismatch is mainly offset by available on-street parking spaces around Veteran’s Park and City Hall.

Although existing parking supply and demand is important, it is the management of future projected supply and demand, based upon development scenarios envisioned by the City’s General Plan and Pueblo Viejo Vision Plan, that ensures successful demand and supply policies and successful parking management strategies.

Findings

Assumptions were included in custom modeling of parking within the Pueblo Viejo district to test potential future parking standards for off-street parking supply and anticipated new on-street parking supply. The study found that adequate supply could be created with the proposed standards, although the issue of market viability, when including the costs of building parking structures (to achieve the envisioned intensities), may call into question the ability of the private sector alone to support the cost of off-street parking supplies without assisting mechanisms to generate and share revenues for construction, operations, and maintenance of parking. Below is a summary of findings of the study:



- Of the total parking spaces in the study area, approximately 60 percent are on-street parking spaces and are publicly accessible. When off-street parking is factored in, a total of 80 percent spaces are publicly accessible in the Pueblo Viejo district. (See **Figure 2**, **Figure 4**, and **Figure 6**.)
- Of the total off-street parking spaces, approximately 50 percent have restricted access, meaning they cannot be used by patrons of other businesses, per private restrictions on the use of those spaces for the current tenant/owner's establishment only. While overall there seems to be surplus parking, restricted access creates a false perception of parking. (See **Figure 6**.)
- Approximately 80 percent of the total spaces are publicly owned. This includes on- and off-street parking spaces. Around 10 percent of the publicly owned spaces are not publicly accessible. Most often these spaces are not fully utilized and create a false perception of parking deficiency. (See **Figure 5**.)
- The current parking supply and demand has an imbalance when examined on a per-block basis. This examination is important as parkers prefer to reduce overall walk times, and supply that is outside of a 5-minute radius is less likely to be considered by parkers. The blocks on the east side of the district have a deficiency of supply while those on the west side have surplus of availability. (See **Figure 7**.)
- Currently, most of the surplus supply of parking falls within the walkshed of the blocks with a deficiency of parking. However, this is mainly due to the availability of additional on-street parking. Off-street parking supply, while available, is primarily privately restricted to patrons of tenants/owners of specific establishments.
- In both future build scenarios (medium-built and full-built) that the study tested, a projected surplus of parking supply is available if on-street parking is included. Most of the blocks that show a surplus of parking are anticipated to be targeted for future development intensification.
- Based upon the study model, both future scenarios would require structured parking to attain the development intensities envisioned in the General Plan and Pueblo Viejo Vision Plan. This cost of development may be a limiting factor due to the cost of construction and economic viability of structured parking versus surface parking. The estimated cost of structured parking could be close to \$32 million in today's value and will satisfy only one-third of required parking demand in the medium-built scenario. In the full-built scenario, the estimated cost is around \$109 million and would satisfy approximately 75 percent of required parking demand.

Recommendations

Based upon the findings, strategies for creating adequate parking supply and managing demand are recommended to both support the Pueblo Viejo district's vision for growth and ensure effective supply/demand balance for parking as the district grows. It is important that regulations do not over-prescribe parking, especially as the future demand for parking may change as overall car ownership decreases, more choices in modes of travel exist, and ridesharing and autonomous vehicles become more prevalent. Over-prescribing off-street parking results in higher development costs, sprawl, increased vehicle miles traveled, and higher infrastructure costs. It may also inhibit the development of the Pueblo Viejo district if the costs of compliance do not result in the economic returns required for the private sector to make investments in the district. Balancing regulations with contingencies to address future supply/demand imbalance may result in less costly development and shared public, private, and public/private partnerships that create more efficient parking options. Several recommendations were also suggested in the document.



Recommendation 1: Lower Parking Ratio - Lowering parking ratio, eliminating minimum parking requirements, and better management of parking spaces can reduce parking costs.

Too much parking is as harmful as too little due to the impacts on development costs, government costs, and increased infrastructure due to lower-density development. Some communities are eliminating minimum parking requirements and introducing maximum requirements, and many are significantly reducing minimum requirements. Various factors affect parking requirements such as geographic location, residential density, employment density, land use mix, transit accessibility, carsharing, walkability and bikeability, demographics, income, housing tenure, pricing, and sharing/overflow.

Recommendation 2: Cooperative Parking Arrangement - Developing cooperative parking agreements with property owners and tenants can improve parking efficiency and convenience for all Pueblo Viejo district's customers and visitors.

Due to owner/tenant restrictions, many off-street parking spaces are restricted to patrons of an individual establishment. This creates inefficiency in parking distribution with empty usable spaces designated for other establishments located near establishments with higher parking demand. If businesses can share parking spaces, the parking supply needed to satisfy the demand can be reduced.

Recommendation 3: Shared Centralized Parking - Shared and centralized structured parking can help reduce some of the cost burden on developers and make the Pueblo Viejo district attractive for investment.

City-provided shared parking can add to the reduction in parking supply by the new development. In order to make the area more attractive to the developers and lower their cost of construction, the City can provide surface and/or structured parking that can be shared by the private sector.

Recommendation 4: Bicycle Parking - Providing safe and convenient biking and walking facilities can make bicycles a viable choice and reduce car trips, leading to a reduction in required spaces.

Many of the City's residential neighborhoods are a short bicycle ride from the Pueblo Viejo district. More available alternatives to the personal automobile will reduce parking demand in the district. Provision of walking and biking facilities, including bicycle parking, will promote cycling as a viable mode of transportation that people may choose if they perceive it as a realistic and safe method for short trips.

Recommendation 5: Time-Restricted Parking - Time-restricted parking can effectively increase the turnover rate of on- and/or off-street parking spaces.

Timed parking frees up immediate parking spaces for short duration parkers and encourages parkers interested in long-term parking (over two hours, all day, or commuter parking) to use off-street and other facilities. Enforcement of frequent and consistent regulation is vital for the success of this strategy.

Recommendation 6: Paid Parking - Paid parking can effectively increase turnover rates and the supply of convenient parking spaces.

Like timed parking, parkers are more efficient with the amount of time they occupy a space. The advantages of paid parking, based upon rates, are higher efficiency, more flexibility for the parker if more time is required, and the generation of revenue for construction, operations, and maintenance of parking operations. Other benefits of paid parking are the easier enforcement of mechanism and more convenience for the parker allowing them to add parking time if needed through additional fees.



Recommendation 7: Overflow Parking - Railroad Land - Overflow parking can be created by converting the land next to the railroad tracks.

The City can explore the option of leasing the railroad land for a public parking lot along Grapefruit Road near the Fourth Street cross-street. This area can also be used as an employee parking area for the businesses in its walkshed. However, there needs to be a certain level of demand before investing in the overflow parking.

Recommendation 8: Temporary and Overflow Parking - Use of school parking and private parking areas can provide temporary supply of parking during events.

A joint use agreement with the school district to make school parking lots available for limited activities, such as weekend events, can provide more than 100 parking spaces and can be in the walkshed of more than half of the blocks in the study area. Using other existing private parking areas for overflow parking should also be explored prior to investing in new parking areas.

Recommendation 9: Effective Use of Leftover Space - Leftover on- and off-street space can be used to provide parking for motorcycles, bikes, and/or compact cars, thus adding to the parking supply.

Using leftover spaces of a lot, such as corners and undeveloped land of a parcel, to provide parking for motorcycles, bikes, and/or compact cars will increase parking spaces without large investment and major construction.

Recommendation 10: Converting Parallel to Angled Parking - Some of the wider streets that do not need bike facilities can be used to accommodate angled parking instead of parallel parking.

Angled parking may interfere with the ability to provide on-street bicycle facilities. There needs to be consideration as to the use and prioritization of the street with respect to provision of angled parking versus bicycle facilities.

Recommendation 11: Signage and User Information - Signage, wayfinding, and readily available information on available parking spaces can be a useful tool in reducing perceived parking shortage.

Various ways to disseminate this information are via wayfinding signage, area maps, brochures, websites, electronic guidance systems, smart apps, and so on.

Recommendation 12: Employee Parking - Encourage employees to not park in the most convenient customer parking spaces.

If employees are encouraged to not park in these parking spaces and instead use the spaces that would otherwise be unused, it will reduce the perception of parking shortage and need to oversupply parking.

Recommendation 13: Unbundle Parking - Unbundling parking can lower the cost for the developer as well as the user and reduce surplus parking.

Unbundling refers to renting or selling parking separately rather than automatically including it with the price of building space. For example, rather than renting an apartment with two parking spaces for \$1,000 per month, the apartment would rent for \$800 per month, plus \$100 per month for each parking space. This strategy allows the owners to buy or rent only required number of spaces. Parking permit programs can be used to avoid adverse effects of unbundling on nearby neighborhoods.



Financing and Organizational Mechanisms for Implementation

Various sources of financing and organizational mechanisms can be used to implement these strategy recommendations. Some recommendations require revenue sharing, revenue generation, and mechanisms for sharing the cost of infrastructure development between property owners and/or the city. State law allows for various districts that may be formed to help finance and operate infrastructure such as shared parking. Some implementation strategies are described in the report and include:

- Parking Assessment District
- Business Improvement District
- In-Lieu Parking Fee
- California Infrastructure and Economic Development Bank (IBank)
- Public-Private Partnership
- Transit Grants
- Mello-Roos
- Infrastructure Financing District
- Joint Use Agreement



Introduction

The purpose of the study is to inform the implementation of the City’s General Plan and Pueblo Viejo Vision Plan, including development standards and design guidelines, through the analysis of existing and projected future parking supply and demand based on the type of development envisioned by these plans. The study provides:

- An analysis of current conditions—existing supply and estimated demand.
- Projections of future supply and demand based upon assumed development scenarios.
- Recommendations for implementation strategies for development standards and contingency planning for parking optimization, demand reduction strategies, and contingencies for increasing supply as future demand increases.

As the vision of the Pueblo Viejo district is realized, understanding the changing parking needs of the area will be vital to its success. Parking is generally one of the largest land uses in the community. Often there is an oversupply of parking in suburban areas but there is a perception of shortage. This is because the available parking is often not convenient or close to the parker’s destination.

It is important to understand the optimal balance between supply and demand and dedicate only the minimal required amount of land to parking, so the remainder of the land can be used for higher valued (and assessed) development. This parking study provides a basis to make decisions on optimization of parking. It also allows the City to reevaluate the parking standards and/or provide incentives for the implementation of best practices that create the proper balance between parking supply and demand and an equitable distributed burden for parking costs.

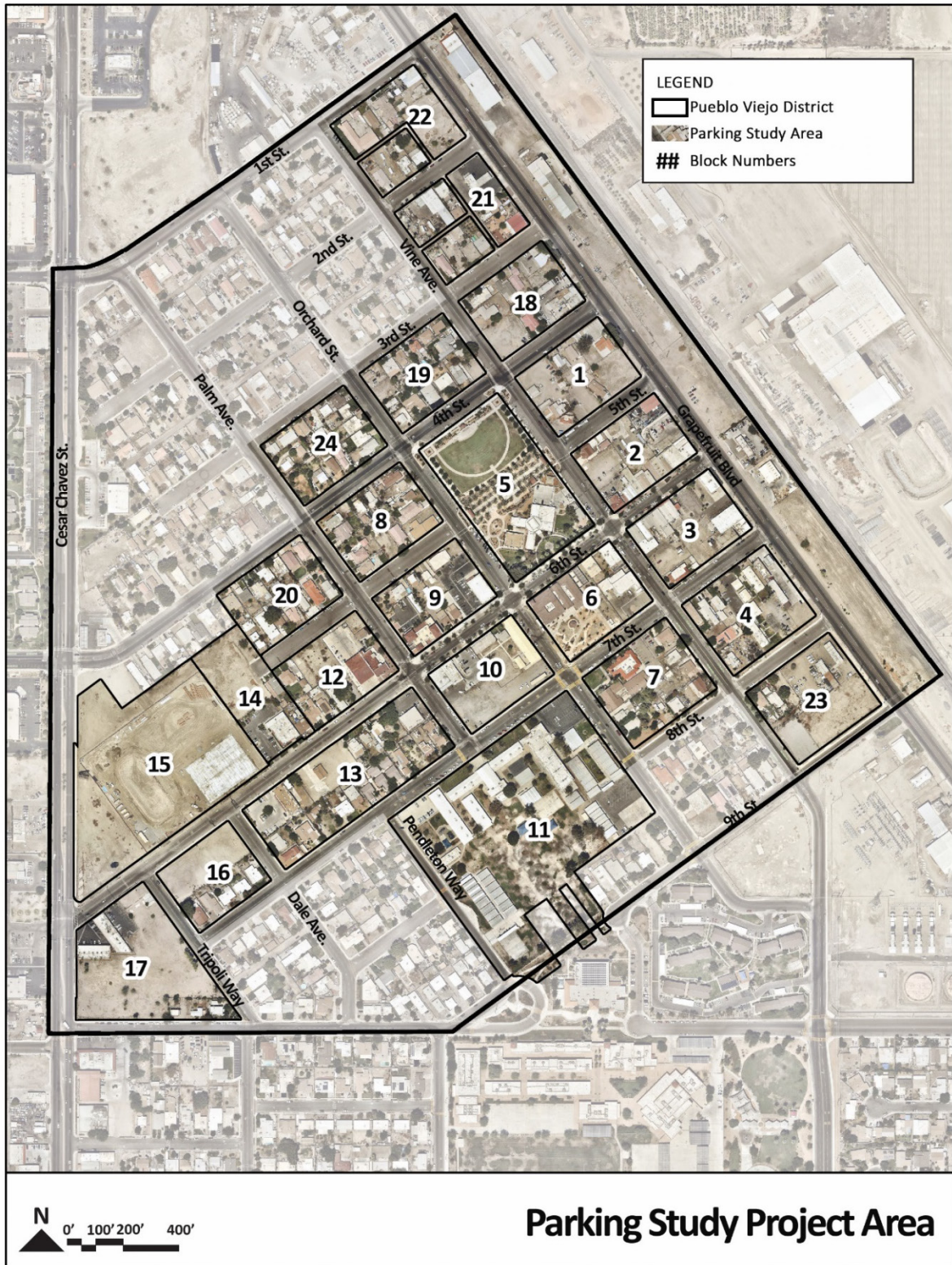
Study Area

This parking study was conducted for the commercial area of Pueblo Viejo district. The boundaries of this parking study are shown in **Figure 1**. The study area includes 24 blocks consisting of civic, residential, retail, office, restaurant, entertainment, auto dealership, and light industrial land uses, mostly in one- to two-storied structures. The city’s General Plan and Pueblo Viejo Vision Plan both envision more intense mixed-use office/commercial/retail/multifamily housing development consisting of more integrated and higher-intensity development.

Mixed-use development trends started in the mid-2000s and have become one of the most active development models. In 2017, 80 percent of the amount spent on construction in United States dealt with some version of mixed-use development. The surge of this type of development hinges on the numerous benefits it provides to developers, investors, public entities, and users. Mixed-use development:

- Creates walkable vibrant communities that can draw potential homebuyers and other tenants, leading to high rental demand and economic development.
- Can provide housing choices to a variety of demographic groups and promote the concept of aging in place.

Figure 1: Parking Study Area Map





- Lessens the overall risk for real estate investors due to a wide variety of tenants (commercial, residential, etc.).
- Lowers infrastructure and maintenance costs due to intensity of development.
- Promotes invest in amenities due to spreading of risk across various uses and lower infrastructure costs.
- Creates a more sustainable environment for retail, which is rapidly changing due to increased demands for experiential and social opportunities and a decrease in the traditional consumer-driven model.

The trends in mixed-use development indicate that people living in multifamily residential developments are opting to live in smaller spaces if the space is well-organized and has an ideal layout. Amenities, including desirable common areas and retail in the immediate vicinity, are very important to these residents. For mixed-use buildings, food establishments are seen as the most important type of retail. Other amenities that work well with residential mixed-use developments are hospitality, full-service gyms, and creative office space.

Methodology

The study area was divided into 24 blocks and numbered for analysis purposes. A customized parking model was prepared on a block level. The model required certain input fields that were either readily available from the County GIS data or from the City of Coachella. These included land use categories, parcel area, building footprint, built-up area, number of stories, and existing parking spaces.

The block-by-block basis of the parking study both aggregates the available on-street parking and accounts for walkshed, or the length people are willing to walk from the parked car to the destination. This helps analyze inefficiencies based on the parker's ultimate destination within the district. Each Pueblo Viejo district block is approximately 250–300 feet. An individual walkshed is usually assumed to be about 0.25 miles or 1,320 feet. Hence, it can safely be assumed that people may be willing to walk up to 3 to 5 blocks to reach the destination from their place of parking, although most parkers prefer to park as close to their initial destination as possible.

A. Assumptions

The parking model was developed with assumptions built into the model. These assumptions could be changed on the block level by changing the development scenario. The assumptions were guided by the city's General Plan, existing conditions, general market knowledge, professional opinion, and discussions with the city staff.

- **FAR for new construction = 3.0:** Floor area ratio (FAR) is the ratio of allowable coverage of floor space compared to the overall area of the parcel. This helps govern the intensity of the development. Based on the City's General Plan, the FAR recommended for the Pueblo Viejo district commercial area is 0.5 to 3.0. An upper limit of the recommended FAR is chosen for new construction as most of the existing development is less than 1.0 FAR and the descriptions of development encouraged by the General Plan and Pueblo Viejo Vision Plan envision more intense development consisting of 2-5 stories of retail, shops, and housing.



- **Average gross floor area (GFA) dwelling unit (DU) = 2,000 square feet:** A studio unit typically starts with a net floor area (NFA) of 500 square feet while a three-bedroom unit may be up to 2,000 square feet. The NFA is the usable space of the dwelling unit and does not include space for garage, amenities, circulation (corridors, elevators, stairs), and other utilities. Adding these to the NFA provides the gross floor area (GFA) of the unit. If a mixed-use development might have a variety of units, the GFA per dwelling unit is assumed as 2,000 square feet for simplification of the model.
- **Percent of residential and commercial/institutional use in mixed use = 30 percent - commercial/institutional; 70 percent - residential:** Based on the current trends of development, there seems to be a higher demand for residential in comparison to the commercial/institutional development. This leads to the assumption of 70 percent residential, and 30 percent commercial development for mixed use.
- **Parking for commercial/institutional/office development = 4 spaces per 1,000 square feet; residential development = 1.33 per DU; industrial development = 1.13 per 1,000 square feet:** A matrix was developed to compare parking regulations for different land uses of seven communities: Coachella, Anaheim, Glendale, Alhambra, Palm Springs, West Hollywood, and Pasadena. These findings were also compared to the Institute of Transportation Engineers (ITE) parking generation recommendations. ITE is an international membership association of transportation professionals and has developed numerous standards related to mobility planning including parking generation. This is based on nationally collected data. The parking ratios provided by ITE are sometimes at the higher end of the spectrum, are based upon a limited sample of studies, and should be adjusted to the reality on the ground.

The assumptions for various land uses in the parking model are a result of comparing the parking standards of the seven communities with ITE guidelines. To simplify the ratio for commercial use and consolidate retail, restaurant, and office use, a computation was done based on existing square footage of these uses and then aggregated based on total square footage. This resulting ratio was 3.8, which was rounded to 4.0 for simplification. The comparison table can be found in **Appendix-PS-1: Zoning Comparison**.

- **Parking for mixed-use development = 3.33 per DU:** Based on the comparison matrix explained above, Palm Springs was the only community to have a parking ratio for mixed-use development. It ranges from 2.08 to 3.33. Glendale has a point system for reducing parking requirement. When the mixed-use numbers for medium-built and full-built scenarios were applied to Glendale's system, the required ratio ranged from 2.3 to 3.3. Due to the current development patterns and similar walkability scores between both Palm Springs and Coachella, as compared with Glendale, the higher ratio was selected. See **Recommendations** for more discussion on off-street parking requirements and potential reduction of this ratio in off-street parking standards.
- **Percent of lot area to be open space = 10 percent:** The zoning code developed for the General Mixed-Use district for Coachella (C-G-PV) requires a 10 percent open space for developments over



0.5 acre. Open space for new development or the existing developments that may transition to commercial use is assumed to be 10 percent of the lot area.

- **Off-street optimal parking factor = 90 percent:** Percentage occupancy rate at a given time is called optimal parking factor. Generally, a vacancy rate of 10 percent is considered healthy when designing parking for commercial areas to allow for circulating parkers to find parking spaces. This parking factor is applied to off-street parking.
- **Use on-street parking spaces to mitigate deficiency:** It is assumed that on-street parking within a certain distance from a proposed development will be used in off-setting total off-street parking required for a development, as the purpose of regulations is to develop a healthy relationship between total supply and total demand within the district, whether parking is available on-site or within the public rights-of-way. If on-street parking is not used to satisfy demand, an overabundance of supply would occur as more spaces are required than demand generated.

Based upon the assumptions above, algorithms were developed to calculate the current demand estimate and projected forecasts for two future-built scenarios based upon existing and projected future land uses. While the existing demand was estimated based upon current land use, future projected demand was calculated based upon two anticipated scenarios: medium-built and full-built based on the City's General Plan. These changes in land use are shown in **Figure 8** and **Figure 10**.

It should be noted that these scenarios are not based upon current development proposals and would require private sector acquisition of land and appropriate development approvals. The scenarios are best guesses based upon development trends and the visions laid out by the city's General Plan and Pueblo Viejo Vision Plan. The land use changes are highly dependent on the availability of land for development, market trends and conditions, new technologies, and so on. It is assumed that the overall impact of development at the district level might remain similar to the assumed changes. The difference between the medium- and full-built scenarios and the underlying assumptions are explained later in the **Future Parking Scenarios** section of this report.

The current and the two projected future-built scenarios were then translated to parking demanded by use and projected parking supply provided by each block. Parking deficiencies and surplus, based upon the ratio of supply versus demand, were also calculated. For current conditions, existing supply and estimated demand using demand assumptions for the single use listed above were used to calculate the current deficiency/surplus. The future-built scenarios assumed that any new construction would be mixed-use development and provide off-street parking supply to counteract the demand. As discussed above, the parking supply needed by mixed-use development was set at the rate of 3.33 spaces per 1,000 square feet of development. Any existing structure that transitions from a residential to a commercial use will utilize the open space available to provide 10 percent open space per zoning and reconfigure the rest to maximize parking supply based on the use. Hence, the deficiency/surplus was calculated based on the resultant supply. In addition, all three scenarios used on-street parking for additional supply.

The block-by-block calculations were summarized in one table and translated to a map graphic for the scenario to provide a snapshot of entire study area. The table also lists estimated development square footage by use and the parking square footage needed to support the development.

Three scenarios were developed using the model:



- **Current Supply and Demand:** The model shows existing conditions on ground.
- **Medium-built:** The model accounts for known changes in the future, including on-street parking changes as well as parcels that are easier to develop (e.g., vacant parcels).
- **Full-built:** This is the most ambitious development scenario and accounts for the eventual transformation of most blocks in the study area.

Maps were developed to show the changes per parcel based on whether there is a change in use, structure, or both. The maps also show the parking surplus or deficiency on a block-by-block basis.

Existing Conditions

B. Existing Parking Supply

Existing parking supply includes the inventory of private or publicly owned parking spaces that currently exist, are in the process of construction (for example, the new county building), or are approved and will soon be constructed in the short term, at the time of this study. The existing parking inventory, for both on-street and off-street parking, was compiled using aerial imagery and a review of existing permits for spaces currently under construction or soon to be constructed. Verification of the inventory, as well as an inventory of parking restrictions, was done through a windshield survey. The inventory also identified the restricted parking spaces that are designated by signs for specific purposes, such as Americans with Disabilities Act (ADA) accessible parking, loading/unloading spaces, timed parking, pickup and drop-off, owner-restricted parking, and electric vehicle parking.

There is a total of 1,568 parking spaces in the study area. Of these, 901 are on-street spaces and 667 are off-street parking spaces. The following sections provide further detail on current parking supply.

Existing On-Street Parking Supply

On-street parking refers to the designated spaces located within the street rights-of-way in the Pueblo Viejo district. These include traditional angled parking spaces and parallel parking spaces that are situated between the travel lanes and the street curb. The existing on-street parking spaces in the study area were inventoried on a block-by-block basis to verify the number of parking spaces and restricted spaces. This is depicted in **Figure 2**.

There are presently 901 on-street parking spaces in the study area, of which 45 are restricted spaces. **Table 1** shows the division of existing parking spaces based on restrictions.

Table 1: On-Street Parking Spaces Based on Restrictions

Restriction Type	Parking Spaces
Regular Parking	856
ADA Accessible Parking	12
Loading/ Unloading Spaces	2
Timed Parking	5
Pickup and Drop-off Parking	23



Electric Vehicle Parking	3
TOTAL PARKING SPACES	901

Off-Street Parking Supply

Off-street parking spaces include facilities such as parking lots and structured parking. These can be publicly or privately-owned facilities. There are a total of 667 off-street parking spaces in the study area, of which 35 are restricted and designated as ADA-accessible spaces. **Figure 4** and **Table 2** show a summary of the existing off-street parking supply.

Table 2: Off-Street Parking Spaces Based on Restrictions

Restriction Type	Parking Spaces
Regular Parking (No Restriction)	632
ADA Accessible Parking	35
TOTAL PARKING SPACES	667

In addition to the above inventory, ownership of the lots was also studied. Ownership refers to publicly or privately held parcels. **Figure 5** shows off-street parking by ownership. Off-street parking provided in parcels owned by the City, school district, or any other public entity are referred to as publicly owned spaces and the remainder of off-street spaces are referred to as privately-owned spaces. **Table 3** shows the summary of these spaces.

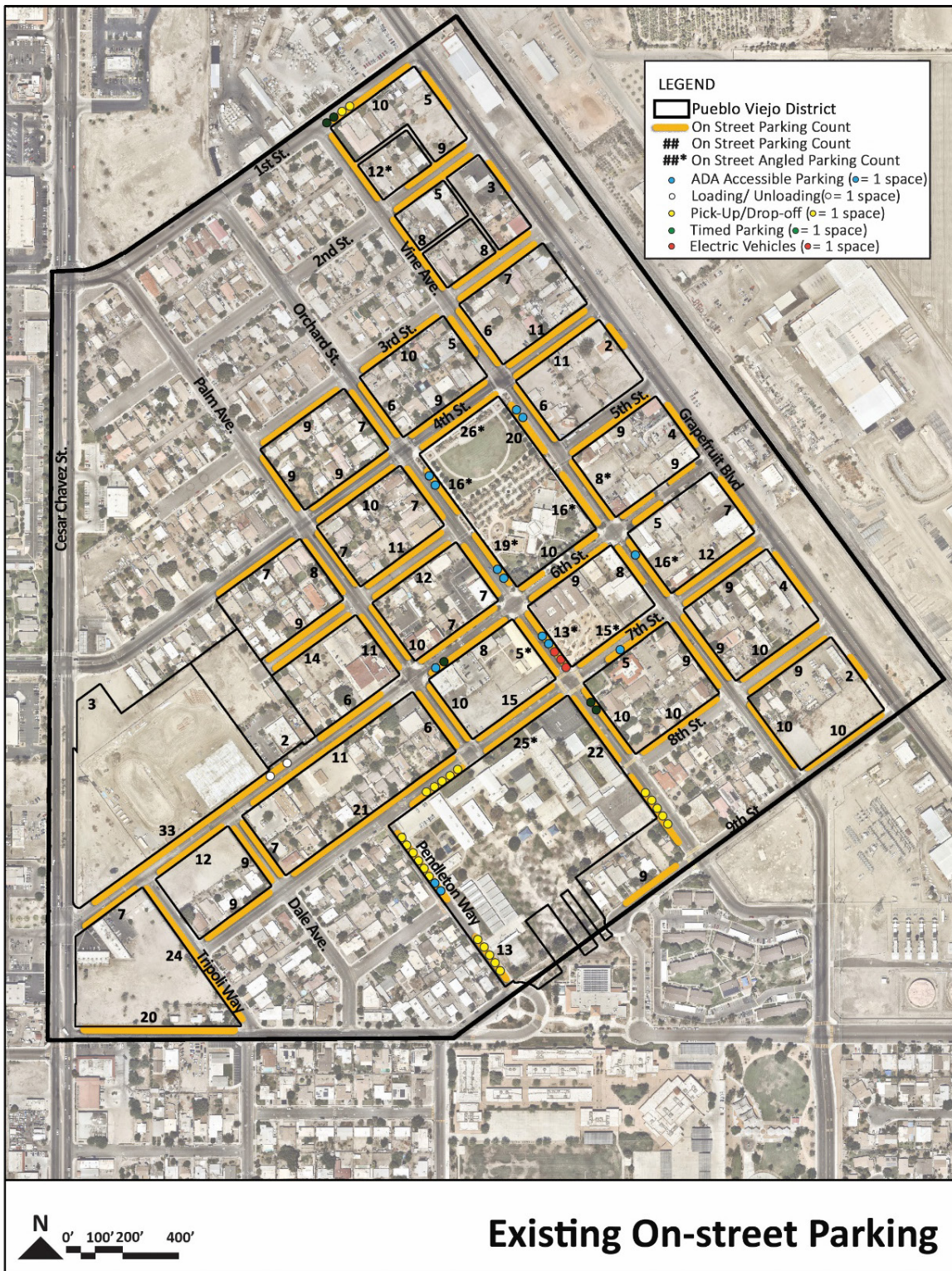
Table 3: Off-Street Parking by Property Ownership

Ownership Type	Parking Spaces
Publicly Owned Property	353
Privately Owned Property	314
TOTAL PARKING SPACES	667



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Figure 2: Existing On- and Off-Street Parking





Access restrictions can be placed on both publicly and privately-owned off-street parking. Owner/tenant restricted lots or spaces are those that indicate the lot or spaces can only be used for patrons of a specific establishment as opposed to unrestricted lots or spaces that allow for public parking regardless of the parker’s destination. The restricted lots are generally marked by a sign that warns against parking in that lot. In some cases, the lots are gated. An example is shown in **Figure 3**. **Figure 6** depicts the access restrictions and **Table 4** summarizes the same.

Figure 3: Restriction Sign



Table 4: Off- Street Parking Access Restriction

Access Type	Parking Spaces
Restricted Parking Access	328
Unrestricted Parking Access	339
TOTAL PARKING SPACES	667

Owner/tenant restricted parking is a common issue in downtowns and creates inefficiencies in downtown parking supply, due to the distribution of parking spaces near primary parking destinations. It also increases trip generation as patrons must move their vehicles when visiting multiple district destinations, reduces the amount of time a patron may spend at more than one establishment, decreases the patron’s ability to find a convenient parking space, and results in a perceived decrease in available parking supply. While the restricted parking lots and spaces might be underutilized, they are still not available to all Pueblo Viejo district parkers. Land that can be used for development is lost to provide additional parking to satisfy this inefficiency. These inefficiencies are often mitigated by higher off-street parking requirements, resulting in decreased availability of land for higher-assessed development as well as increased construction costs and ongoing operations/maintenance costs for both the public and private sectors due to more infrastructure being required for parking. Removing owner/tenant parking restrictions can eliminate inefficient distributions of supply and more efficiently distribute demand throughout the Pueblo Viejo district. Recommendations for cooperative parking arrangements can be found in the **Recommendations** section.



Figure 4: Existing Off-Street Parking

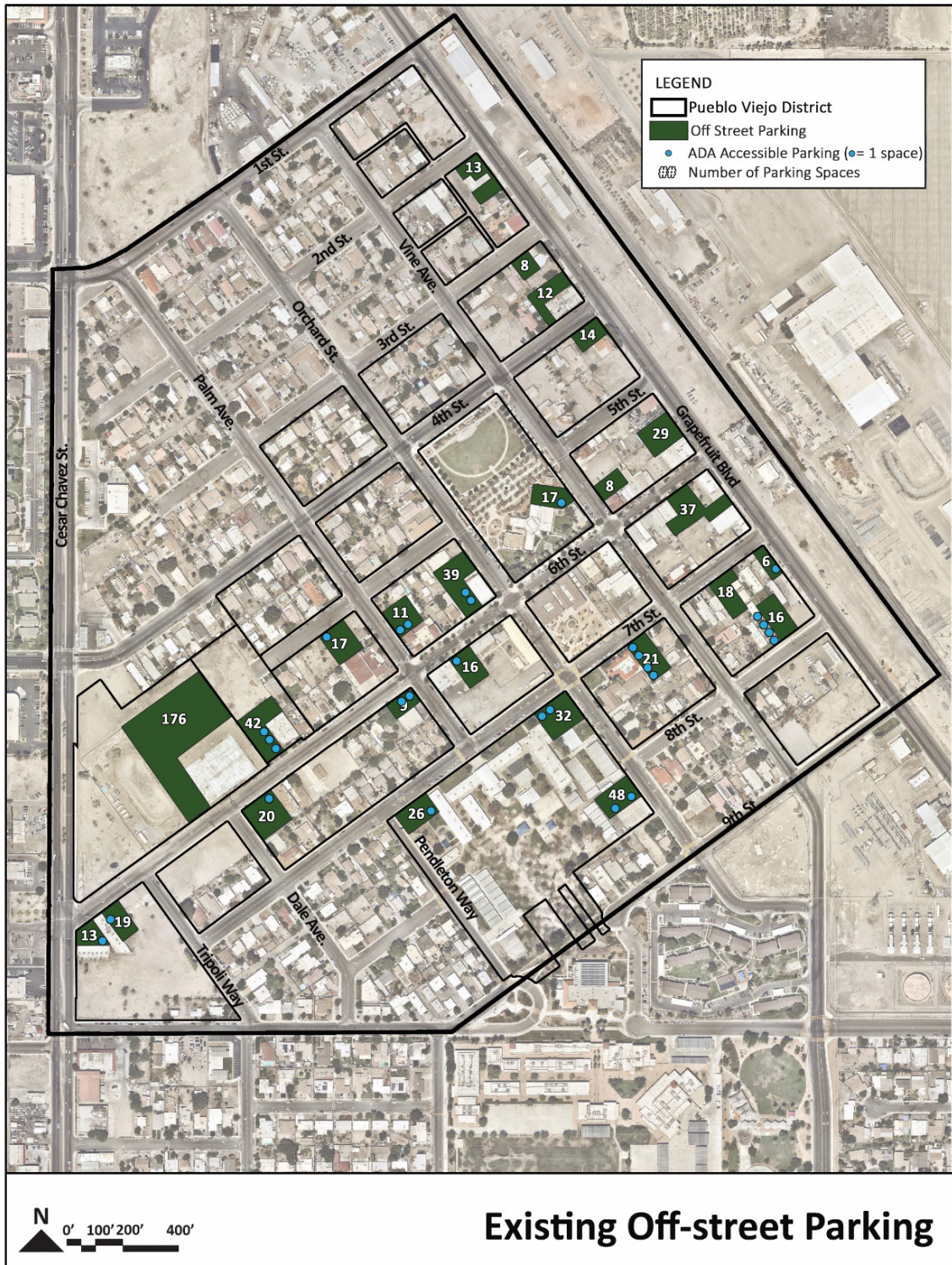




Figure 5: Off-Street Parking Lots by Ownership

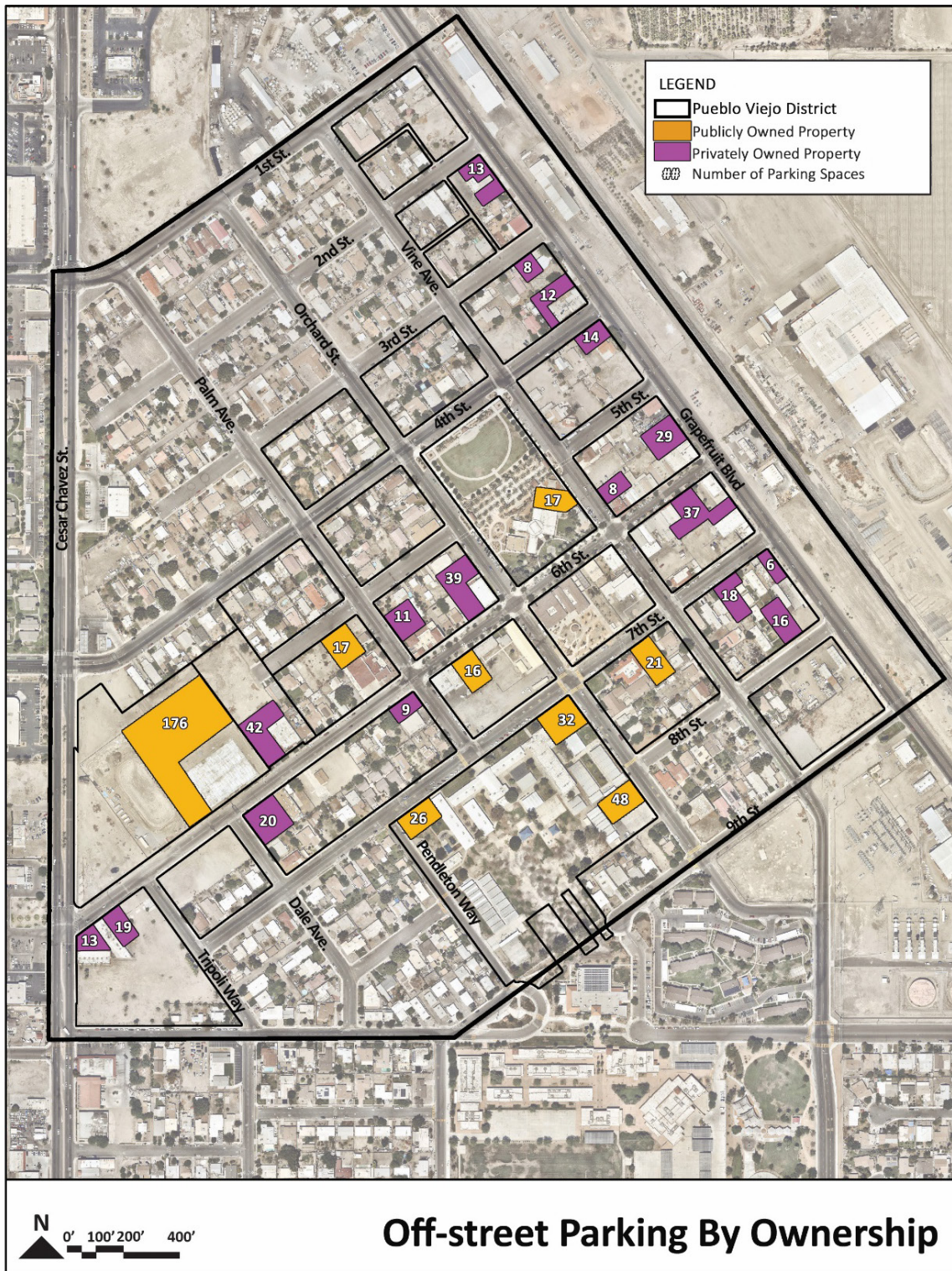
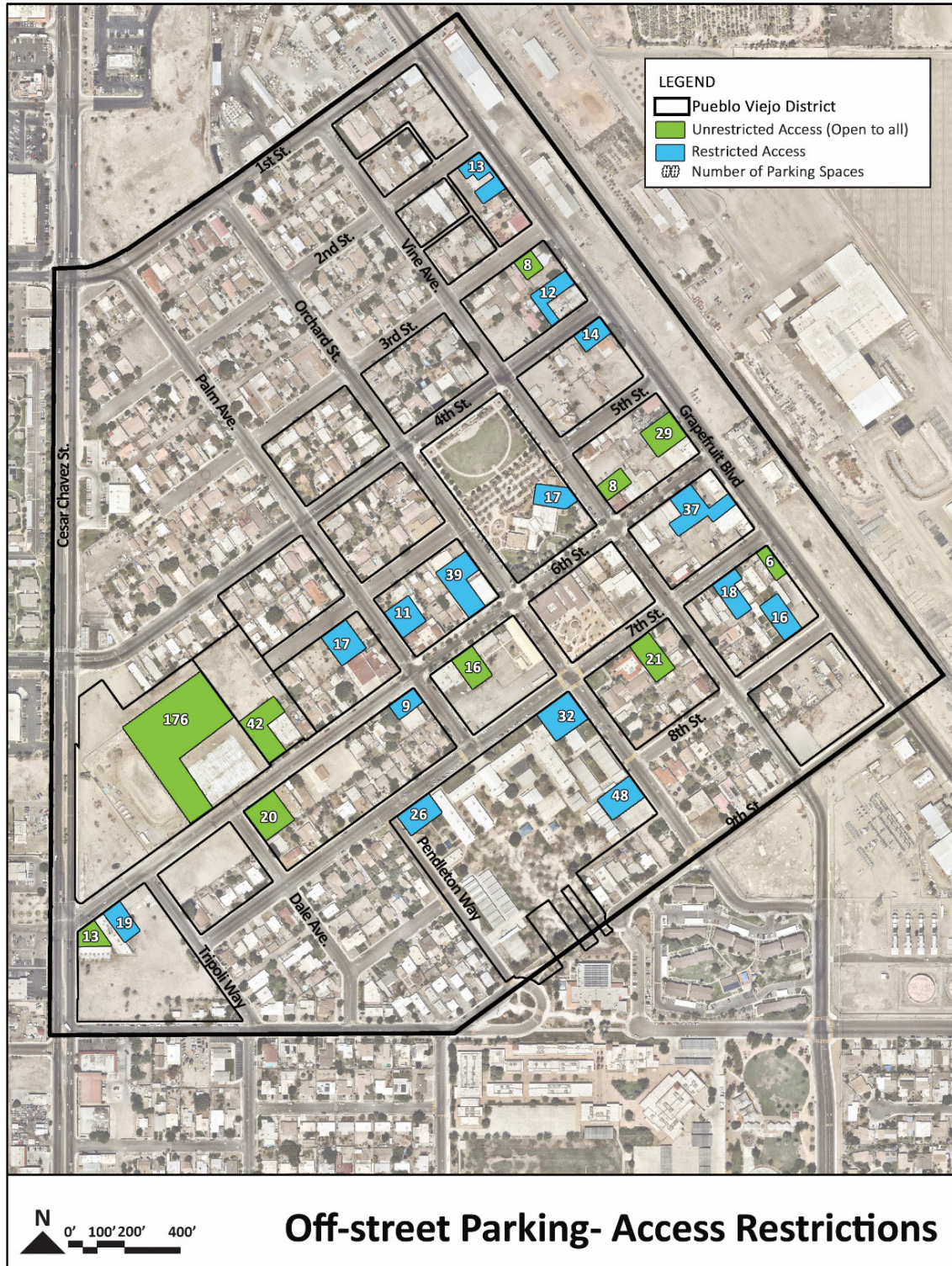




Figure 6: Off-Street Parking Lots by Access Restriction





C. Current Surplus/ Deficiency

Current demand is estimated based on existing study area uses. As indicated in the **Methodology** section of the report, current demand is estimated based on the assumptions made in the model individual separated uses. It also includes the approved plans that are currently under construction, such as the County building and the new library building.

The current estimated demand is compared to the existing parking supply to calculate the current deficiency/surplus of parking in the district. There is a surplus of parking supply when examining the study area as a whole; however, as illustrated in **Figure 7**, there are deficiencies and surpluses on a block-by-block basis due to the distribution of demand generators and existing parking supplies.

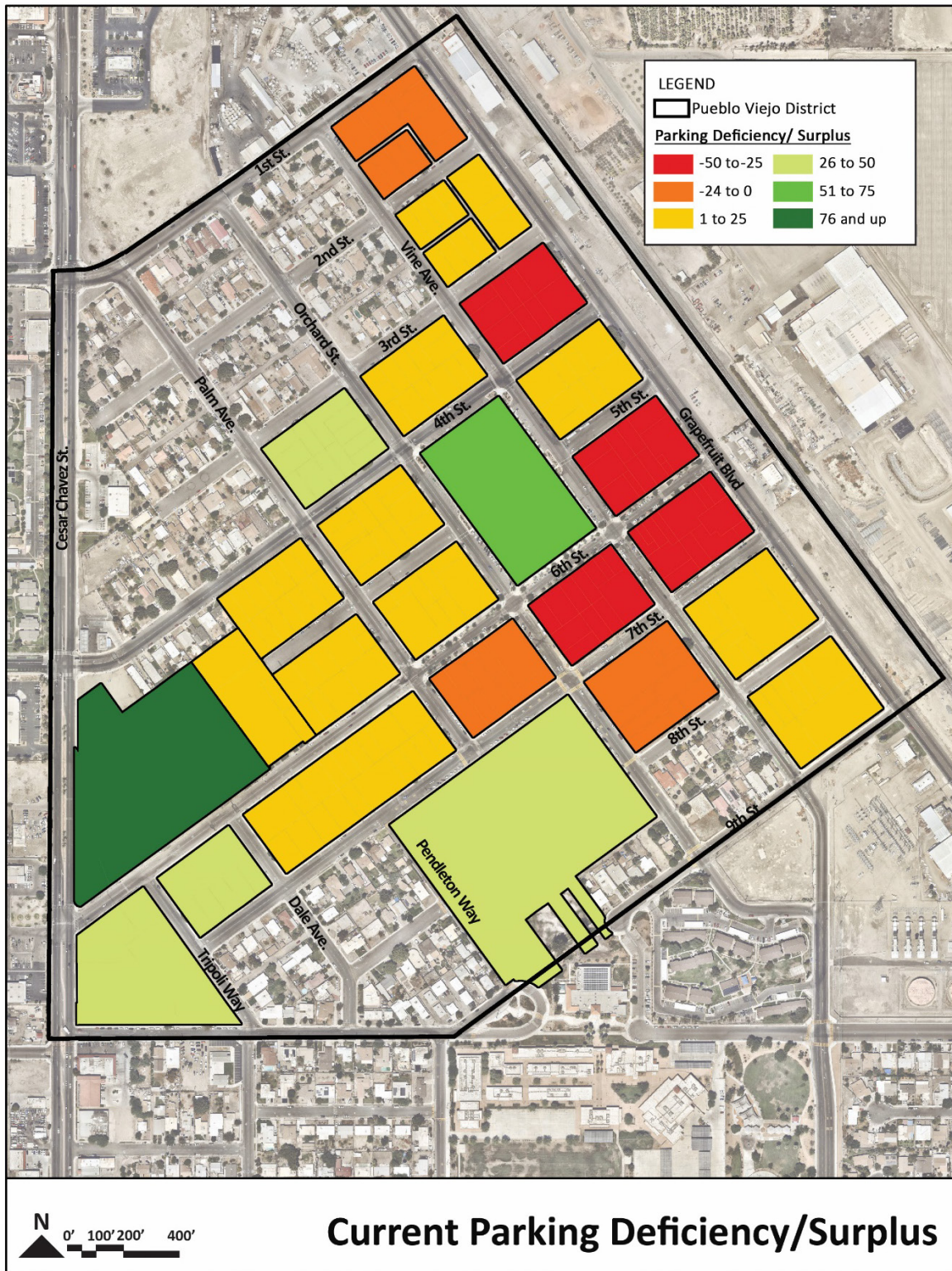
The blocks that are shown in green in **Figure 7**, namely the park block, County building block, school block, and others on the west side of the district, along Sixth Street, have a surplus of more than 25 spaces per block. The park block and County building block have more than 50 surplus parking spaces. The blocks shown in yellow have surplus parking of less than 25 spaces. Most blocks on the east end of the district shaded in orange and red have parking deficiencies. Most of these blocks are concentrated along Sixth Street on the eastern end of the district. The new library building block doesn't provide any off-street parking and therefore shows a severe deficiency due to the calculated demand of the library compared to the surrounding parking available.

Although deficiencies on a block-by-block basis exist, it is important to consider the proximity of surplus parking within walking distance that may satisfy the additional demand generated on the deficient blocks. The parking-deficient blocks along Sixth and Seventh Streets are within the walkshed of the blocks with surplus parking, such as the park block and the school block. Some of the deficiency is mitigated by the parking supply provided by these blocks. Generally, a 5-minute walking distance is considered optimal for a walkshed; this translates to 0.25 miles or 1,320 feet. However, only on-street parking of these blocks can be used for mitigation since the off-street parking areas of most of the blocks in the walkshed of the deficient blocks have owner/tenant restricted parking; see **Figure 6**.

A summary table of the calculations from the model can be found in **Appendix-PS-2: Current Supply and Demand**.



Figure 7: Existing Parking Deficiency/Surplus





Future Parking Scenarios

To understand the future parking needs of Pueblo Viejo district, two future scenarios were studied:

- **Medium-built Scenario:** The medium-built scenario looked at a horizon of 5 to 7 years and assumed to take into account the changes in land use based on their readiness to develop properties such as vacant properties and those ripe for development or transition.
- **Full-built Scenario:** The full-built scenario considers a long-term horizon (20-30 years) and assumes that the visions set out in the General Plan and the Pueblo Viejo Vision Plan have come to fruition. A more intensive mix of shops, office, retail/restaurant, and housing now exists.

The two scenarios were represented in land use change maps using the categories listed below. These were later used to compute parking deficiency and surplus as explained in the **Methodology** section.

- **New Mixed-use Construction:** Assumes that there will be a new mixed-use development with commercial and residential structure. It is also assumed that the new construction will comply with the off-street parking requirements as established for mixed-use development.
- **Retain Structure and Change Use:** Applies to transition areas and assumes that the existing structure will be retained but the land use will change to commercial. On-site parking will be maximized by reconfiguring the available space. However, this parking will be surface parking.
- **Retain Structure and Retain Use:** Assumes there is no change in structure, parking, or the use.

D. Medium-built Scenario

The Medium-built Scenario assumes several changes in land uses as well as streetscapes that will influence available parking. Some of these changes may include:

- Mixed-use development in Block 15.
- Development of vacant properties and properties ripe for development.
- Transition of properties from residential to commercial around Veteran’s Park.

Figure 8 shows the projected new development that may take place on a block-by-block basis. **Table 5** documents the findings of the model for the medium-built scenario. **Figure 9** shows the snapshot of optimized parking. Other than the blocks on the east side of the district, as illustrated in **Figure 9**, all the blocks show a surplus of parking. These are also the blocks that have no to minimal parcels identified for new development in the medium-built scenario. As discussed in the previous section, only on-street parking of the blocks showing surplus parking can be used for mitigation since the off-street parking areas of most of the blocks in the walkshed of the deficient blocks have restricted access (see **Figure 6**). Also, **Table 5** indicates that most of the surplus is from adding the on-street parking to the supply, meaning the blocks that show surplus may use some of the on-street parking and leave the rest for use by blocks with deficient parking.

Based on the area dedicated to parking, the study found that while most blocks will be able to satisfy demand with surface parking, most of these blocks will see minimal change in projected use. However, most of the blocks that will transform with a new construction of mixed-use development will require structured parking to meet required parking demands, based upon the assumed off-street parking



requirements and the intensity of development necessary to generate private sector interest. As seen in **Table 5**, the cost of this structured parking is projected to be approximately \$32 million in today’s dollars and will satisfy only one-third of the required parking demand. In comparison, the cost of providing surface parking is projected to cost approximately \$8 million in today’s dollars and will satisfy up to two-thirds of the projected parking demand.

Table 5: Medium-built Scenario Findings

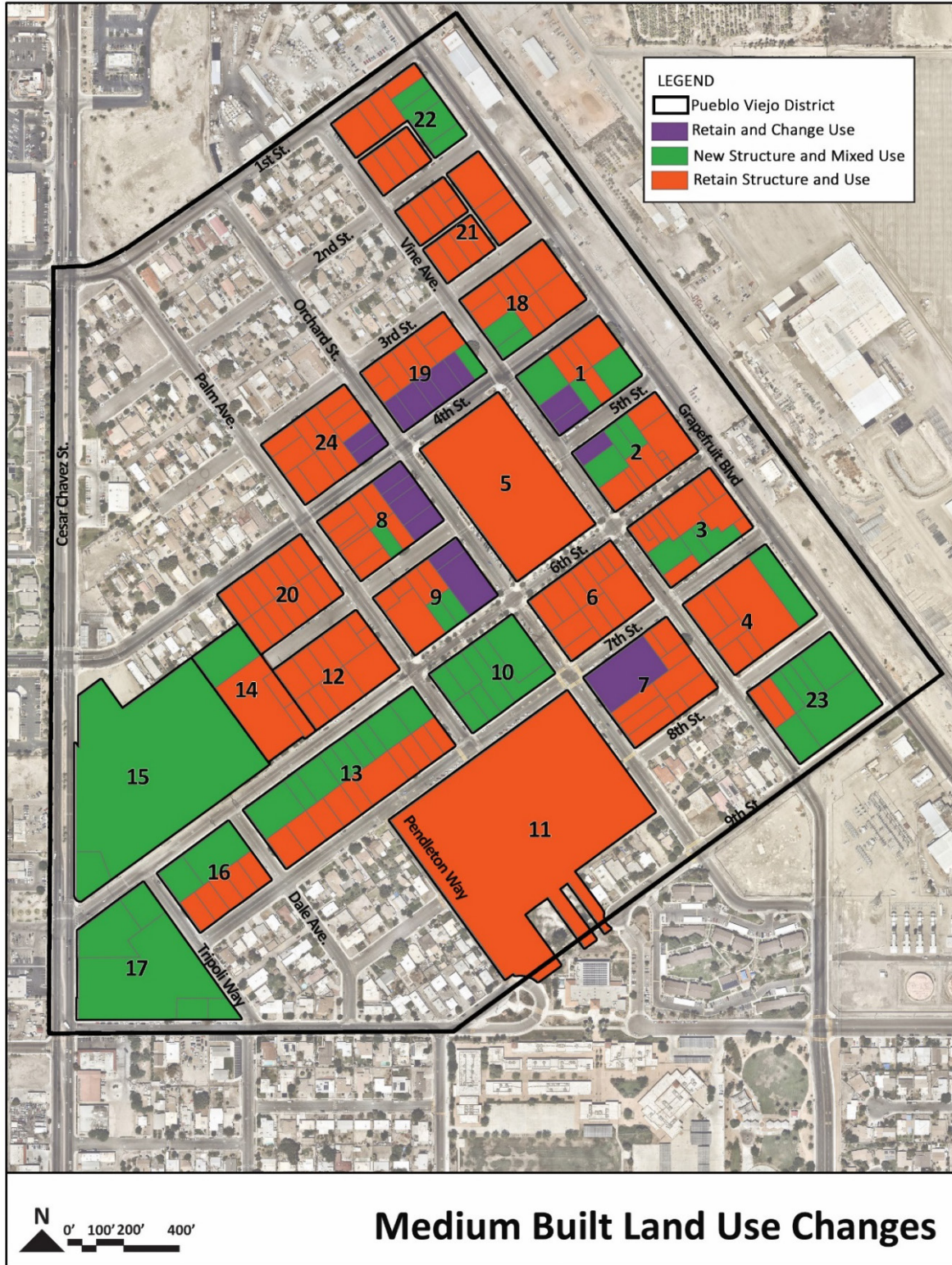
	Parking Spaces	Area (approx.)	Cost (approx.)
Total parking	4,500	1.4 million square feet	\$40 million
Parking deficiency/surplus without factoring on-street parking	-300	NA	NA
Parking deficiency/surplus factoring on-street parking	540	NA	NA
Surface parking	2,800 (approx.)	0.9 million square feet	\$8 million
Structured parking	1,700 (approx.)	0.5 million square feet	\$32 million
<i>Note: A 10 percent optimal parking vacancy factor is considered.</i>			

The comparison above shows the cost burden a developer faces for required parking at the off-street ratio assumed in the model. The current market may not generate the necessary returns to justify the cost of parking, and alternatives to lower some of the costs borne by the developer will be required to realize the development envisioned by the General Plan and Pueblo Viejo Vision Plan. Some of these strategies are discussed in the **Recommendations** section of this report.

A summary table of the calculations from the model can be found in **Appendix-PS-3: Medium-built Scenario**.



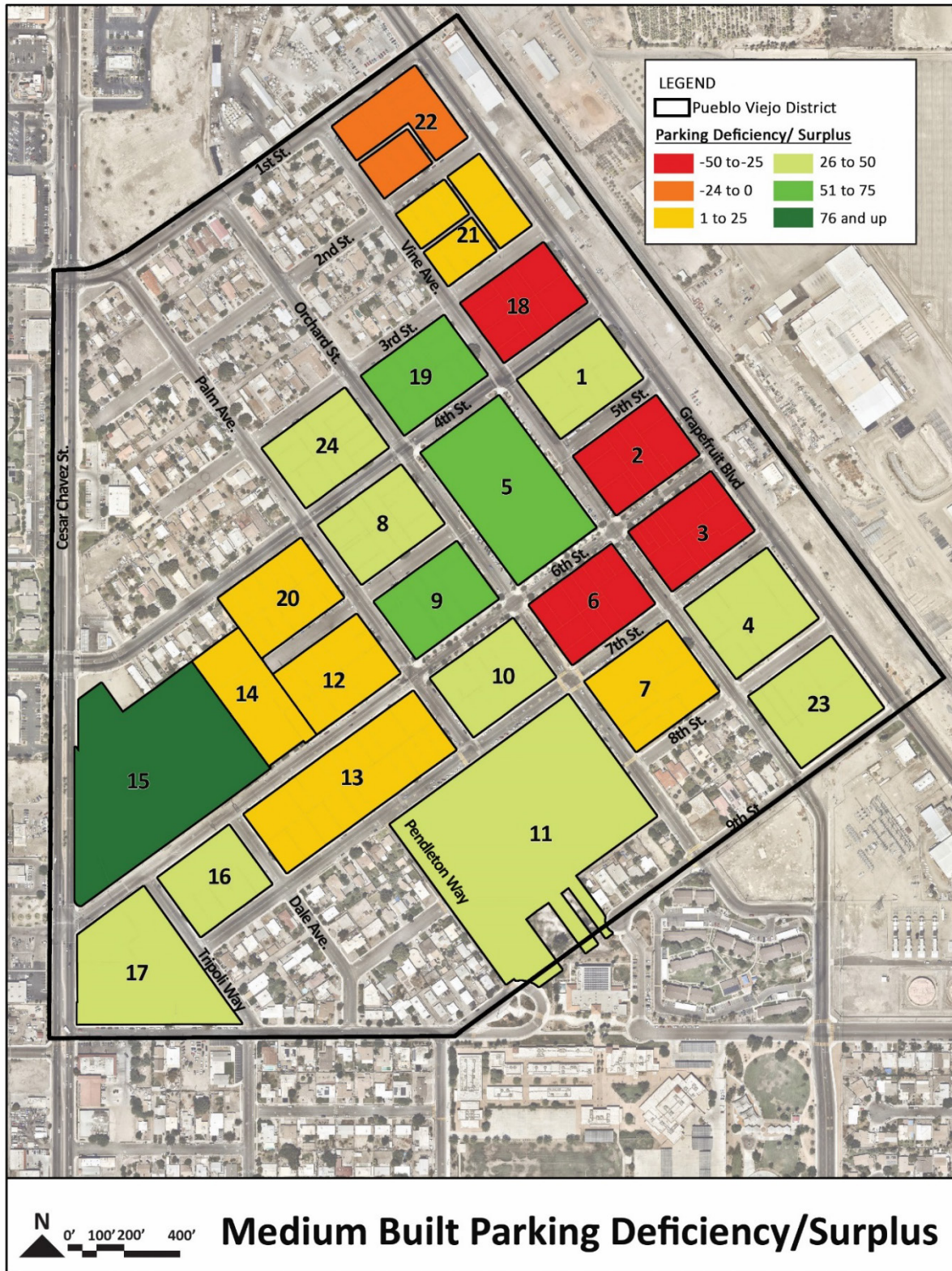
Figure 8: Medium-built Scenario - Land Use Changes Assumption





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Figure 9: Medium-built Scenario with Optimized Parking





E. Full-built Scenario

The full-built scenario assumes changes in land uses, as specified by the General Plan and Pueblo Viejo Vision Plan. The changes involve large amounts of new construction, including the intensification of development on the east side of the Pueblo Viejo district. As stated in the Methodology section, all new development is assumed to be mixed-use development which will provide the parking supply required for the development based upon the assumed future off-street parking standards. Figure 10 illustrates the future projected development that may take place in a full-built scenario.

Table 6 documents the findings of the model for the full-built scenario. Figure 11 shows the snapshot of optimized parking. All blocks show a surplus of parking supply except one block northeast of the Veteran’s Park along Fourth Street. Most of the blocks that show a surplus of parking are those anticipated to witness development intensification. The remainder are residential blocks, the park block, and the school block.

Table 6: Full-built Scenario Findings

	Parking Spaces	Area (approx.)	Cost (approx.)
Total parking	6,800	2.2 million square feet	\$152 million
Parking deficiency/ surplus without factoring on-street parking	-30	NA	NA
Parking deficiency/ surplus factoring on-street parking	800	NA	NA
Surface parking	1,400(approx.)	0.4 million square feet	\$4 million
Structured parking	5,400 (approx.)	1.8 million square feet	\$109 million

Note: A 10 percent optimal parking vacancy factor is considered.

While the blocks may seem to provide a balance between supply and demand, most blocks will need structured parking to accommodate future projected demand. As seen in Table 6, the cost of structured parking for the full-built scenario is estimated to cost as much as \$109 million, in today’s dollars, and will cover approximately 80 percent of the projected future demand. Surface parking, on the other hand, will cover approximately 20 percent of the projected future demand at \$4 million. As stated previously, requiring 100 percent of demand for each block to be borne by off-street (and adjacent on-street) spaces, which will still require structured parking, may be economically infeasible for the private sector, and alternative means may be necessary to mitigate costs or redistribute supply and demand. Some of the recommendations are discussed in the next section.

A summary table of the calculations from the model can be found in Appendix-PS-4: Full-built Scenario.



Figure 10: Full-Built Scenario - Land Use Changes Assumption

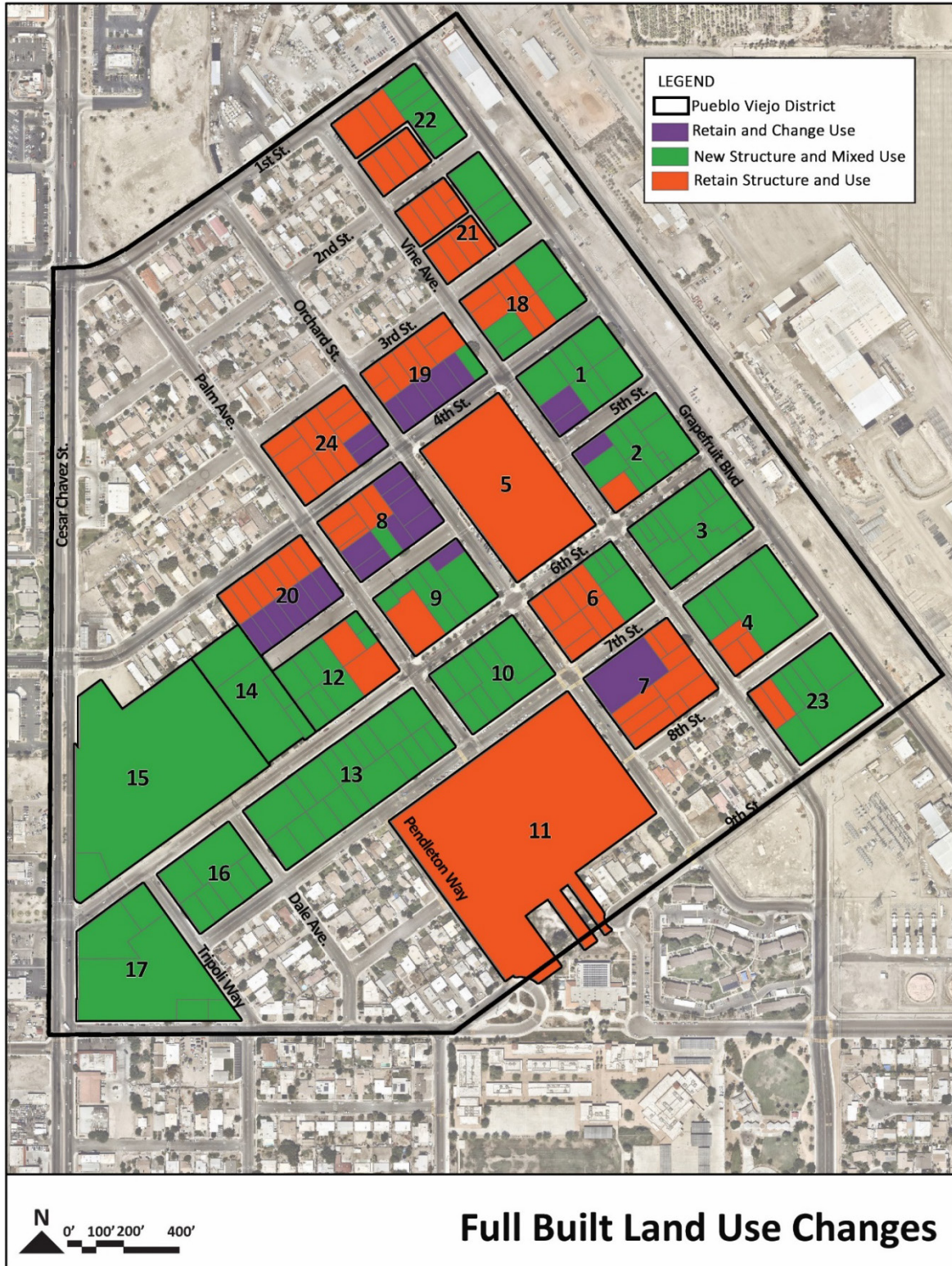
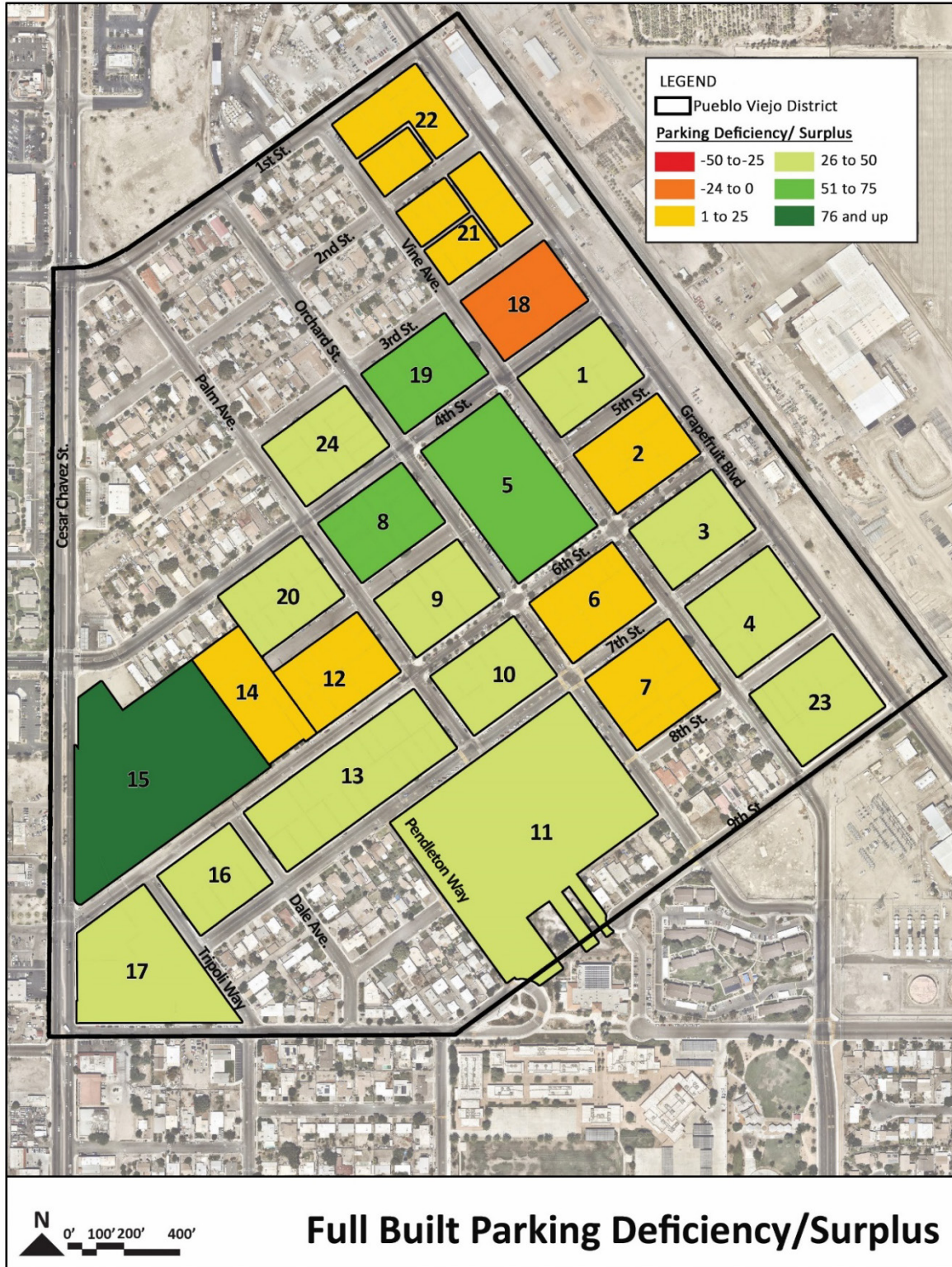




Figure 11: Full-built Scenario with Optimized Parking





Findings

This section documents the main findings of the existing and future built scenarios.

- Of the total parking spaces in the study area, approximately 60 percent are on-street parking spaces and are publicly accessible. When off-street parking is factored in, a total of 80 percent spaces are publicly accessible in the Pueblo Viejo district. (See **Figure 2**, **Figure 4**, and **Figure 6**).
- Of the total off-street parking spaces, approximately 50 percent cannot be used by patrons of other businesses, per restrictions on the use of those spaces for the current tenant/owner's establishment only. While overall there seems to be surplus parking, restricted access creates a false perception of parking deficiency.
- Approximately 80 percent of the total spaces are publicly owned. This includes on- and off-street parking spaces. Around 10 percent of the publicly owned spaces are not publicly accessible. Most often these spaces are not fully utilized and create a false perception of parking deficiency. (See **Figure 5**)
- The current parking supply and demand has an imbalance when examined on a per-block basis. This examination is important as parkers prefer to reduce overall walk times, and supply that is outside of a 5-minute radius is less likely to be considered by parkers. The blocks on the east side of the district have a deficiency of supply while those on the west side have surplus of availability. (See **Figure 7**.)
- Currently, most of the surplus supply of parking falls within the walkshed of the blocks with a deficiency of parking. However, this is mainly due to the availability of additional on-street parking. Off-street parking supply, while available, is primarily privately restricted to patrons of tenants/owners of specific establishments.
- Of the total parking supply within the study area, 5 percent are restricted spaces for Americans with Disabilities Act (ADA) accessible parking, loading/ unloading spaces, timed parking, pick up and drop off, and electric vehicle parking. However, they are a part of overall parking supply (see **Figure 2** and **Figure 4**). These are provided by law and there was no indication during the study that the ratio of these restricted spaces needs to change.
- In both future build scenarios that the study tested, a projected surplus of parking supply is available if on-street parking included. Most of the blocks that show a surplus of parking are anticipated to be targeted for future development intensification.
- Based upon the study model, both future scenarios would require structured parking to attain the development intensities envisioned in the General Plan and Pueblo Viejo Vision Plan. This cost of development may be a limiting factor due to the cost of construction and economic viability of structured parking versus surface parking. The estimated cost of structured parking could be close to \$32 million in today's value and will satisfy only one-third of required parking demand in the medium-built scenario. In the full-built scenario, the estimated cost will be around \$109 million and would satisfy approximately 80 percent of required parking demand.



Recommendations

As the Pueblo Viejo district grows and realizes its vision, various parking strategies will be needed for the efficient and effective management of parking supply and demand to ensure peak operations while balancing the costs of development and changing preferences in future parking demand. These strategies may lead to lowering the parking maximums currently assumed in the parking model, hence lowering the built area dedicated to structured parking for the mixed-use development. Parking recommendations below can be applied to the Pueblo Viejo district to make the highest and best use of land. This in turn will reduce the cost of development, making the area more attractive to developers and creating potentially higher tax revenue generation. In addition, some of the recommendations outlined would result in separate revenue streams for the construction, operations, and maintenance of future parking.

F. Lower Parking Ratio

Recommendation 1: Lowering parking ratio, eliminating minimum parking requirements, and better management of parking spaces can reduce parking costs.

Parking is costly and requires careful management. It was previously assumed that the parking should be abundant and free. In an article titled “Reduced and More Accurate Parking Requirements,” Todd Littman states that the land use patterns based on this current development paradigm show that nationwide there is typically an average of two to six parking spaces per vehicle in a community, making parking more expensive compared to the vehicle it parks. However, this parking is not free, but is paid for directly or indirectly by the end user, either through increased rent, housing costs, and/or taxes. Although this ultimately places the burden back on the user, it is indirect and often unnoticed; therefore, the behavior of how people view parking and demand parking as a “free commodity” leaves the private sector and government to grapple with the issues of supply and the costs associated with constructing and maintaining that infrastructure. Studies have now recognized that too much parking is as harmful as too little due to the impacts on development costs, government costs, and increased infrastructure due to lower-density development. Parking should be managed more effectively. Hence, some communities are eliminating minimum parking requirements and introducing maximum requirements, and many are significantly reducing minimum requirements.

Parking requirement reductions for various land uses should be done with a careful analysis of the area. Too little parking supply can create frustration among parkers and can lead to a reduction in visitors and customers. Various factors affect parking requirements, such as geographic location, residential density, employment density, land use mix, transit accessibility, carsharing, walkability and bikeability, demographics, income, housing tenure, pricing, and sharing/overflow. Various strategies of parking reduction discussed below in this section can lead to a 10 to 30 percent reduction in the required supply. This leads to a range of 2.3 to 3.0 parking spaces per 1,000 square feet of mixed-use development, leading to the required parking supply reductions as depicted in **Table 7**.



Table 7: Recommendation - Lower Parking Ratio

Parking Ratio for Mixed Use Development (per 1,000 square feet of built space)	Medium-built Parking spaces	Full-built Parking spaces
3.33 (Current ratio used in the model)	4,500	6,800
3.0 (10 percent reduction)	4,050	6,120
2.3 (30 percent reduction)	3,150	4,760

It should also be recognized that parking conditions change with changing land uses and new technology. Hence, parking ratios should not be static. A study by University of Michigan Transportation Research Institute, Texas A&M Transportation Institute, and Columbia University, focused on Austin, Texas, suggests that the rideshare services such as Uber and Lyft are affecting car ownership. Many studies have also indicated that the influx of autonomous vehicles in the future will greatly reduce the need for parking and therefore, the provision of iterative parking—meaning that the provision of parking at a lower rate and increasing supply on an as-needed basis using land bank or set-aside open space may be a better strategy. In addition, using in-lieu parking fees as explained in the **Financing and Organizational Mechanisms for Implementation** section can provide for centralized parking as the area reaches a threshold, and increasing supply becomes necessary.

G. Cooperative Parking Arrangement

Recommendation 2: Developing cooperative parking agreements with property owners and tenants can improve parking efficiency and convenience for all Pueblo Viejo district customers and visitors.

Due to owner/tenant restrictions, many off-street parking spaces are restricted to patrons of an individual establishment. As seen in **Figure 6** and documented in the **Findings** section, approximately 50 percent of the off-street parking spaces have restricted access. In downtowns, this pattern of space utilization creates inefficiency in parking distribution with empty usable spaces designated for other establishments near establishments with higher parking demand. According to the Victoria Transportation Policy Institutes’ 2018 Parking Pricing Implementation Guidelines, if businesses share parking spaces, there can be a reduction of 10 percent to 20 percent in the supply without any effect on demand. A 10 percent reduction applied to off-street parking after deducing the parking for employees and residents could result in an overall reduction of 4 percent required spaces in future build scenarios. **Table 8** documents the reductions.

Table 8: Recommendation - Cooperative Parking Arrangement

	Medium-built	Full-built
Original Off-Street Parking Demand	4,492	6,813
Parking Reduction —Applying Cooperative Parking Arrangement to on- and off-street parking	168	252

Note: A 10 percent reduction percentage for shared parking is applied to off-street parking after deducing the parking for employees and residents.



According to The Institute of Transportation and Development Policy (ITDP) report on shared parking, for a city to reap the full benefits of a cooperative parking program, all available parking needs to be publicly accessible with no private or reserved spaces. If all parking is publicly accessible, then it can be traded as a commodity. While parking pricing and commodity trading go beyond the auspices of this report, it shows that parking can be valuable, and when property owners are in a cooperative situation, they are more likely to allocate parking in a more efficient manner.

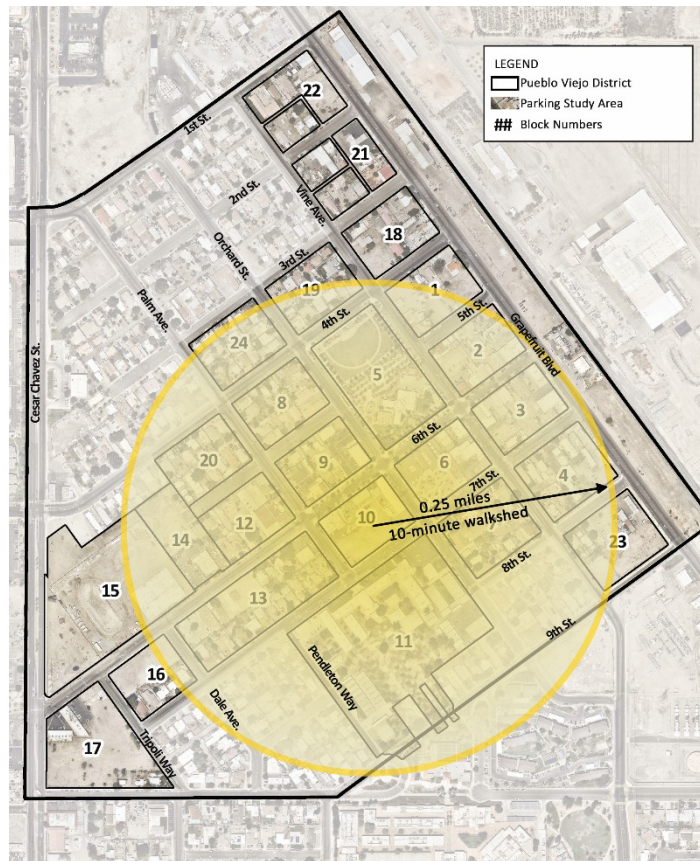
Cooperative parking is most successful when sharing uses have different peak periods and are within walkable distance (5 minute or 1,200 feet). Some examples include shared space between school/public utilities and restaurants, offices and bars, etc. Agreements and memorandum of understanding are needed between these uses. The City can act as an intermediary to determine how and where spaces can be shared. The City can also facilitate agreements between the property owners and serve as a party to develop a parking district to provide a legal mechanism for joint-ownership and/or management of parking.

H. Shared Centralized Structured Parking

Recommendation 3: City-provided shared centralized structured parking can help reduce some of the cost burden on developers and make the Pueblo Viejo district attractive for investment.

Both medium- and full-built scenarios show that structured parking may be necessary in certain blocks to provide the required parking supply. However, it should be kept in mind that the estimate on where the structured parking needs to be is based upon assumed distributed development, at a certain FAR, with the assumption that demand, and preferred modes of travel will not change. The actual development might be different compared to the assumptions in the model. The aim of this analysis is to illustrate the relationship between the type/intensity of development and the overall cost of providing off-street parking, which might be a limiting factor in meeting the future vision.

Figure 12: Walkshed of a Central Parking Facility





The cost of providing structured parking is approximately \$20,000 per space, on average, in comparison to surface parking at

Cost of multistoried parking structure	\$20,000 per space X 1,000 spaces = \$ 20 Million
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an average of approximately \$3,000 per space. Effective distribution and sharing of parking is vital to making desired development economically viable for the developers.

City-provided shared parking can add to the reduction in parking supply by the development. To make an area more attractive to the developers and to lower their cost of construction, the City can provide surface and/or structured parking that can be shared by the private sector. One of the most opportune blocks based on walkshed is Block 10. The block is centrally located to cover a large area using a walking radius of 0.25 miles. **Figure 12** shows the walkshed of the proposed centralized location for multistoried parking structure. These 350-foot by 250-foot block can accommodate approximately 250 cars per level. A four-level structure will be able to accommodate approximately 1,000 cars and reduce the amount of parking required by future development in surrounding blocks.

Different legal mechanisms can allow private partnerships, public ownership, or public/private partnerships to develop shared control of parking construction, operations, and maintenance. Some of the implementation mechanisms are explained later in the **Financing and Organizational Mechanisms for Implementation** section of this report.

While centralized parking might be a great option to lower required parking supply by the development and efficiently distribute parking, it should be noted that the development will happen over time, and full absorption of new space will happen over time. Combined with trends showing a potential reduction in future parking needs, it may be more cost-effective for communities to adopt lower parking standards with mitigation measures in place to handle increased demand, outside of what the standards assume, when and if it occurs. This means developing programs for development of additional shared parking that can be triggered based upon future parking need, and not permitting the supply to be potentially over-built. This would require thoughtful planning for land acquisition for shared parking locations and mechanisms for cost-sharing, whether private, public, or public/private partnerships.

I. Bicycle Parking

Recommendation 4: *Providing safe and convenient biking and walking facilities can make bicycles a viable choice and reduce car trips, leading to a reduction in required spaces.*

The General Plan update and Revitalization Plan envision the Pueblo Viejo district as more bicycle friendly. The Pueblo Viejo district currently has bicycle lanes on Vine Street, while Sixth Street has sharrows and is marked as a shared path. Other streets in the area are proposed as good candidates for bicycle lanes as they are reconstructed/repared. These changes will result in increased cycling as a viable mode of transportation that people may choose if they perceive it as a realistic and safe method for short trips. Many of the City's residential neighborhoods are a short bicycle ride from the Pueblo Viejo district. More choice toward alternatives to the personal automobile will reduce parking demand within the district.



Typically, a vehicle parking space can accommodate up to 10 bicycle parking spaces. Along with providing active transportation facilities such as bicycle lanes and a pleasant walking experience, safe and convenient bicycle parking facilities can be used to reduce vehicle parking demand. The Victoria Transportation Policy Institute suggests that a 5 percent to 10 percent reduction can be achieved in demand with provisions of such facilities. A 5 percent reduction applied after deducing the parking for employees and residents resulting in an overall reduction of 2 to 3 percent required spaces in future build scenarios. The reduction percentage is based upon the community's current walk score and observed use of bicycles to calculate the potential reductions.

Table 9: Recommendation - Bicycle Parking

	Medium-built	Full-built
Original Off-Street Parking Demand	4,492	6,813
Parking Reduction —Applying parking pricing to on-street parking	43	43
Parking Reduction —Applying parking pricing to on- and off-street parking	127	169
<i>Note: A 5 percent reduction percentage for bicycle parking is applied to off-street parking after deducing the parking for employees and residents.</i>		

Detailed calculations for all three strategies can be found in **Appendix-PS-5: Parking Reductions** of this study.

J. Time-restricted Parking

Recommendation 5: Time-restricted parking can effectively increase turnover rate of the prime on-street and off-street parking spots. However, enforcement is vital for the success of this strategy.

There are currently only two parking spaces that are time-restricted in the Pueblo Viejo district. Time-restricted parking generally varies from 30 minutes to two hours and is indicated either by a sign or by green-painted curb with the maximum duration written on the curb. Typically, a two-hour restriction is recommended within downtown areas to encourage multiple destination trips. Time-restricted parking can be an effective strategy to increase turnover rates of prime on-street or off-street parking spaces as parkers will utilize the space more efficiently. This behavior frees up immediate parking spaces for short duration parkers and encourages parkers interested in long-term parking (over two hours, all day, or commuter parking) to use off-street and other facilities. One disadvantage of timed parking is that the time is fixed and therefore the parker will only change their behavior based upon that fixed time. It also is inconvenient for the parker if they are spending more time (and potentially more money) in the district if they are required to change the space in which they are parked.

Another disadvantage is enforcement. Enforcement of more frequent and consistent regulation is vital for the success of this strategy. A parking enforcement officer is required to observe vehicles and whether or not they have exceeded the parking time in a space. Recent court cases have also called into question the legality of marking vehicle tires as a method to help determine time parked. Technologies are



available to allow enforcement officers to record license plates and software will notify the officer to recheck spaces at regular intervals.

K. Paid Parking

Recommendation 6: Paid parking can effectively increase turnover rates and the supply of convenient parking spaces.

Paid parking refers to the user-paid parking where the user is directly charged for using a parking space, typically calculated by the time the parking space is utilized. Paid structured parking spaces, parking meters, pay stations, etc. are used as various mechanisms to implement paid parking. Paid parking, especially when rates are based upon utilization, are the most effective and efficient means of increasing parking turnover as parkers have the greatest incentive to use the space for only the time required. Other benefits of paid parking are the easier enforcement of mechanism and more convenience for the parker allowing them to add parking time if needed through additional fees. Newer technology eliminates coin meters and allows for the use of credit cards, smart apps, and other methods to not only pay fees, but also extend time or locate parking spaces. Similarly, the same technology improves the efficiency of enforcement by identifying potential expired spaces, reducing the overhead of parking enforcement.

Revenue collection is based on the ownership of the parking space, i.e., public or private. It can also be shared by the businesses or by the City and businesses of the area depending on the investment. Various mechanisms are explained in the **Financing and Organizational Mechanisms for Implementation** section of this report

According to the Victoria Transportation Policy Institute, efficient parking pricing can provide numerous benefits:

- Increased turnover of the most convenient spaces. This increases consumer convenience, facilitates deliveries, and reduces cruising for parking (searching for an unoccupied space).
- Reduces the number of spaces needed to meet demand, thus reducing total parking costs and allowing more compact development.
- Encourages longer-term parkers to use less convenient spaces (such as off-street or urban fringe) and encourages travelers (particularly commuters) to use alternative modes when possible.
- Reduces total vehicle traffic and therefore problems such as traffic congestion, accidents, energy consumption, and pollution emissions.

Table 10: Recommendation - Paid Parking

	Medium-built	Full-built
Original Off-Street Parking Demand	4,492	6,813
Parking Reduction —Applying parking pricing to on-street parking	171	171
Parking Reduction —Applying parking pricing to on- and off-street parking	508	675



Note: A 20 percent reduction percentage for bicycle parking is applied to off-street parking after deducing the parking for employees and residents.

- Generates revenue; ensures that users pay their share of municipal road and parking costs.

The study suggests that if the parking pricing is calculated correctly, the parking supply can be reduced between 10 percent to 30 percent. This study utilized a 20 percent reduction to calculate effects on parking reduction. It excluded supply required for potential employees and residents in these calculations, because subjecting residents and employees to hourly parking restrictions is cumbersome and unpopular. Overall, a 10 to 12 percent reduction in the parking spaces could be achieved by applying this strategy on both on- and off-street parking.

Variable parking pricing can also be used as a pricing strategy. Meter rates are set (and periodically adjusted) with the goal of achieving an occupancy level of approximately 85 to 90 percent. Prices can vary in multiple ways: across the course of the day as demand varies (time-of-day pricing); and geographically as demand varies with distance from high-demand areas such as a commercial core (geographic/ differential pricing).

Progressive pricing or length-of-stay pricing strategy can also be used. By charging a higher hourly meter rate for each additional hour, short-term parking is encouraged and the turnover increases, while providing flexibility and convenience to users. While both variable and progressive pricing are more easily achievable with current parking technologies, they are not as popular due to the lack of predictability to the consumer. These programs are typically more effective in more congested and urbanized areas than the City of Coachella.

L. Overflow Parking - Railroad Land

Recommendation 7: Overflow parking can be created by converting the land next to the railroad tracks.

Overflow parking plans are strategies that will be applied when parking facilities fill, for example, during special events, peak shopping periods, or temporary reductions in parking supply. Converting the land next to the railroad tracks in the Pueblo Viejo district may be a part of this strategy. **Figure 13** shows the walkshed of providing parking near railroad tracks. This area can also be used as employee parking area for the businesses in its walkshed. However, there needs to be a certain level of demand before investing in the overflow parking. Other options such as using existing private parking areas for overflow parking should be explored prior to investing in new parking areas. Requiring smaller lots with some set-aside for open space that can be later converted to parking if needed can also be an option.

M. Temporary and Overflow Parking

Recommendation 8: Use of school parking and using private parking areas can provide temporary supply of parking during events.



There are more than 100 parking spaces on the school property. These are in the walkshed of most of the blocks in the Pueblo Viejo district, as seen in **Figure 14**. While it might not be feasible to use these spaces

Figure 13: Walkshed-Parking Near Railroad Tracks

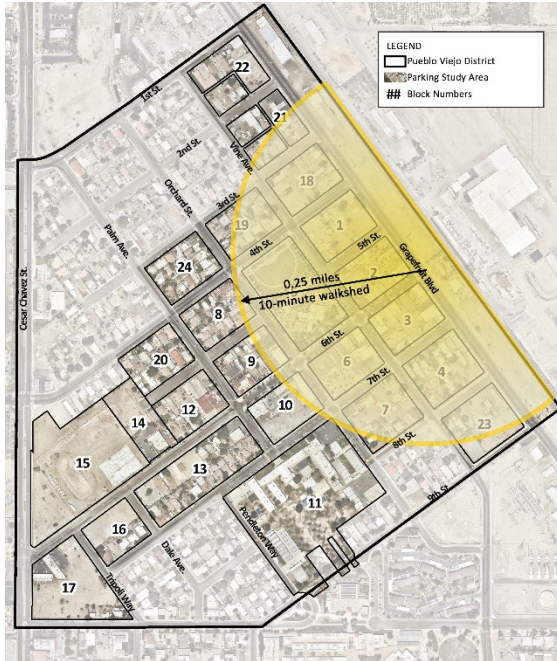
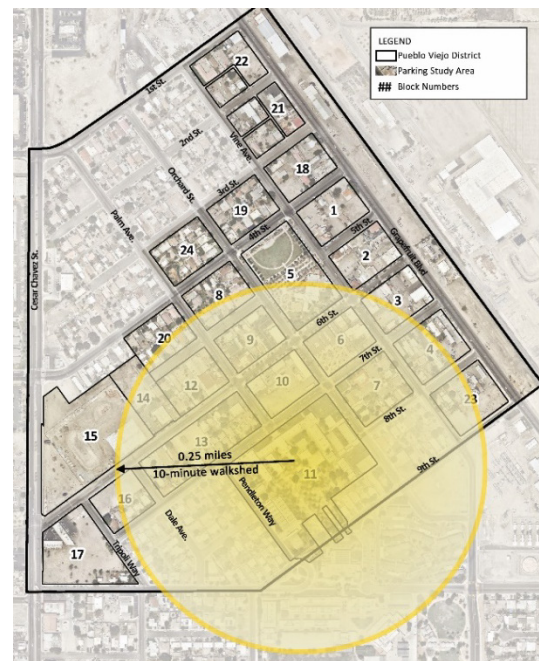


Figure 14: Walkshed-Parking on School Property



on regular basis during the off-school hours, there can be a joint agreement with the school district for event parking at certain times of the year.

N. Effective Use of Leftover Space

Recommendation 9: *Leftover on- and off-street space can be used to provide parking for motorcycles, bikes, and/or compact cars, thus adding to the parking supply.*

Using leftover spaces of a lot, such as corners and undeveloped land, to provide parking for motorcycles, bikes, and/or compact cars will increase parking spaces without large investment and major construction. This strategy is well suited to the transition areas of the Pueblo Viejo district.

O. Converting Parallel to Angled parking

Recommendation 10: *Some of the wider streets that do not need bike facilities can be used to accommodate angled parking instead of parallel parking.*

Changing parallel to angled on-street parking where street width is available can add to the parking supply, thereby reducing the need for off-street spaces. The City has already implemented this strategy along various street sections in the Pueblo Viejo district. Angled parking may interfere with the ability to provide on-street bicycle facilities. There needs to be consideration as to the use and prioritization of the street with respect to provision of angled parking versus bicycle facilities.



P. Signage and User Information

Recommendation 11: *Signage, wayfinding, and readily available information on available parking spaces can be a useful tool in reducing perceived parking shortage.*

Most often there is a perceived parking shortage due to the lack of information on available parking. Readily available information such as parking availability, regulations and price, and other travel options, such as walking, ridesharing and transit, will enable efficient use of parking spaces. Various ways to disseminate this information include wayfinding signage, area maps, brochures, websites, electronic guidance systems, and smart apps.

Q. Employee Parking

Recommendation 12: *Encourage employees to not park in the most convenient customer parking spaces.*

If clients must drive around the block several times to find a space to park, they could potentially leave to seek another similar business with better parking space options. This situation often arises because employees use the on-street and off-street spaces that are most convenient to business patrons. If employees are encouraged to not park in these parking spaces and instead utilize the spaces that would otherwise be unused, it will reduce the perception of parking shortage and need to oversupply parking.

R. Unbundle Parking

Recommendation 13: *Unbundling parking can lower the cost for the developer as well as the user and reduce surplus parking. Parking permit programs can be used to avoid adverse effects of unbundling on nearby neighborhoods.*

Unbundling refers to renting or selling parking separately rather than automatically including it with the price of building space. For example, rather than renting an apartment with two parking spaces for \$1,000 per month, the apartment would rent for \$800 per month, plus \$100 per month for each parking space. This strategy allows the owners to buy or rent only required number of spaces.

One of the concerns that immediate neighborhoods might have is that the residents of these developments might park on their street to avoid the cost of parking. This concern can be resolved by a residential parking permit program where the city gives or sells residents the right to the street parking in their neighborhood and grants them the ability to temporarily give permission to their guests. All other parkers are either strictly limited in how much time they can spend in a given space or are banned from parking in that zone entirely.

Financing and Organizational Mechanisms for Implementation

There are various sources of financing and organizational mechanisms in place to implement recommendations in this study that require cooperative arrangements between more than one party.



These are listed below. Some recommendations require revenue sharing, revenue generation, and mechanisms for sharing the cost of infrastructure development between property owners, businesses, district residents, and/or the City. State law allows for various districts to help finance and/or operate shared parking infrastructure.

Parking Assessment District: Parking Assessment Districts are used to finance the costs of building and operating public parking facilities. To finance the costs of the project, local government levies assessments on property and may issue bonds. Cities and counties may use the Parking District Law of 1943 (Streets & Highways Code §31500 et seq.) to form a district and levy assessments. Parking district laws state that these districts can be used to finance the acquisition of land, the construction, operation and maintenance of parking facilities and structured parking, and the costs of professional staff needed to complete the project. The charges created by these laws are considered benefit assessments and therefore must be calculated based on the benefit each property will receive from the improvements financed. These districts are initiated by a petition of landowners and require a landowner vote for approval. Once formed, the districts are managed by an appointed commission.

Pros: Locally initiated; specific use of revenue proceeds; can issue bonds for large up-front capital expense.

Cons: Local landowner opposition to keep from formation; nexus requirement to set rates.

Business Improvement District: Business Improvement Districts (BIDs) are public/private sector partnerships that perform a variety of services to improve the image of their cities and promote individual business districts. They also carry out economic development services by working to attract, retain, and expand businesses. The Parking and Business Improvement Area Law enables a local government to establish a BID and levy annual assessments on businesses within its boundaries. A city council can only establish a BID after the owners of the businesses or property have indicated their support (or lack of opposition) for the BID via a petition, a ballot or protest proceeding, or both. Improvements which may be financed include parking facilities, parks, fountains, benches, trash receptacles, street lighting, and other amenities. Bonds cannot be issued. Assessments must be directly proportional to the estimated benefit being received by the businesses upon which they are levied.

Pros: Partnership between city and businesses; can define use of the revenue.

Cons: Local business opposition to keep from formation; nexus requirement to set rates.

In-Lieu Parking Fee: A city may offer developers within a district the option to pay a fee in lieu of providing the amount of on-site parking required by zoning code. An in-lieu fee program is typically established for a specific area, as opposed to establishing a citywide program. The developer would only pay the fee if the applicant chooses not to provide all or a portion of the required parking spaces on-site. The amount of the fee is often set at a value that is estimated to represent the actual cost of developing a new parking space in the area. The fee can be a one-time payment or an annual lease payment. Given that the amount of money generated tends to be insufficient to fund a completely new parking facility, in-lieu fees are generally used in combination with other funding mechanisms to fund parking improvements.



Pros: Cash to pay for parking facility where needed; more efficient use of land.

Cons: Administrative effort to account for program revenue; pushback from developers on fee amount.

California Infrastructure and Economic Development Bank (IBank): One program under IBank is the Infrastructure State Revolving Fund (ISRF) Program. The ISRF Program provides financing to public agencies and nonprofit corporations, sponsored by public agencies, for a wide variety of infrastructure and economic development projects (excluding housing). ISRF Program funding is available in amounts ranging from \$50,000 to \$25 million with loan terms for the useful life of the project up to a maximum of 30 years. Revenue-producing enterprise systems such as parking facilities may be eligible if the proposed project and repayment stream are acceptable to IBank. IBank funding can be used in conjunction with other revenue-generating districts as a source for up-front financing with the revenues of the district being used for retirement of the debt.

Pros: Up-front funding for construction.

Cons: Loan program to repay.

Transit Grants: Federal and state funding could be available through transit grants that help fund inter-modal or multimodal transit facilities such as transit centers, rail stations, and park-and-ride facilities. The transit project could include parking facilities that promote multimodal travel. Similar to a joint use agreement, the transit agency and the City could use the parking facility for transit use and to increase the supply of parking for downtown, subject to the conditions of the transit grant.

Pros: Joint use potential; serves dual purpose of providing parking supply and multimodal access.

Cons: Conditions of grant funds may be restrictive; parking is limited to location of the property.

Mello-Roos: The act allows any county, city, special district, school district or joint powers authority to establish a Mello-Roos Community Facilities District (CFD) which allows for financing of public improvements and services. A CFD is created by a sponsoring local government agency. The proposed district will include all properties that will benefit from the improvements to be constructed or the services to be provided. Once approved, a special tax lien is placed against each property in the CFD. Property owners then pay a special tax each year. If the project cost is high, municipal bonds will be sold by the CFD to provide the large amount of money initially needed to build the improvements or fund the services.

Pros: Broad range of facilities; can issue bonds for large up-front capital expense; CFD is entitled to recover expenses needed to form the CFD and administer the annual special taxes and bonded debt.

Cons: Requires landowner approval; additional property tax burden on affected properties.

Infrastructure Financing District: This includes Enhanced Infrastructure Financing Districts (EIFDs). In the aftermath of redevelopment, new forms of tax increment financing have emerged to give local jurisdictions options to finance infrastructure and economic development projects. EIFDs are empowered to provide financing for a broad range of infrastructure work, including traditional public works such as roads, bridges, and parking facilities. EIFDs may also finance a broader range of public uses for economic development purposes. EIFDs are only able to collect property tax increment from cities,



counties, and special districts that voluntarily agree to contribute those funds, and cannot collect tax increment from K-12 school districts, community college districts, and county offices of education.

Pros: Large sums of revenue could be generated; defined process to create district; broad range of facilities.

Cons: Requires agreement from other taxing agencies; district creation requires specific steps and financing plans to be developed; cannot pay for maintenance, routine repairs, or operations.

Joint Use Agreement: Joint use, also called shared use or community use, occurs when government entities, or sometimes private, nonprofit organizations, agree to open or broaden access to their facilities for community use. Joint use can take place on a formal basis (based on a written, legal document) or on an informal basis (based on historical practice). An agreement is entered into by the landowner (such as a school district) and the City to jointly use parking facilities on the landowner's property. Joint use is beneficial when the asset is underutilized by the landowner at certain times, such as on weekends when school offices are close, and could instead be used by the City.

Pros: Smaller footprint for parking supply; partnership between agencies; more efficient use of land.

Cons: Landowner concerns about costs, vandalism, security, maintenance, and liability in the event of injury; parking is limited to location of the landowner property.



Appendix-PS-1: Zoning Comparison

Use	CASE STUDIES																		Remarks		
	Residential Multi family- 2BDR	Residential Single Family	Senior Housing/ bed room	Live Work Units	Bank	Health/fitness facilities	Medical office	Hotel/ Motel/ Bed and Breakfast	Office	Commercial Retail	Restaurant Full service	Restaurant Fast Food	Taverns/ Bar	Theater	Preschool/ Day care	Auto Dealership	LI	School Elementary		Mixed Use	Walkscore*
Unit of Measurement	DU	DU	DU	DU	Per 1,000 SF	Per 1,000 SF	Per 1,000 SF	Per room	Per 1,000 SF	Per 1,000 SF	Per 1,000 SF	Per 1,000 SF	Per 1,000 SF	5 seats	Per 1,000 SF	Per 1,000 SF	Per 1,000 SF		Per 1,000 SF		
ITE Manual Average	1.23	1.23	0.59		4	5.57	4.94	0.89	2.84	3.76	10.6	12.4	10.6	0.75	3.16	2.25	0.75	0.17 per Student			
ITE Manual 85th Percentile	1.94	1.94	0.66		5.67	8.46	4.96	1.08	3.45	5.05	14.2	14.5	16.3	1	3.7	2.74	1.13	0.21 per Student			
Coachella	1.33	1.33			4	4	4	1/ suite +1/three employees	4	4	22/1000 SF customer area; 5/1000 SF noncustomer area	22/1000 SF customer area; 5/1000 SF noncustomer area	22/1000 SF customer area; 5/1000 SF noncustomer area	1.7		4	2.5	1 space per teacher and staff member, plus 1 space per 2 classrooms		35	
Anaheim	2.25	3	1			4	6	1	4	4	15	10	17	3	1 space per employee, plus 1 space per 10 children or adult clients	4	1.55	1 space per classroom, plus 1 space per non-office employee, plus 4 spaces per 1,000 square feet of GFA for office use	CUP	54	Due to variations in parking demand and needs of each Planned Mixed Use Development, vehicle parking requirements and the design of the parking areas, including ingress and egress, shall be determined as part of the final plan review process, based on information contained in a parking demand study prepared by a California-licensed, independent traffic engineer, as approved by the Planning Services Manager of the Planning and Building Department and/or his or her designee. The parking demand study shall be prepared at the developer's expense at the time of application for the use.
Glendale	2	3	1	3	4/1000 SF customer area; 2.7/1000 SF noncustomer area	10	5	1	2.7	4	10	12.5	10	1	3		2			69	A parking reduction may be granted where the hearing officer determines that a reduction is justified based on characteristics of the uses, an hourly parking demand study published by the Urban Land Institute, and/or other appropriate source as determined by the director of community development.
Glendale DSP Zone	2	3	1	3	4/1000 SF customer area; 2.7/1000 SF noncustomer area	10	4	1	2	3	5	5	5	1	3		2		2.30 to 3.33**	92	Parking reductions based on the total point value to the applicable TDM measures.
Palm Springs-CBD***	1.5	2	1 space per 2 beds / 1space per 1000 sf whichever is greater + 1 space per 3 employees		5	2.5 +1 per employee	6.67 for spaces up to 10,000SF, 5 thereafter	1 for hotel up to 50 rooms 0.75 thereafter	5 for spaces up to 10,000SF, 4 thereafter	4	28/1000 SF customer area	28/1000 SF customer area	16.7	1.7	1 space per employee, plus 1 space per 5 children	1.25	2	1 space per employee	3.08 to 3.33	City 35 Downtown 76	
Pasadena	2	3	1		4	8	5	1	4	5	8.33	8.33		1	1 for each staff member and 1 for each 10 children	2	2	2 space per classroom; plus 14 spaces per 1,000 sq. ft. of auditorium assembly area.		66	
*	Source: https://www.walkscore.com/ , Accessed May 22, 2019.																				
**	Computed using numbers from medium and full built scenario from Pueblo Viejo Parking study and applying Glendale parking ratios and 10% reduction for parking pricing as a TDM Measure.																				
***	The C-B-D zone is intended for the central business district, primarily retail business in character, with related hotels, multiple-family dwellings, and service, office, cultural and institutional uses. Additional parking need not be provided for restaurants, provided that, no more than twenty-five (25) percent of the total floor area of the whole complex is devoted to restaurant use.																				



Appendix-PS-2: Current Supply and Demand

BASELINE SCENARIO																											
SUMMARY ITEM	UNIT	Block 1	Block 2	Block 3	Block 4	Block 5	Block 6	Block 7	Block 8	Block 9	Block 10	Block 11	Block 12	Block 13	Block 14	Block 15	Block 16	Block 17	Block 18	Block 19	Block 20	Block 21	Block 22	Block 23	Block 24	TOTALS	
Land Use Type		■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	
Existing Off-Street Parking Spaces	Spaces	20	41	47	48	17	13	35	14	54	16	106	27	49	46	176	10	34	30	18	20	21	6	4	18	870	
Existing On-street Parking Spaces	Spaces	27	30	40	32	107	45	34	35	36	35	69	31	45	3	39	39	51	24	30	24	24	36	31	34	901	
Forecasted Parking Required- Commercial/ Institutional	Spaces	33	91	130	29	64	90	74	17	55	67	126	35	36	27	120	0	45	79	0	0	19	38	10	0	1,186	
Forecasted Parking Required- Industrial	Spaces	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Forecasted Parking Required- Residential	Spaces	3	3	2	24	0	0	11	20	9	0	0	15	45	3	0	8	8	8	21	32	15	18	5	23	269	
Forecasted Parking Required- Total	Spaces	36	97	132	53	64	90	84	37	64	67	126	50	81	30	120	8	53	87	21	32	34	56	15	23	1,458	
Estimated Parking Deficiency/ Surplus (Using On-Street Parking)	Spaces	11	(26)	(45)	27	60	(32)	(15)	12	26	(16)	49	8	13	19	95	42	32	(33)	27	13	11	(14)	20	30	313	
Parking Deficiency/Surplus Considering Optimal Parking for Commercial (Using On-Street Parking)	Spaces	10	(29)	(49)	25	54	(36)	(17)	11	23	(17)	44	8	12	17	86	37	29	(36)	24	11	10	(16)	18	27	246	
Block Area	SF	87,248	87,411	87,177	103,572	185,822	87,893	104,387	87,681	84,370	86,472	451,333	88,215	178,606	88,837	316,147	75,348	167,050	87,941	87,906	87,706	95,874	98,727	105,637	88,560	3,019,920	
Existing Total Built Area	SF	13,103	32,731	40,064	23,979	32,184	20,343	26,326	25,703	27,041	16,667	135,000	26,867	38,376	9,790	30,000	8,360	14,255	39,588	18,319	24,193	14,048	21,670	6,256	16,906	661,769	
Forecasted Development Built Area- Commercial/ Institutional	SF	10,466	28,042	40,064	7,178	32,184	20,343	18,410	4,280	21,111	16,667	135,000	17,256	8,934	6,829	30,000	0	11,307	19,794	0	0	4,692	9,616	2,618	0	444,791	
Forecasted Development Built Area- Industrial	SF	0	2,425	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2,425	
Forecasted Development Built Area- Residential	SF	2,637	2,264	0	16,801	0	0	7,916	21,423	5,930	0	0	9,611	29,442	2,961	0	8,360	2,948	19,794	18,319	24,193	9,356	12,054	3,638	16,906	214,553	
Forecasted Development Built Area-Gross	SF	13,103	32,731	40,064	23,979	32,184	20,343	26,326	25,703	27,041	16,667	135,000	26,867	38,376	9,790	30,000	8,360	14,255	39,588	18,319	24,193	14,048	21,670	6,256	16,906	661,769	
Forecasted Residential Dwelling Units	DU	0	0	0	1	0	0	3	2	1	0	0	4	3	0	0	3	1	4	14	3	7	1	1	3	51	
Forecasted Built Area Demolished	SF	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Forecasted Parking Area	SF	6,500	13,325	15,275	15,600	5,525	4,225	11,375	4,550	17,550	5,200	34,450	8,775	15,925	14,950	57,200	3,250	11,050	9,750	5,850	6,500	6,825	1,950	1,300	5,850	282,750	
Forecasted Average Height of the Building (including area for parking and considering 10% open space)	Stories	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
Separate parking structure stories using maximum 5 storied separate building structure and considering 10% open space (1 indicates surface parking)	Stories	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
Percentage Block Area Covered by Parking	%	7%	15%	18%	15%	3%	5%	11%	5%	21%	6%	8%	10%	9%	17%	18%	4%	7%	11%	7%	7%	7%	2%	1%	7%		

Land Use Legend	
	Commercial
	Residential
	Park
	Institutional
	Vacant
	Industrial



Appendix-PS-3: Medium-built Scenario

MEDIUM BUILT SCENARIO																											
SUMMARY ITEM	UNIT	Block 1	Block 2	Block 3	Block 4	Block 5	Block 6	Block 7	Block 8	Block 9	Block 10	Block 11	Block 12	Block 13	Block 14	Block 15	Block 16	Block 17	Block 18	Block 19	Block 20	Block 21	Block 22	Block 23	Block 24	TOTALS	
Land Use Type		■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	
Existing Off-Street Parking Spaces	Spaces	20	41	47	48	17	13	35	14	54	16	106	27	49	46	176	10	34	30	18	20	21	6	4	18	870	
Existing On-street Parking Spaces	Spaces	27	30	40	32	107	45	34	35	36	35	69	31	45	3	39	39	51	24	30	24	24	36	31	34	901	
Forecasted Parking Required- Commercial/ Institutional	Spaces	151	173	198	88	64	90	74	61	91	259	126	35	268	104	164	112	501	125	49	0	19	140	276	13	3,180	
Forecasted Parking Required- Industrial	Spaces	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
Forecasted Parking Required- Residential	Spaces	62	38	45	59	0	0	11	22	23	136	0	15	172	43	23	67	263	32	20	32	15	64	148	20	1,309	
Forecasted Parking Required- Total	Spaces	213	213	244	147	64	90	84	83	114	395	126	50	440	148	188	179	764	157	69	32	34	203	424	32	4,492	
Estimated Parking Deficiency/ Surplus (Using On-Street Parking)	Spaces	40	(23)	(37)	29	60	(32)	17	46	80	35	49	8	26	19	95	42	51	(33)	77	13	11	(7)	30	40	634	
Parking Deficiency/Surplus Considering Optimal Parking for Commercial (Using On-Street Parking)	Spaces	36	(25)	(41)	26	54	(36)	15	41	72	32	44	8	23	17	86	37	46	(36)	69	11	10	(8)	27	36	544	
Block Area	SF	87,248	87,411	87,177	103,572	185,822	87,893	104,387	87,681	84,370	86,472	451,333	88,215	178,606	88,837	316,147	75,348	167,050	87,941	87,906	87,706	95,874	98,727	105,637	88,560	3,019,920	
Existing Total Built Area	SF	13,103	32,731	40,064	23,979	32,184	20,343	26,326	25,703	27,041	16,667	135,000	26,867	38,376	9,790	30,000	8,360	14,255	39,588	18,319	24,193	14,048	21,670	6,256	16,906	661,769	
Forecasted Development Built Area- Commercial/ Institutional	SF	43,313	52,010	61,224	25,284	32,184	20,343	18,410	16,318	31,865	77,825	135,000	17,256	80,357	29,924	43,297	33,773	150,345	33,587	13,198	0	4,692	40,034	82,805	3,179	1,046,223	
Forecasted Development Built Area- Industrial	SF	0	2,425	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2,425	
Forecasted Development Built Area- Residential	SF	81,408	50,644	58,506	60,702	0	0	7,916	28,438	27,529	181,591	0	9,611	208,625	56,849	31,025	87,163	350,805	51,979	22,758	24,193	9,356	79,320	196,131	13,727	1,638,275	
Forecasted Development Built Area-Gross	SF	124,721	105,079	119,730	85,986	32,184	20,343	26,326	44,756	59,394	259,416	135,000	26,867	288,982	86,773	74,322	120,935	501,150	85,566	35,956	24,193	14,048	119,354	278,936	16,906	2,686,923	
Forecasted Residential Dwelling Units	DU	39	25	29	24	0	0	3	9	13	91	0	4	96	27	16	42	175	20	13	3	7	36	97	3	774	
Forecasted Built Area Demolished	SF	912	0	3,914	4,662	0	0	0	0	2,930	16,667	0	0	17,252	0	0	0	14,255	0	552	0	0	3,710	3,338	0	68,192	
Forecasted Parking Area	SF	73,288	52,151	54,072	46,671	5,525	4,225	21,804	30,675	51,505	128,489	34,450	8,775	136,570	53,080	79,153	59,008	248,220	32,523	37,751	6,500	6,825	52,170	137,362	12,373	1,373,165	
Forecasted Average Height of the Building (including area for parking and considering 10% open space)	Stories	3	2	2	1	1	1	1	1	1	5	1	1	3	2	1	3	5	1	1	1	1	2	4	1		
Separate parking structure stories using maximum 5 storied separate building structure and considering 10% openspace (1 indicates surface parking)	Stories	1	1	1	1	1	1	1	1	1	5	1	1	1	1	1	1	5	1	1	1	1	1	3	1		
Percentage Block Area Covered by Parking	%	61%	60%	62%	45%	3%	5%	21%	35%	61%	30%	8%	10%	58%	60%	25%	58%	30%	37%	43%	7%	7%	53%	37%	14%		

Land Use Legend	
	Commercial
	Residential
	Park
	Institutional
	Vacant
	Industrial



Appendix-PS-4: Full-built Scenario

FULL BUILT SCENARIO																											
SUMMARY ITEM	UNIT	Block 1	Block 2	Block 3	Block 4	Block 5	Block 6	Block 7	Block 8	Block 9	Block 10	Block 11	Block 12	Block 13	Block 14	Block 15	Block 16	Block 17	Block 18	Block 19	Block 20	Block 21	Block 22	Block 23	Block 24	TOTALS	
Land Use Type		■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	■ ■ ■ ■	
Existing Off-Street Parking Spaces	Spaces	20	41	47	48	17	0	35	14	54	16	106	27	49	46	176	10	34	30	18	20	21	6	4	18	857	
Existing On-street Parking Spaces	Spaces	27	30	40	32	107	45	34	35	36	35	69	31	45	3	39	39	51	24	30	24	24	36	31	34	901	
Forecasted Parking Required- Commercial/ Institutional	Spaces	217	239	261	255	64	128	74	87	200	259	126	204	535	266	164	226	501	182	49	54	118	151	276	32	4,670	
Forecasted Parking Required- Industrial	Spaces	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Forecasted Parking Required- Residential	Spaces	103	110	137	139	0	49	11	18	86	136	0	89	281	140	23	119	263	82	20	12	74	75	148	29	2,143	
Forecasted Parking Required- Total	Spaces	320	349	399	394	64	177	84	104	286	395	126	294	817	406	188	344	764	264	69	66	192	226	424	61	6,813	
Estimated Parking Deficiency/ Surplus (Using On-Street Parking)	Spaces	56	21	40	34	60	10	17	60	51	35	49	13	45	3	95	39	51	(15)	77	52	20	3	30	40	885	
Parking Deficiency/Surplus Considering Optimal Parking for Commercial (Using On-Street Parking)	Spaces	50	19	36	30	54	9	15	54	46	32	44	12	41	3	86	35	46	(16)	69	47	18	3	27	36	794	
Block Area	SF	87,248	87,411	87,177	103,572	185,822	87,893	104,387	87,681	84,370	86,472	451,333	88,215	178,606	88,837	316,147	75,348	167,050	87,941	87,906	87,706	95,874	98,727	105,637	88,560	3,019,920	
Existing Total Built Area	SF	13,103	32,731	40,064	23,979	32,184	20,343	26,326	25,703	27,041	16,667	135,000	26,867	38,376	9,790	30,000	8,360	14,255	39,588	18,319	24,193	14,048	21,670	6,256	16,906	661,769	
Forecasted Development Built Area- Commercial/ Institutional	SF	64,138	75,544	78,459	76,699	32,184	27,943	18,410	22,686	65,484	77,825	135,000	68,255	160,745	79,953	43,297	67,813	150,345	52,590	13,198	13,535	35,463	43,942	82,805	8,886	1,495,199	
Forecasted Development Built Area- Industrial	SF	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Forecasted Development Built Area- Residential	SF	136,731	146,570	183,072	182,986	0	65,201	7,916	22,070	114,801	181,591	0	118,997	375,073	186,558	31,025	158,231	350,805	108,977	22,758	10,658	90,166	94,215	196,131	27,043	2,811,573	
Forecasted Development Built Area-Gross	SF	200,869	222,113	261,531	259,685	32,184	93,144	26,326	44,756	180,285	259,416	135,000	187,251	535,818	266,511	74,322	226,044	501,150	161,567	35,956	24,193	125,629	138,157	278,936	35,929	4,306,772	
Forecasted Residential Dwelling Units	DU	68	73	92	90	0	33	3	9	57	91	0	59	188	93	16	79	175	53	13	3	46	44	97	10	1,393	
Forecasted Built Area Demolished	SF	7,564	20,003	40,064	19,957	0	20,343	0	0	10,757	16,667	0	9,611	38,376	9,790	0	8,360	14,255	18,988	552	0	6,628	6,186	3,338	0	251,439	
Forecasted Parking Area	SF	113,368	110,600	129,536	128,580	5,525	46,134	21,804	41,971	97,762	128,489	34,450	89,724	265,391	132,003	79,153	111,960	248,220	73,071	37,751	30,537	61,149	62,710	137,362	21,795	2,209,043	
Forecasted Average Height of the Building (including area for parking and considering 10% open space)	Stories	4	4	5	4	1	2	1	1	4	5	1	3	5	5	1	5	5	3	1	1	2	2	4	1		
Separate parking structure stories using maximum 5 storied separate building structure and considering 10% open space (1 indicates surface parking)	Stories	3	3	5	3	1	1	1	1	2	5	1	2	5	5	1	5	5	2	1	1	1	1	3	1		
Percentage Block Area Covered by Parking	%	44%	39%	30%	40%	3%	52%	21%	48%	47%	30%	8%	48%	30%	30%	25%	30%	30%	53%	43%	35%	64%	62%	37%	25%		

Land Use Legend	
	Commercial
	Residential
	Park
	Institutional
	Vacant
	Industrial



Appendix-PS-5: Parking Reductions

REDUCTION STRATEGY-1: PARKING PRICING		
Considering On- and Off-Street Parking		
	Medium Built	Full Built
Total On Street Parking Available ¹	856	856
Total Off Street Parking Needed	4492	6813
Pricing Strategy Reduction ²	171	171
Total Parking Needed after Reduction	4321	6642
Notes:		
1) Total on-street parking - Designated spaces for ADA, timed, loading and electric vehicles		
2) 20% reduction applied to available on-street parking after deducing restricted spaces. Source: http://www.vtpi.org/parkpricing.pdf		
Considering On- and Off-Street Parking		
	Medium Built	Full Built
Total On-Street Parking Available ¹	856	856
Total Off-Street Parking Needed	4492	6813
Total Off Street Parking Needed (Residential)	1309	2143
Total Off-Street Parking Needed by Employees (Commercial/ Institutional/ Industrial) ²	1500	2150
Total Off-Street Parking for Applying Reduction Techniques ³	1683	2520
Total On- and Off-Street Parking for Applying Reduction Techniques	2539	3376
Pricing Strategy Reduction ⁴	508	675
Total Parking Needed after Reduction	3984	6138
Notes:		
1) Total on-street parking - Designated spaces for ADA, timed, loading and electric vehicles		
2) Total forecasted commercial space for medium and full built is divided in 3 equal sections for retail, restaurants and office space. The square footage per employee is applied to each use to compute total employee parking in each scenario. Source: https://www.eia.gov/consumption/commercial/data/2012/bc/cfm/b2.php		
3) Total off-street parking- parking for residences and employees.		
4)20% reduction applied to available on-street parking after deducing restricted spaces, employee and residential parking. Source: http://www.vtpi.org/parkpricing.pdf		



REDUCTION STRATEGY-2: SHARED PARKING		
Considering On- and Off-Street Parking		
	Medium Built	Full Built
Total On-Street Parking Available ¹	856	856
Total Off-Street Parking Needed	4492	6813
Total Off Street Parking Needed (Residential)	1309	2143
Total Off-Street Parking Needed by Employees (Commercial/ Institutional/ Industrial) ²	1500	2150
Total Off-Street Parking for Applying Reduction Techniques ³	1683	2520
Total On- and Off-Street Parking for Applying Reduction Techniques	2539	3376
Shared Parking Reduction ⁴	168	252
Total Parking Needed after Reduction	4324	6561

Notes:

- 1) Total on-street parking - Designated spaces for ADA, timed, loading and electric vehicles
- 2) Total forecasted commercial space for medium and full built is divided in 3 equal sections for retail, restaurants and office space. The square footage per employee is applied to each use to compute total employee parking in each scenario. Source: <https://www.eia.gov/consumption/commercial/data/2012/bc/cfm/b2.php>
- 3) Total off-street parking- parking for residences and employees.
- 4) 10% reduction applied to available on-street parking after deducing restricted spaces, employee and residential parking. Source: <http://www.vtpi.org/parkpricing.pdf>



REDUCTION STRATEGY-3: BICYCLE PARKING		
Considering On-Street Parking		
	Medium Built	Full Built
Total On Street Parking Available ¹	856	856
Total Off Street Parking Needed	4492	6813
Bicycle Parking Reduction ²	43	43
Total Off Street Parking Needed after Reduction	4449	6770
Notes:		
1) Total on-street parking - Designated spaces for ADA, timed, loading and electric vehicles		
2) 20% reduction applied to available on-street parking after deducing restricted spaces. Source: http://www.vtppi.org/parkpricing.pdf		
Considering On- and Off-Street Parking		
	Medium Built	Full Built
Total On-Street Parking Available ¹	856	856
Total Off-Street Parking Needed	4492	6813
Total Off Street Parking Needed (Residential)	1309	2143
Total Off-Street Parking Needed by Employees (Commercial/ Institutional/ Industrial) ²	1500	2150
Total Off-Street Parking for Applying Reduction Techniques ³	1683	2520
Total On- and Off-Street Parking for Applying Reduction Techniques	2539	3376
Bicycle Parking Reduction ⁴	127	169
Total Off Street Parking Needed after Reduction	4365	6644
Notes:		
1) Total on-street parking - Designated spaces for ADA, timed, loading and electric vehicles		
2) Total forecasted commercial space for medium and full built is divided in 3 equal sections for retail, restaurants and office space. The square footage per employee is applied to each use to compute total employee parking in each scenario. Source: https://www.eia.gov/consumption/commercial/data/2012/bc/cfm/b2.php		
3) Total off-street parking- parking for residences and employees.		
4) 5% reduction applied to available on-street parking after deducing restricted spaces, employee and residential parking. Source: http://www.vtppi.org/parkpricing.pdf		



STUDY REPORT

ENGAGEMENT SUMMARY



A 3-day charrette was hosted in the City of Coachella to gather feedback from City staff, City Council members, Planning Commissioners, and public stakeholders. The primary goal of the charrette was to gain confirmation of the vision presented in the 2010 Pueblo Viejo Revitalization Plan and discuss the best methods for codifying the items within the 2010 Plan. The charrette included a site tour, stakeholder interviews, a technical charrette with City staff to focus on policy-based decisions, a study session with the Planning Commission, a meeting with the city’s Mayor, and a meeting with City Councilwoman Betty Sanchez.

Site Tour

The Michael Baker team was given a tour of the Pueblo Viejo District by the City’s Director of Development Services, Luis Lopez.

The tour followed 6th Street east to Grapefruit Boulevard, headed south on Grapefruit Boulevard to 7th Street, east on 7th Street to Orchard Street and then continued west on 6th Street. The tour concluded at Veteran’s Park, located directly north of City Hall.

The tour focused on opportunities and constraints, details about various properties, recent improvement projects, and future development plans. Notable opportunities and planned improvements that were discussed included:

- plans for new cannabis dispensaries
- the new City Library, which was under construction at the time of the tour
- the planned renovation of Palm View Elementary
- the planned development of the Riverside County Department of Public Social Services Building and a mixed-use transit-oriented development
- the opportunity that vacant buildings and lots throughout the district provide

The Director of Development Services mentioned that bungalow conversion may be an option for businesses in the future. This type of development will be allowed through the creation of a mixed-use zoning district, that will be created through the Implementation Strategy Plan.

Key Findings

Overall, the Michael Baker team recognized that there is immense opportunity for redevelopment and the creation of a thriving mixed-use district within the Pueblo Viejo District. It appeared that surface parking is not currently at a shortage, however recognizing that this is a concern for the City in the future, it was considered in the exercise discussed below. The findings of the tour were presented graphically using layered aerial maps. In the core area of the Pueblo Viejo district (the area centered around 6th Street between Harrison and Grapefruit) each property’s frontage was defined based on their contribution to the streetscape of the fronting street.

- Properties lined in red do not currently contribute to the streetscape. Many of these properties are currently vacant or serve only as entrances to parking lots.
- Properties lined in yellow do not currently contribute to the streetscape, but could contribute if the building’s façade was modified to include entryways, windows, or murals. Most of the properties classified in this way have a large blank wall facing the sidewalk.



- Properties lined in green currently contribute to the streetscape by providing entryways to businesses, or visual interest with windows or murals. These properties do not need to be immediately improved and in the future, could be encouraged to tweak their facades to comply with the design guidelines created as a product of the Implementation Strategy Plan. An example of the influence of design guidelines is Les Tres Conchitas Bakery that was asked to modify a stone finish on their façade to follow the 6th Street Revitalization project.



Those vacant properties that were identified in red can serve as opportunity areas where new mixed-use development could be built. These new buildings would be ideally built to create building frontage directly along the street with parking and other amenities located in the interior of the block (identified with gray in the image below). The entryways to these interior amenity areas could be provided in between or under buildings and could greatly increase the amount of parking in the Pueblo Viejo district as it becomes necessary in the future.



Site analysis maps were prepared to further graphically represent the existing conditions of the Pueblo Viejo District. One map focused on the existing land use conditions of the area and the other focused on the existing transportation conditions. Both maps are located at the end of this document.

Stakeholder Interviews

Four stakeholder interviews were held with a variety of community stakeholders. Two members of the Michael Baker team guided the stakeholders through a focused conversation about the Pueblo Viejo District. The conversations focused on the opportunities, issues, and constraints that the participants saw in the Pueblo Viejo district, but also included discussion of city-wide issues like lack of employment and affordable housing.

At least one representative from each of the ten (10) following groups participated in the interviews:

- Coachella Valley Chamber of Commerce
- Raices
- Building Healthy Communities Coachella Valley
- Coachella Valley Housing Coalition



- Wilson Johnson Commercial Real Estate
- Growing Coachella Valley
- Chelsea Investment Corporation
- Santa Rosa Del Valle Medical Group
- California Rural Legal Assistance, Inc.
- Coachella Valley Unified School District Superintendent

The themes that were echoed amongst all groups were:

- The need to encourage young people to stay in Coachella after high school and college.
 - A possible solution discussed was to provide attractive and well-paying jobs and internships and affordable and diverse housing.
- Events like the “Run with Los Muertos”, and “Tacos, Tequilas, and Chavelas” have been extremely successful at bringing people into Coachella and helping to change the stereotypes associated with the City. However, there continues to be a need to create branding to counteract the current stereotypes and stigmas of Coachella.
- The city’s population predominantly identifies as Hispanic or Latino and of those that identify in this way the majority identify as Mexican. Due to this Mexican heritage is an extremely importance aspect of the City’s identity.
- There is a strong need for non-profit meeting and office space to support these organizations.
- It is important to preserve the culture, walkability, and unique nature of the district while enhancing the businesses that are currently prospering.
- True mixed-use would make Coachella stand out amongst the other cities in Coachella Valley and may attract new types of people to the City.
- The Pueblo Viejo district needs “stickiness”, the ability to attract individuals into the area for dinner, but to also keep those people engaged through the night, whether it be with coffee shops, live entertainment, museums, art galleries, or bars.

Overall, the stakeholders agreed that with an increase in programming, service oriented land uses, and more diverse residential units the Pueblo Viejo district could become a prosperous business district that would attract visitors and residents, alike.

Key Findings

The Michael Baker Team found that Latino culture and heritage is the main aspect shaping the Pueblo Viejo district and preserving the unique features that this cultural background brings to the city is of the utmost importance. Through the stakeholders’ discussion of the extremely popular special events that occurred in the last year it became evident that these events were successful not only because they encouraged visitors to come experience in the City in a positive light, but also because they accurately and respectfully portrayed the culture of Coachella. As the team begins to create Design Guidelines and a Zoning District for the Pueblo Viejo District it will be important to ensure that the Mexican heritage of the area is preserved and respected and that unique uses that may stem from this heritage are permitted. Ultimately, the discussions with the stakeholders made it clear that the “stickiness” of the area, housing, and preserving the city’s unique heritage and culture are necessary final products of this planning effort.



Technical Charrette

City of Coachella staff from various departments, including Development Services, Engineering, Utilities, Public Works, and Environmental Compliance, attended a technical charrette hosted by the Michael Baker Team. The charrette included a discussion of general project background, conversation to define mixed use and gain feedback from City staff regarding the appropriateness of mixed use in the Pueblo Viejo district and a discussion of opportunities discussed and observed through the site tour conducted the previous day.

Shane Burkhardt, from Michael Baker, provided a brief description of the contents of the 2010 Pueblo Viejo Revitalization Plan and explained that this effort’s primary goal is to create tools that will assist the City in ensuring that the vision defined in the Revitalization Plan will begin to take shape as new development comes to the District. He also stated that the current project will confirm that the vision outlined in the 2010 Revitalization Plan continues to be appropriate.

Madison Roberts, from Michael Baker, presented slides that illustrated several types and applications of mixed-use. The types of mixed-use discussed included:

- Horizontal Mixed Use – *an area in which commercial or retail uses are located directly next to residential uses*
- Vertical Mixed Use – *a building or development in which commercial or retail uses are interspersed with residential uses. The most traditional application of this type is to have commercial or retail uses on the ground floor and residential uses on the floors above.*

Example of Mixed-Use



It was emphasized that when planning for vertical mixed use it is important to allow for flexibility of ground floor land uses and it is recommended to build this flexibility into Design Guidelines by requiring a higher floor plate on ground floor units and designing the units to be able to be converted from commercial to residential and vice versa depending on market fluctuations. This flexibility will help to ensure that new development is not left vacant and is a constant contributor to the vibrancy of the District.

The City staff in attendance expressed support for this type of flexibility in mixed use and for a “form-light” approach to the Zoning Code update. They mentioned that there may be a need to permit residences with community kitchens and outdoor bars and entertainment, as well, as a packing house/Mercado type development.

Peter Quintanilla, from Michael Baker, presented a variety of quick design sketches depicting programming or redevelopment opportunities throughout the district. The designs included:

Sketch design for gateway at Harrison Street and 6th Street





- Reuse of a back alley as parking with an entrance driveway off of 6th Street
- A District Gateway at 6th Street and Harrison Street (pictured) *Sketch design for Cafe Outdoor Seating Area*
- A District Gateway at 6th Street and Grapefruit
- An example of reutilizing the area around Veteran’s Park for Farmers Markets
- A reimagination of the Historic Fire House next to City Hall as a café with an outdoor patio and eating area (pictured)
- A redesign of a blank façade and fenced storage area to include murals and creative design features (pictured)
- A redesign of a warehouse into a Mercado



Sketch design of fence mural

The City staff liked the ideas presented and agreed that there is a need to reprogram certain areas in the District and improve the facades and curb appeal of many businesses. They also provided many additional ideas including:

- A Farmer’s Market that provides cultural items or a market that brings together the independent vendors that sell things like elote and tosti locos
- A brewery or distillery that incorporates the Mexican heritage of the city.



Key Findings

Overall the City staff in attendance was in support of the creation of a Mixed-Use District that permits and encourages traditional vertical mixed use with a flexible or form light approach to the ground floor use. The main concern with this type of district was the permitted height of the buildings. The ideas for additional programming and uses in the District were numerous and indicated that it would be extremely useful to provide a comprehensive list of all ideas discussed throughout the technical charrette, stakeholder interviews, and other aspects of the planning process that clearly outlines the estimated cost of the items and provides additional guidance on implementation and design.

Planning Commission Study Session

Following a packed agenda, the Michael Baker Team had the opportunity to address the Planning Commission. In attendance was Commissioners Mario Zamora, Mike Etheridge, and Alternate Commissioner Denise Delgado. Although time did not permit the presentation of an in-depth Visual Preference Survey, the Commissioners were extremely interested and excited about this project and asked for the Michael Baker Team to attend a meeting again and present the Visual Preference Survey at another time, ideally as part of a joint study session with the City Council.



Meeting with Mayor

The Michael Baker Team met with the Mayor of Coachella. The Mayor shared his vision for the Pueblo Viejo District which included an increase in the number of bars, restaurants, and dispensaries and a desire to make the District come alive in the evenings. He mentioned that there is a desire and need to create bed and breakfasts and boutique hotels in the area to attract tourists to stay and experience the unique downtown area that the City should offer. He was in support of mixed-use and density.

Meeting with City Councilwoman

The project team met with City Councilwoman Betty Sanchez who echoed many of the same sentiments as the mayor. She stated that the Pueblo Viejo District is very walkable and that this aspect should be emphasized and capitalized upon by providing shade and shelter wherever possible. The main issue that she cited was code enforcement – many properties have excessive rubbish and inoperable vehicles, homes are rundown and Grapefruit Boulevard needs to be improved. She mentioned that tax revenue from marijuana sales could be allocated towards Pueblo Viejo District beautification. She also mentioned that it is extremely important to let visitors know that they have arrived in Coachella by installing gateways (at the locations previously discussed and at secondary gateway opportunities at 1st Street or 9th Street) and by creating unified branding (i.e. - #therealcoachella). Although she was in support of mixed-use is she concerned about preserving view sheds as the area is built up.

Next Steps

Based upon the key findings of the Charrette, Michael Baker International proposes:

1. Scheduling a joint study-session with the Planning Commission and City Council to discuss the Pueblo Viejo and potential issues regarding architectural character, density, use and massing;
2. Developing draft design guidelines based upon consensus already reached through the original 2010 plan and feedback obtained during the Charrette;
3. Based upon feedback from the study session, develop draft zoning ordinance amendments to better facilitate preferred development in the Pueblo Viejo;
4. Conduct traffic studies to determine feasibility of increased on-street parking and improvements to streetscaping throughout the district; and gateway options for entry into the district that may include pedestrian improvements and intersection improvements;
5. Compile a final implementation plan that includes identification of specific projects, general cost opinions for each project, project prioritization and potential funding sources, and phasing strategy.

Visual Preference Survey

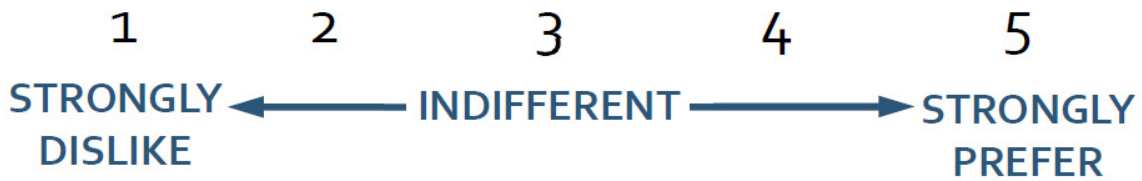
The Pueblo Viejo Implementation Strategy Visual Preference Survey (VPS) was presented on January 31st, 2018 at a special study session of the City of Coachella Planning Commission. There were five members of the public, 5 Planning Commissioners, and various City staff members present. The purpose of the VPS was to gather preferences on design features that will be incorporated into the updated Design Guidelines for the district.



The design features were presented in 19 categories: 1) architectural styles; 2) pavement types; 3) onstreet parking; 4) light poles; 5) luminaires; 6) traffic calming; 7) bicycle facilities; 8) sidewalk pavement, 9) bus shelters; 10) sidewalk lighting; 11) furnishing materials; 12) bike racks; 13) landscaping; 14) outdoor dining; 15) gateways; 16) water features; 17) public art; 18) business signage; and 19) shade structures.

For each design feature the survey participants were asked to specify their preference on a scale from one to five – one indicating “Strongly Dislike” and five indicating “Strongly Prefer”.

VPS Survey- Preference Scale

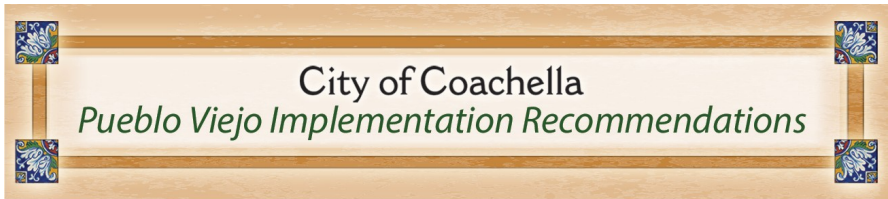


As the participants considered their preference, they took into account the appropriateness of the feature in the Pueblo Viejo, as well as, their general like or dislike of the feature. As the survey was presented discussion followed many of the different feature types. The resulting discussion is summarized in the **Appendix - Visual Preference Survey Summary** at the end of the report.

City of Coachella
Pueblo Viejo Implementation Recommendations

VISUAL PREFERENCE SURVEY (VPS) SUMMARY

*FROM VPS CONDUCTED ON JANUARY 31, 2018 AT PLANNING
COMMISSION SPECIAL STUDY MEETING*



VISUAL PREFERENCE SURVEY SUMMARY

January 31st, 2018

The Pueblo Viejo Implementation Strategy Visual Preference Survey (VPS) was presented on January 31st, 2018 at a special study session of the City of Coachella Planning Commission. There were five members of the public, 5 Planning Commissioners, and various City staff members present. The purpose of the VPS was to gather preferences on design features that will be incorporated into the updated Design Guidelines for the district.

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As the participants considered their preference, they considered the appropriateness of the feature in the Pueblo Viejo, as well as, their general like or dislike of the feature. As the survey was presented discussion followed many of the different feature types. The resulting discussion is summarized below.

Results

The polling results are in **Appendix A**. A summary of the results is below and focuses on Architectural Styles, Sign Types, Outdoor Dining Furniture, Furnishing Styles, Gateways, Shade Structures, and Public Art as they were the categories that sparked the most discussion among the participants. All results will be considered throughout the process of drafting the updated Design Guidelines for the Pueblo Viejo.



Architectural Styles

Traditional Downtown (Italianate and Victorian)

Most did not prefer this style and stated that the architecture of the district should draw more inspiration from the traditional architecture of Mexico and incorporate bright colors. Lynwood Plaza de Mexico was mentioned as an example.



Plaza Mexico in Lynwood, California as an example of the desired traditional downtown look for the Pueblo Viejo District.

Spanish Colonial

All preferred the Spanish Colonial style which is a style very similar to that of the new City library. Due to the familiarity with this style these responses were not surprising.



Mission Revival

Results for this style were mixed and while most preferred the style there was consensus that it was not quite right for the district.



Contemporary

It was unanimous that this style doesn't embody the desired look and feel of the district.





Art Deco

Approximately one-third of respondents preferred this style while two-thirds disliked it. A member of the public thought that the metal accents made the style look too futuristic for the rustic district. Most agreed but thought that the style may be suitable for auto-oriented uses, along Harrison Street and Grapefruit Boulevard.



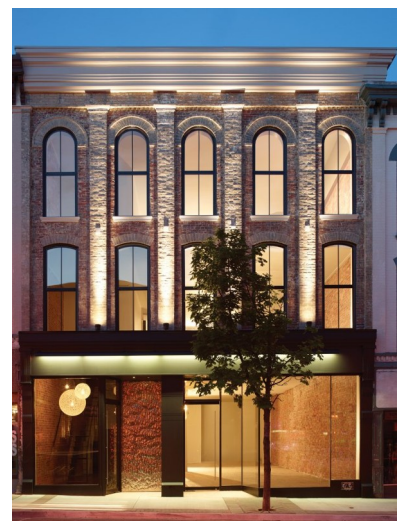
Modernism

Most disliked this style and it was discussed that it may be appropriate for a piece of sculpture but is inappropriate for a building in this district.



Blended Re-use

The participants liked that the style lends itself well to mixed use while retaining the history of the area and adapting an existing building to new needs. There was a general preference for the lighting elements provided as part of this example and the use of glass. They stated that the glass façade provides the feeling of a downtown and creates a feeling of safety due to the visibility the glass provides.





Mono-Style

This style was thought to be sterile and a bit boring. There was consensus that the Pueblo Viejo should look and feel different than other neighborhoods and be an authentic portrayal of the City's history and heritage.



Mixed Styles

The group of participants was not opposed to a blending of styles, but agreed that the examples of mixed styles shown was too drastic for the district and departed for the district's narrative far too much.



Sign Types

Hanging Signs

The participants shared that in the City these types of signs tend to not be well lit and would need to be smaller than they currently are. Most participants did prefer the signs but would like the design guidelines to include required additional lighting and smaller maximum size.



Hanging Sign Under Canopy

Although the polling results for this type of sign were mixed, the comments received stated that these signs promote walkability and would be a terrific addition to the district - where walkability is a primary focus.





Wall Signs

There was a general concern that these types of signs may look too modern and that due to the narrow streets in the district people in cars wouldn't be able to see them. Signs with a pedestrian-focus tended to be more popular with the group.



Window Signs

Currently in the City window signs are permitted and shop owners tend to cover their entire window with signs. It was recommended that window signs are permitted with requirements that they can only cover a certain portion of the window space. The participants agreed that window signs are important and beneficial for businesses, especially in walkable area like the Pueblo Viejo District, but that they will need to be permitted in a stricter way.



Canopy Signs

The participants stated that the problems with these types of signs is that they are so big that they appear like a banner. Another issue identified was that these signs become faded quickly and become a maintenance issue. One positive aspect was that when the sign or identifier is small and only placed on the valence or trim of the canopy it can look tasteful, but this would be difficult to control.





Figurative Signs

There was a concern that these types of signs can be expensive and restrictive if they are required. MBI Consultants explained that this type of sign would be permitted but would not be required. Most participants thought that figurative signs were fun and would add a bit of whimsy to the district.



A-Frame Signs

Due to the low cost of these signs, it was mentioned that sign propagation would need to be controlled to ensure that the sidewalk is clear for pedestrian traffic. It was also stated that requirements about where to put them would be useful. For example, requiring that the sign is at a maximum 2 feet away from the front door of the business. All agreed that these signs are great for businesses and are preferable to business details (i.e. – daily specials) painted onto the business’s window.



Painted Signs

The participants could see these signs appealing to a younger audience (i.e. – 20 to 30-year old people).





Cabinet Signs

Some participants stated that cabinet signs could enhance the district but liked cabinet signs with halo lighting instead of traditional cabinet signs with internal lighting. Other participants were concerned with maintenance needs and the undesirable examples that already exist in Coachella. Overall any hanging signs that could be located closer to pedestrian line of sight were preferred due to these signs' potential to enhance the walkability of the area.



Neon Signs

The participants did not prefer placement of neon signs directly on the building's façade and preferred using neon in a window. There were comments that neon signs would not be appropriate for all types of businesses but may be appropriate for businesses that are more light-hearted in nature, like an ice cream shop.



Outdoor Dining Furniture

Metal

There was concern that this material would become too hot in the desert sun and would not be used. It was also expressed that this material is not very comfortable.



Wicker

The participants voiced concerns about potential issues with the maintenance needed for this material and its potential lack of durability. There was also discussion regarding the look of the material and that it may not fit well into the existing aesthetics of the district.





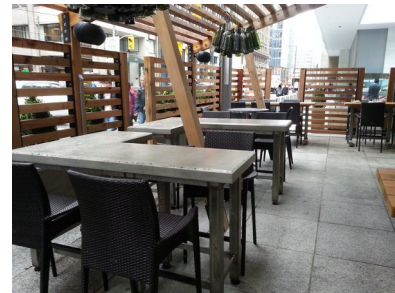
Wood

The participants stated that the combination of wood with wrought iron would look good with the existing aesthetics of the community.



Concrete

Concerns were expressed that this material may look too modern and get too hot in the sun.



Furnishing Styles

Rustic

Some survey participants stated that the rustic style could be reminiscent of Mexico, but most thought that it looks too rustic for the Pueblo Viejo.





Custom

It was stated that custom furniture could allow businesses to be creative with the furniture placed in front of their store front, but if used to represent the Pueblo Viejo, most felt that this furniture style looked too corporate.



Gateways

Archway

Participants thought that an archway gateway feature would work well as a welcome sign to the district if it was placed at 6th Street and Grapefruit Boulevard.



Vertical Feature

The participants liked the look of two columns – one on either side of the road – and stated that it could imply a feeling of passing through and almost function as two separate monuments. There was also stated preference for a clock as a vertical feature.





Welcome Sign

The participants stated that welcome signs are almost omnipresent and used far too often. Their concern was that this type of sign wouldn't make the district feel special when compared with similar downtown districts.



Shade Structures

Awnings and Canopies

Most participants were concerned about the durability of this type of shade structure and felt that smaller awnings and canopies do not provide shade to pedestrians, but instead shade the interior of the storefront.



Trellis

A similar concern about maintenance and durability was voiced for this type of shade structure.





Public Art

Wall Murals

The participants agreed that murals shouldn't be used to camouflage poor design and that a new building should be designed to be aesthetically appealing. Murals should be used to complement the design of buildings in the district.



Sculpture

All participants were excited about the idea of a "Coachella" sculptural feature that could welcome visitors to the City and the Pueblo Viejo, as well as, a tourist attraction and photo opportunity.



Kinetic/Interactive Sculpture

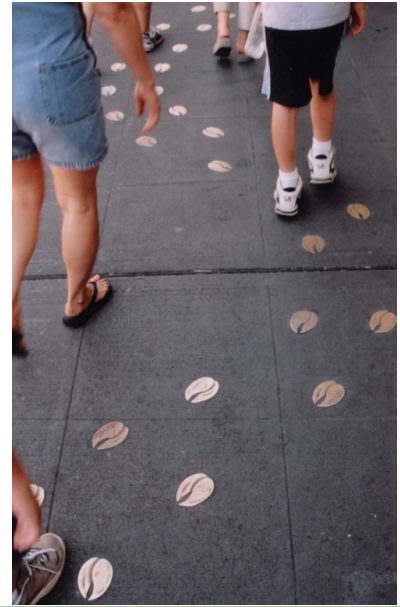
One participant stated that they thought these types of sculptural elements would be appropriate if they were temporary, like in a park. Many other participants stated that there isn't a place for children to play in the Pueblo Viejo. It was stated that with such a young population there should be an area designated that is specifically for children and includes sculptural pieces that also act as play objects.





Pavement Details

The participants thought that a design element like this in the pavement could be an effective way to incorporate the City's history into the pedestrian experience.



Key Takeaways

For those design features where the participants voted very differently the comments above will be carefully considered. They will provide guidance on how to regulate the design feature if it is included in the Design Guidelines . Ultimately, each of the design features polled in the Visual Preference Survey will be careful reviewed for inclusion in the Design Guidelines .

APPENDIX A

Visual Preference Survey Poll Results from January 31st, 2018 Special Study Session of the City of Coachella Planning Commission

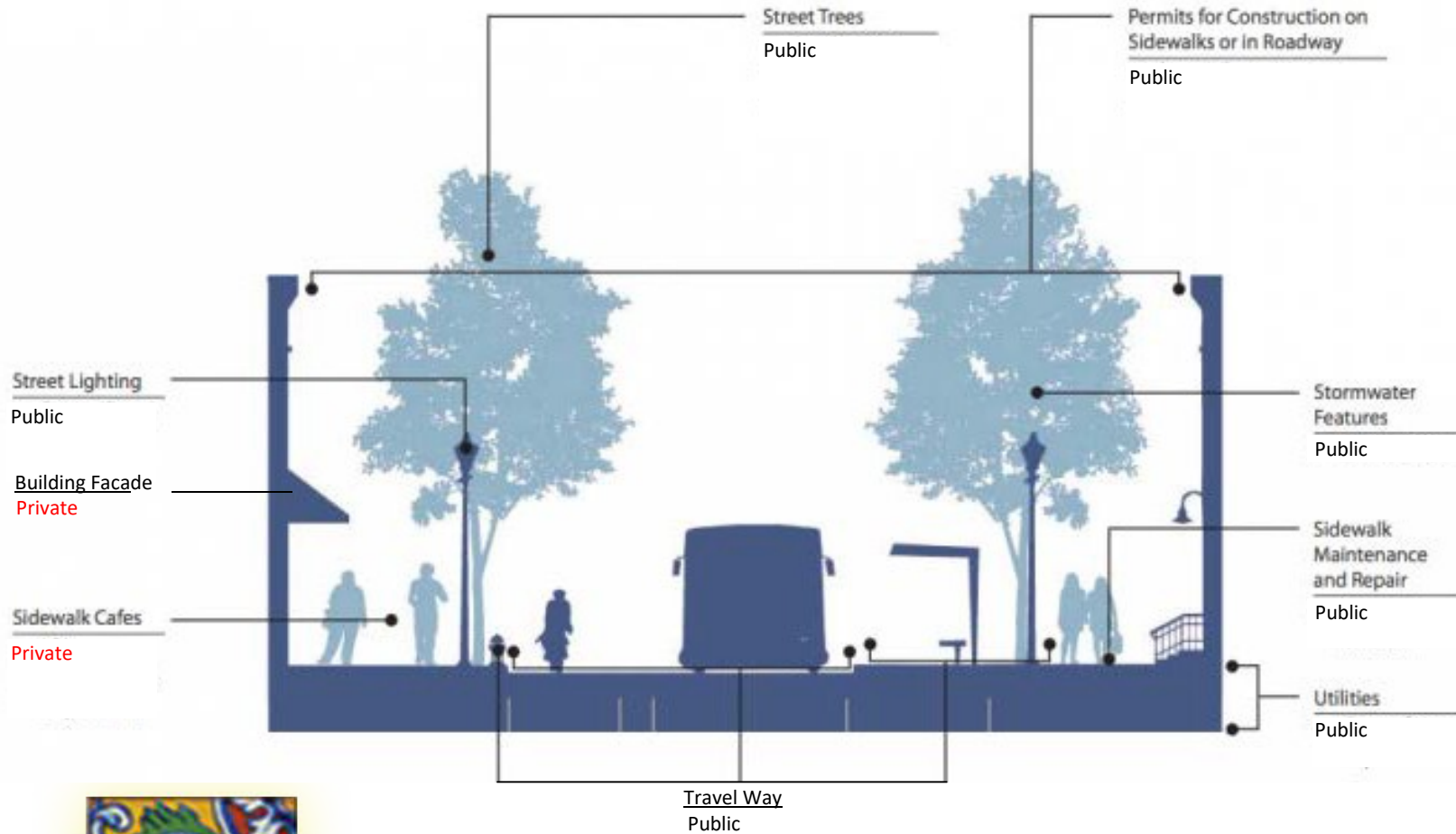


City of Coachella
Pueblo Viejo Implementation Strategy Plan

Visual Preference Survey

Wednesday, January 31st, 2018

Private Realm vs. Public Realm



Private Realm

- Building Façade
- Sidewalk Cafes
- Private Plazas

Public Realm

- Streetscape and Utilities
- Parks and Public Gathering Spaces
- Street Trees



Score each image with your level of preference from *Strongly Dislike* to *Strongly Prefer*

1	2	3	4	5
Strongly Dislike	Dislike	Indifferent	Prefer	Strongly Prefer

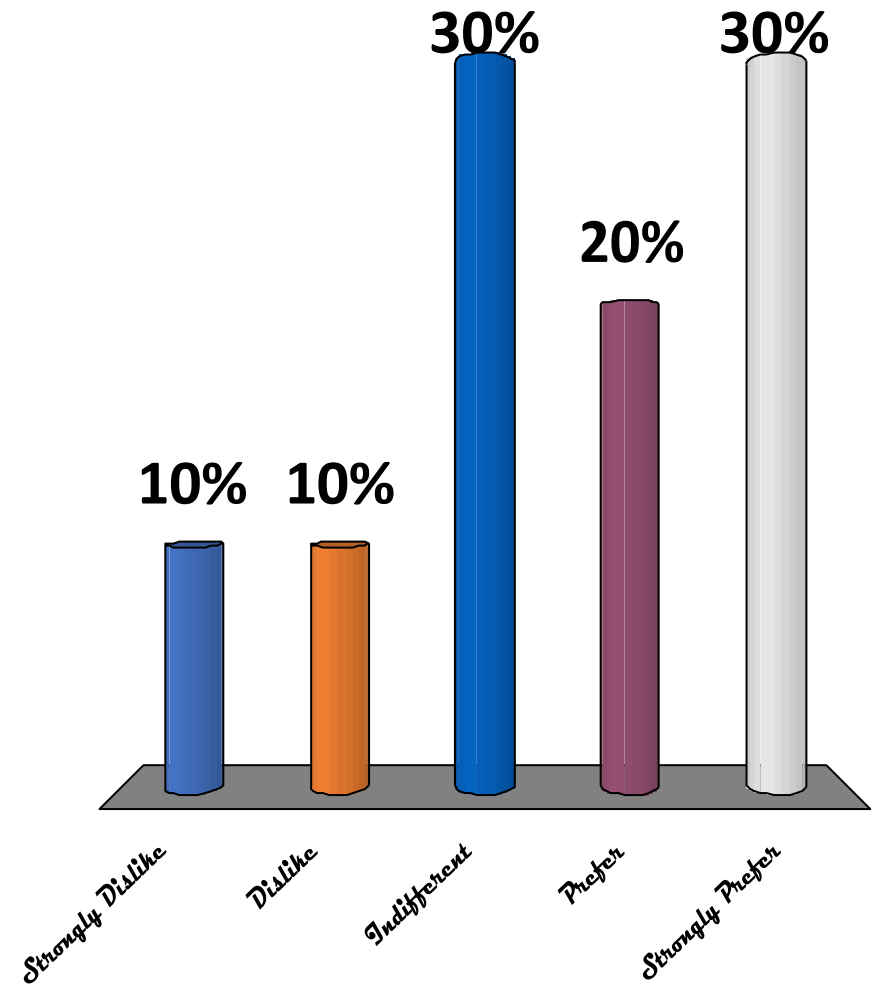


Test Question

What do you think of this llama?



Mean = 3.50 Item 14.



Architectural Styles

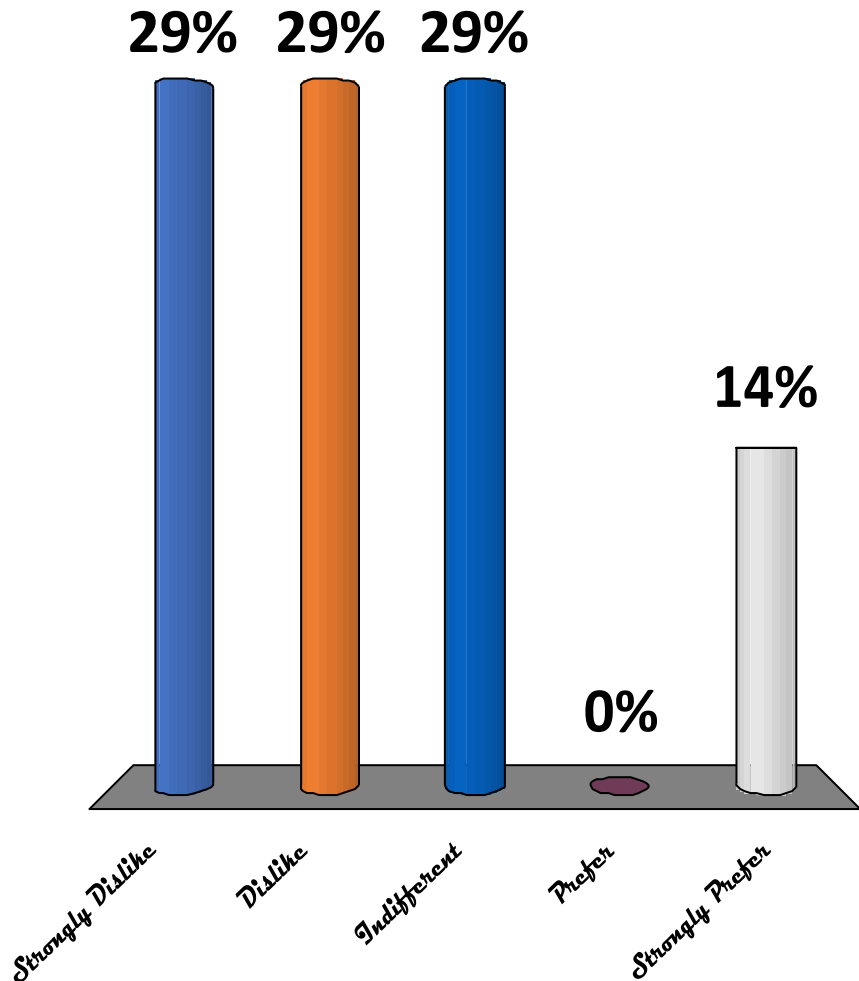
Architectural Styles

Traditional Downtown
(Italiante and Victorian)



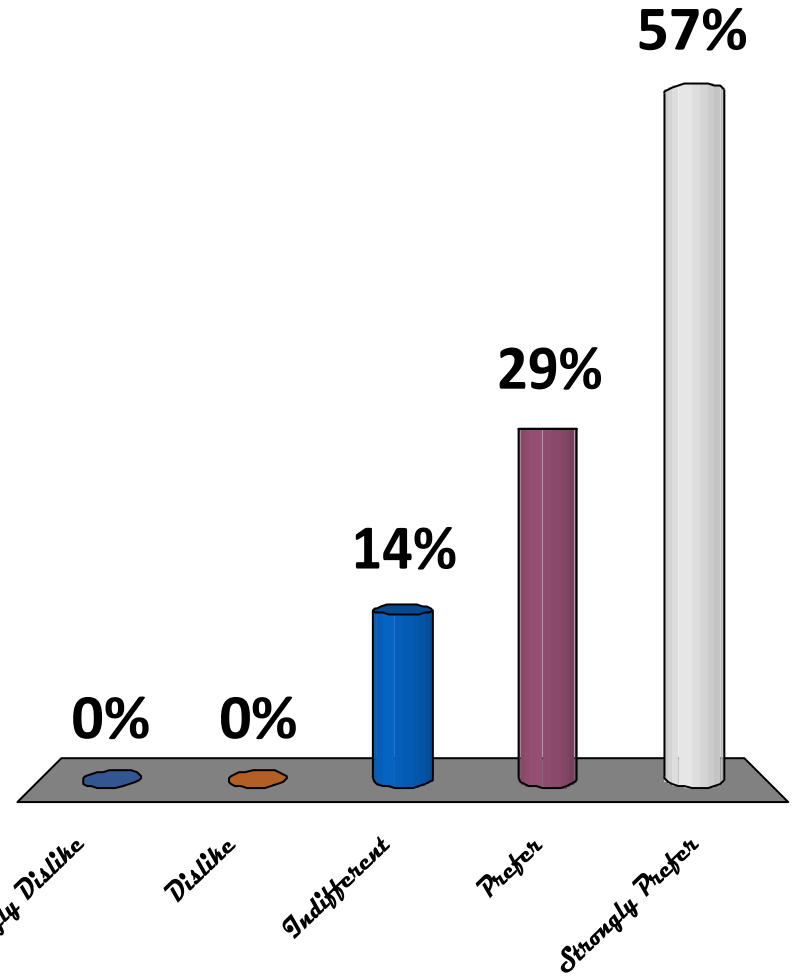
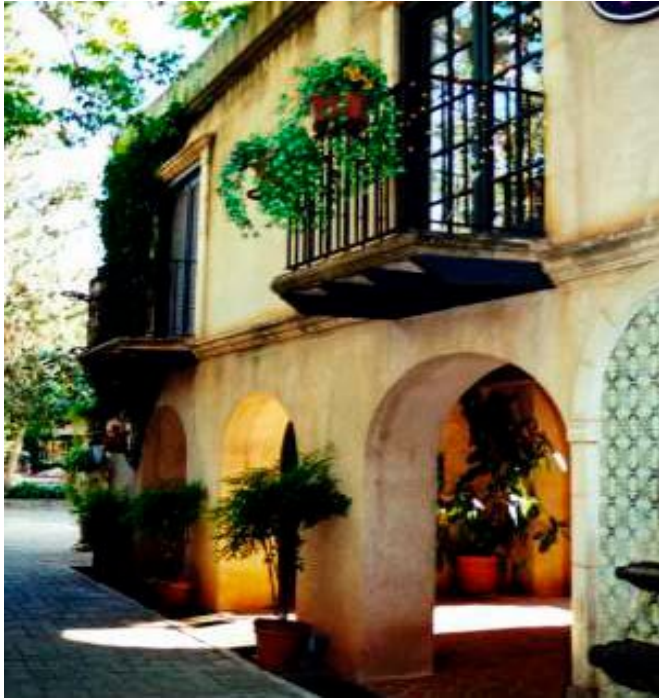
Mean = 2.43

Item 14.



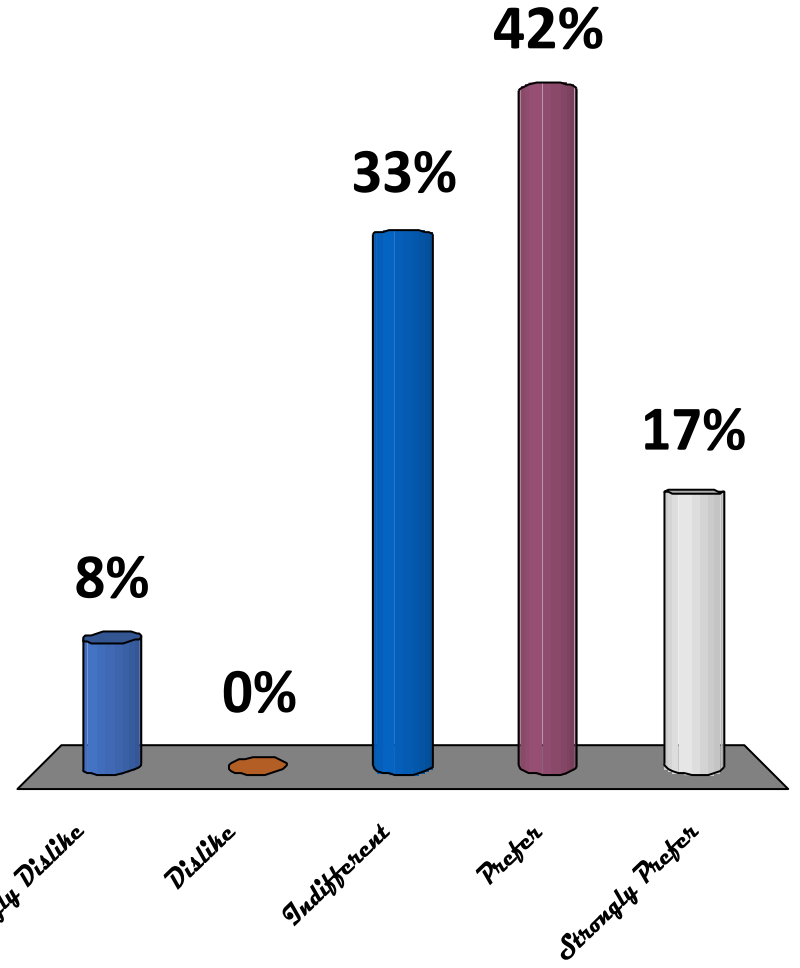
Architectural Styles | Spanish Colonial Revival

Mean = 4.43 Item 14.



Architectural Styles | Mission Revival

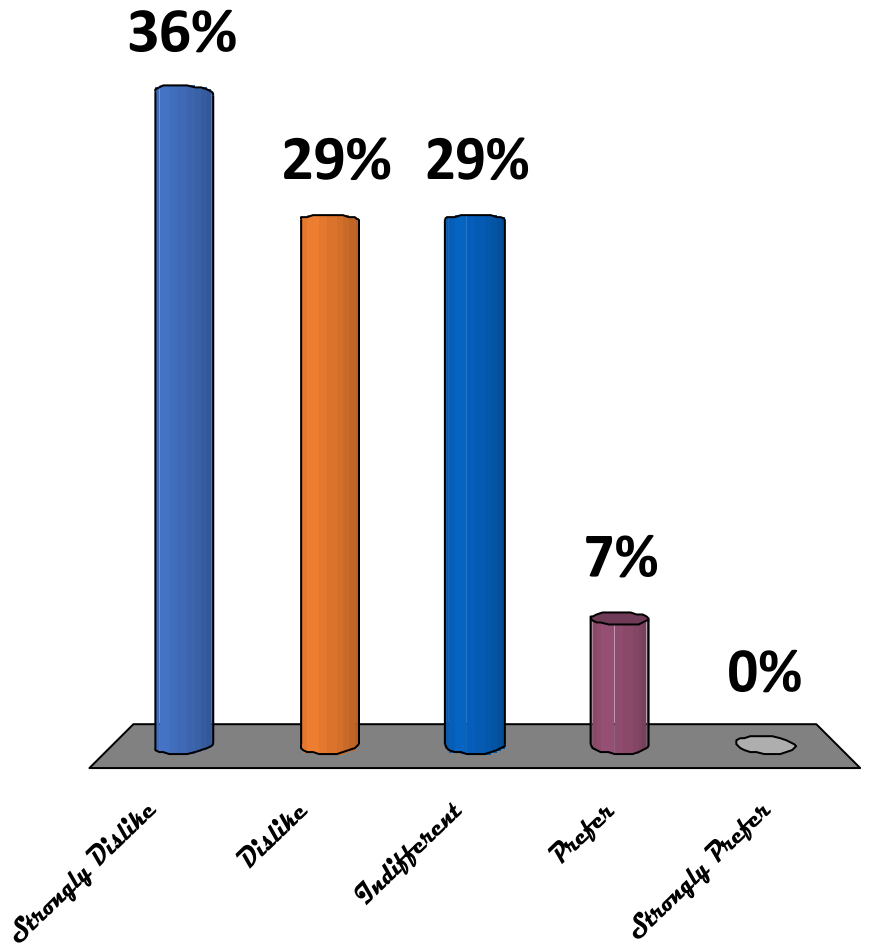
Mean = 3.58 Item 14.



Architectural Styles | Contemporary



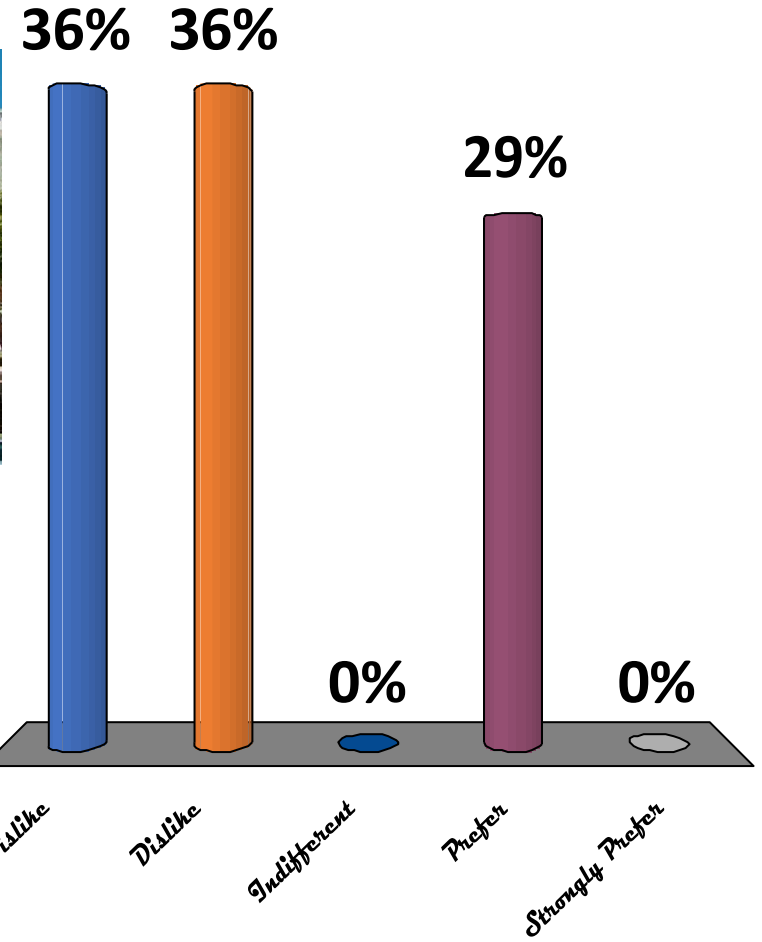
Mean = 2.07 Item 14.



Architectural Styles | Art Deco

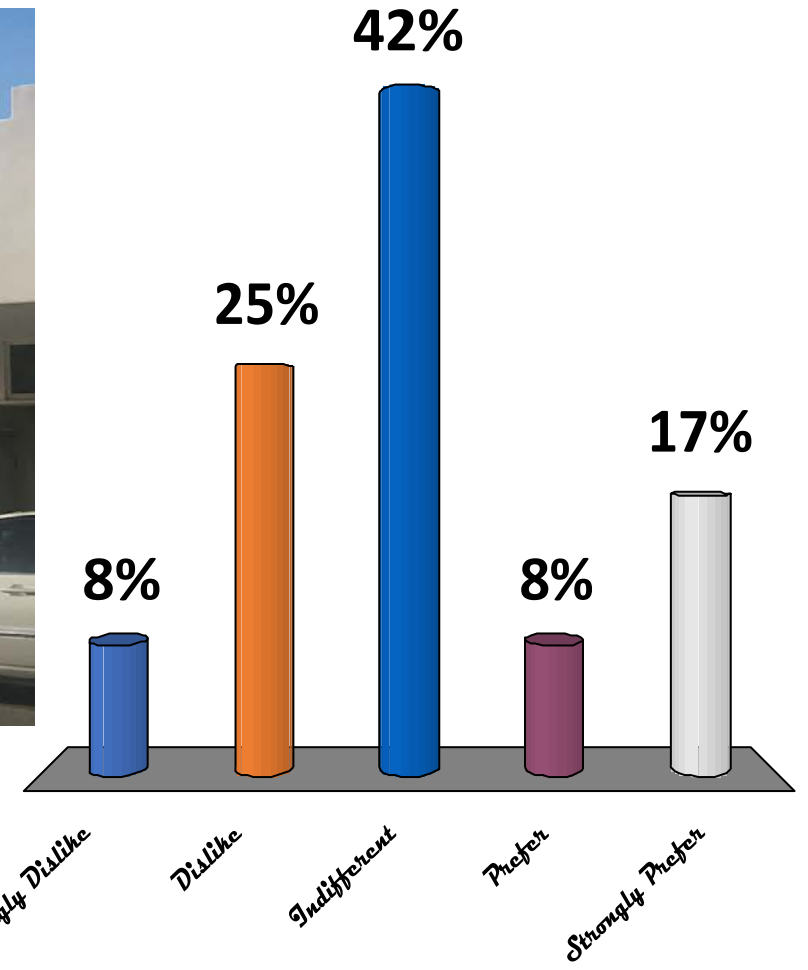
Item 14.

Mean = 2.21



Architectural Styles | Local Art Deco

Mean = 3.00 Item 14.

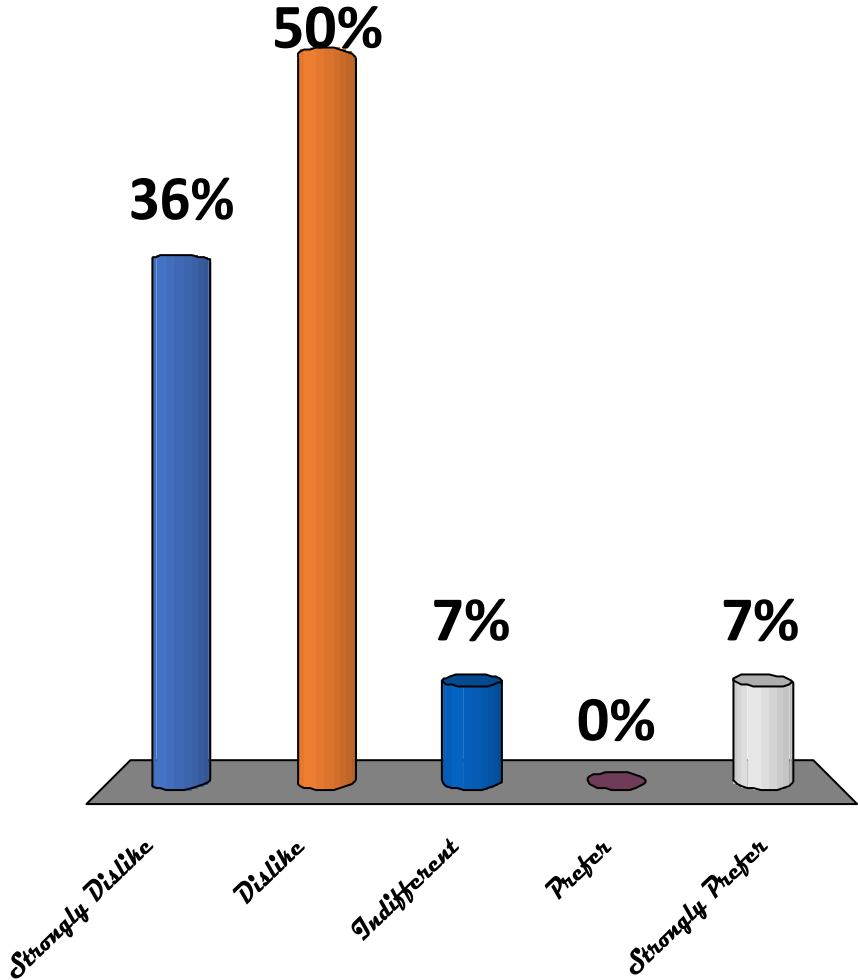


Architectural Styles | Modernism



Mean = 1.93

Item 14.



Architectural Styles | Googie Style

Mean = 1.92

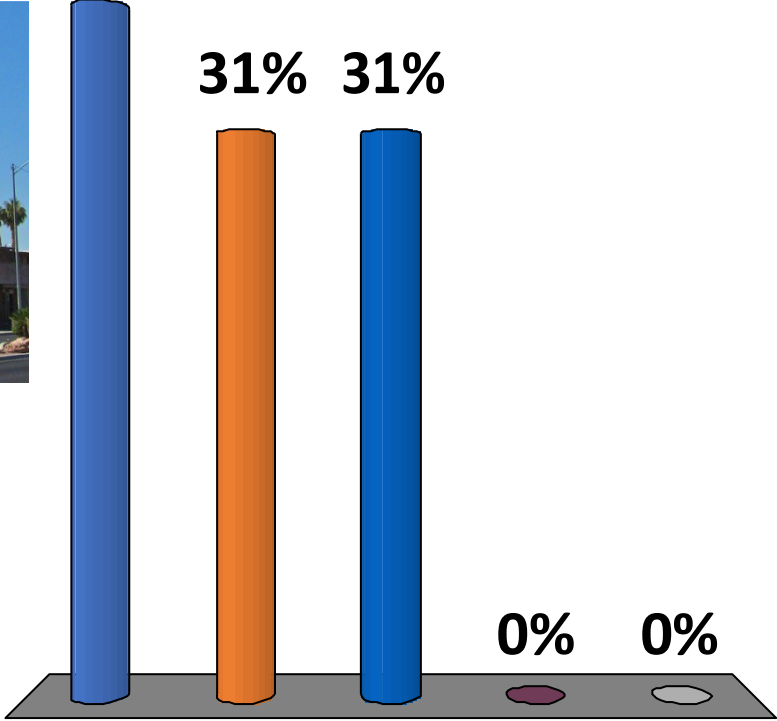
Item 14.



38%

31% 31%

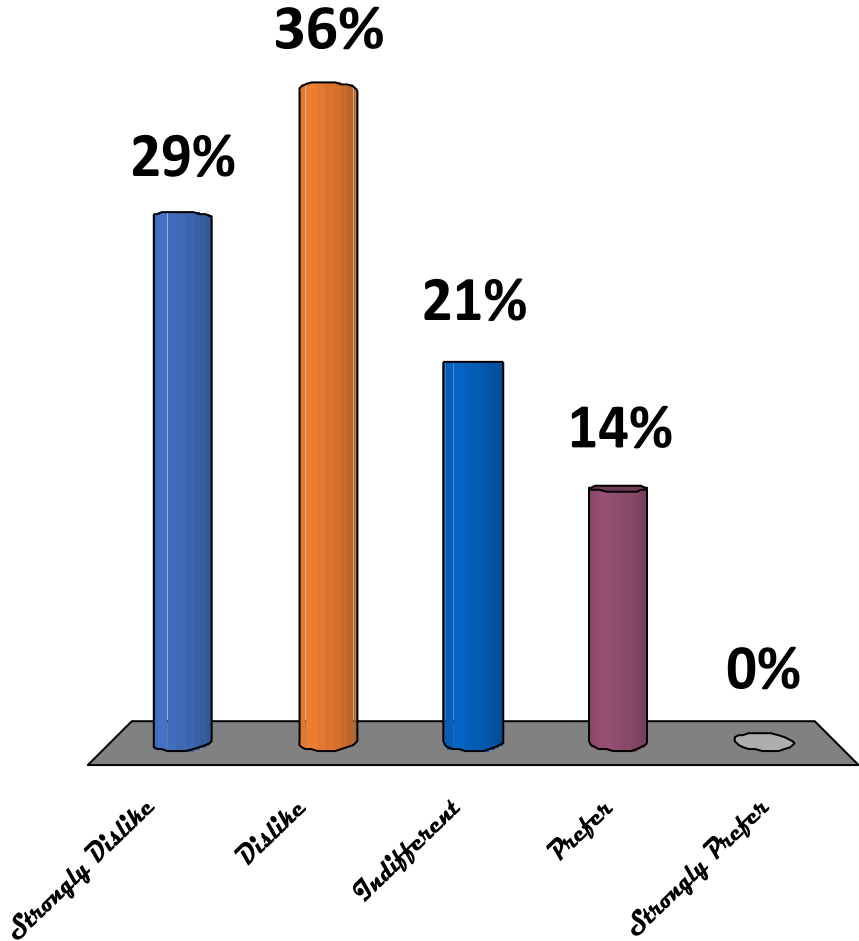
0% 0%



Architectural Styles | Post-Modern

Mean = 2.21

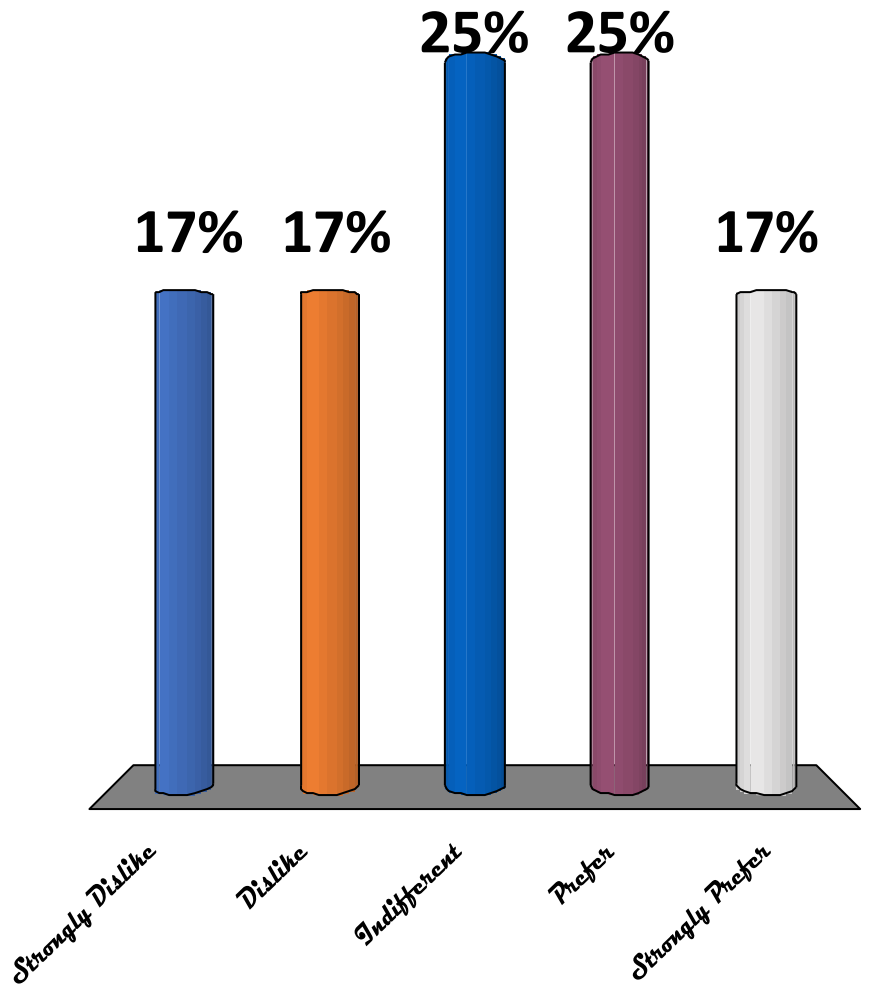
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Architectural Styles | Blended Re-use



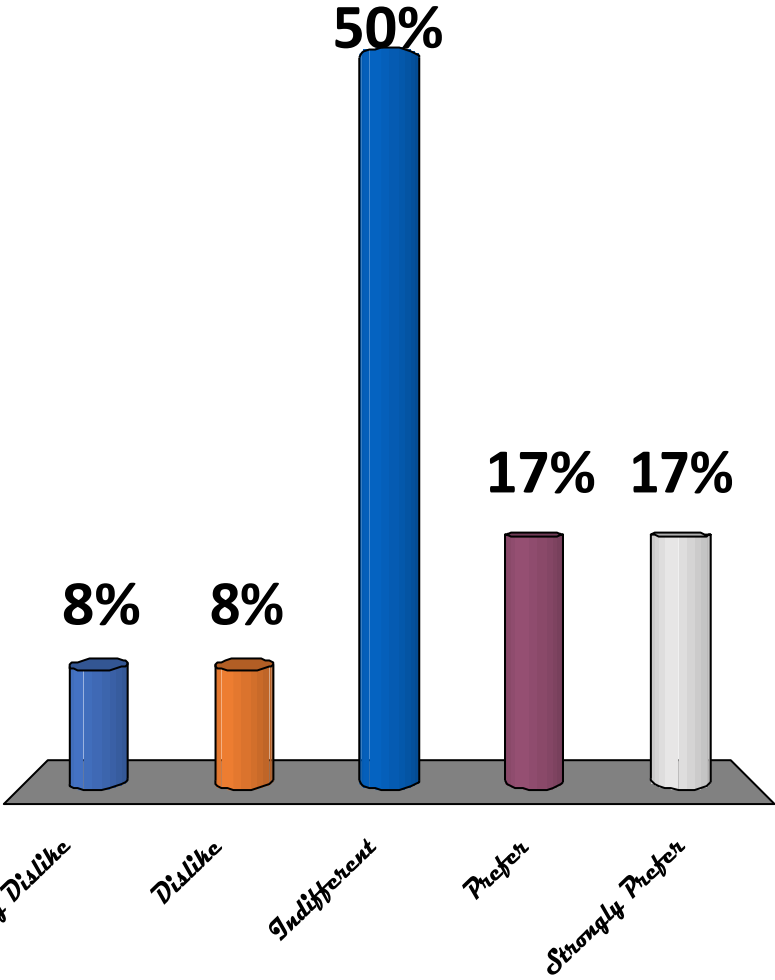
Mean = 3.08 Item 14.



Architectural Styles | Mono-Style

Mean = 3.25

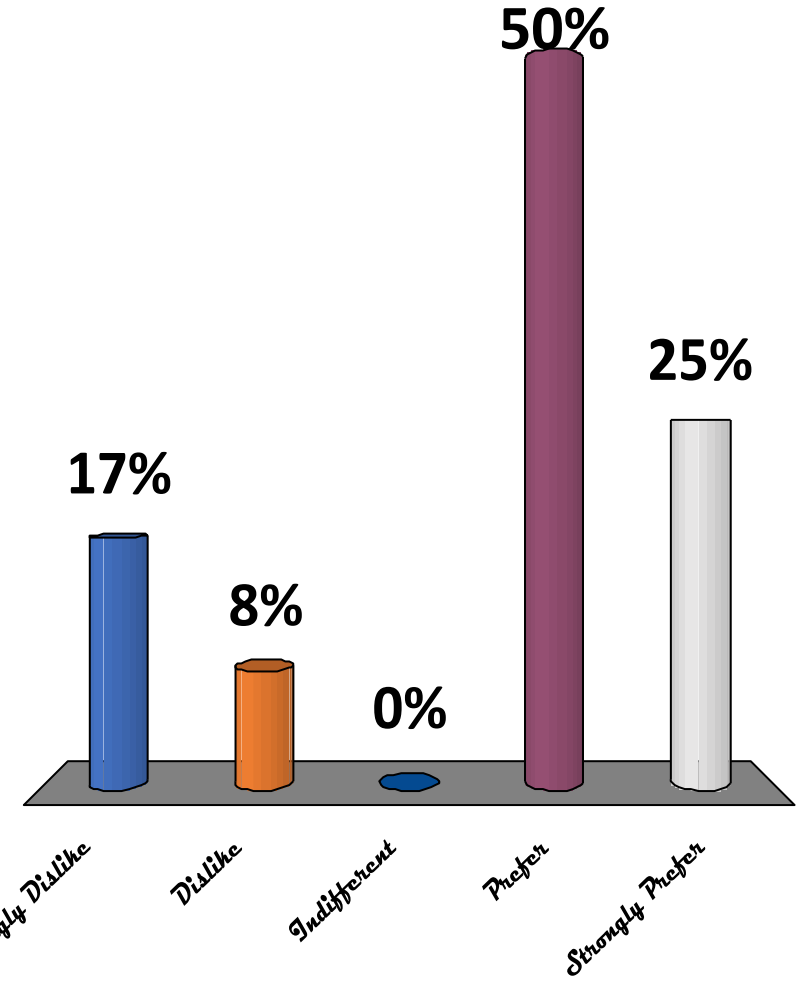
Item 14.



Architectural Styles | Mixed Styles

Mean = 3.58

Item 14.

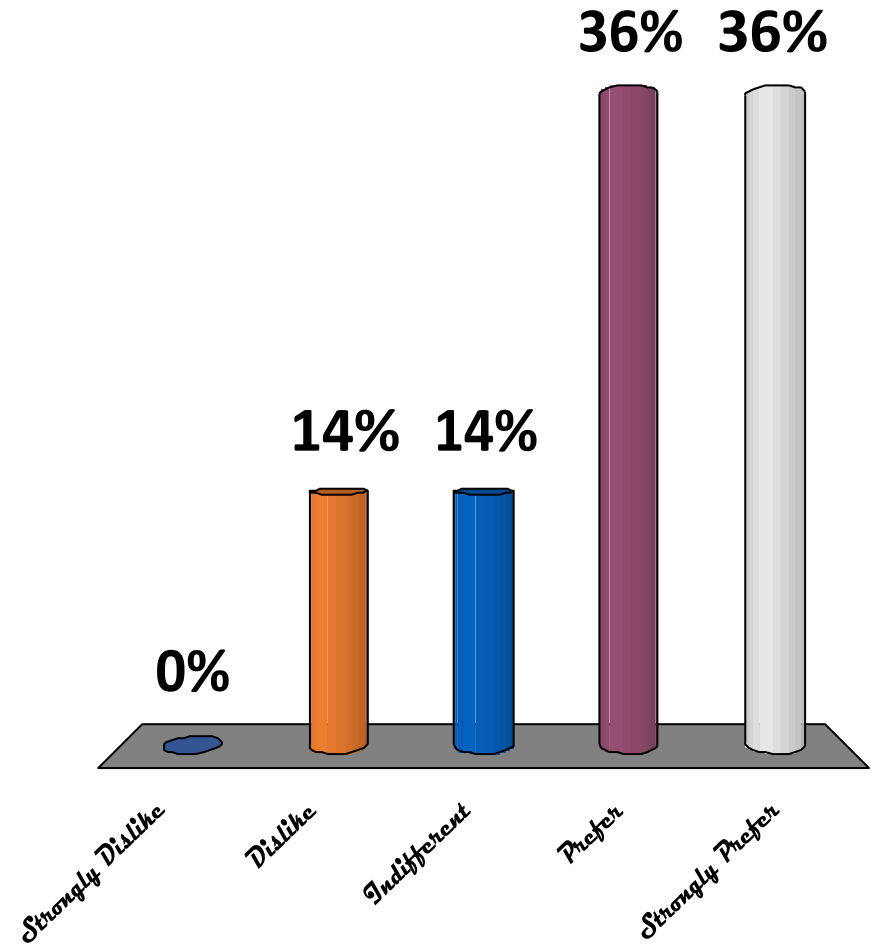


Business Signage

Business Sign Styles | Hanging Signs



Mean = 3.93 Item 14.

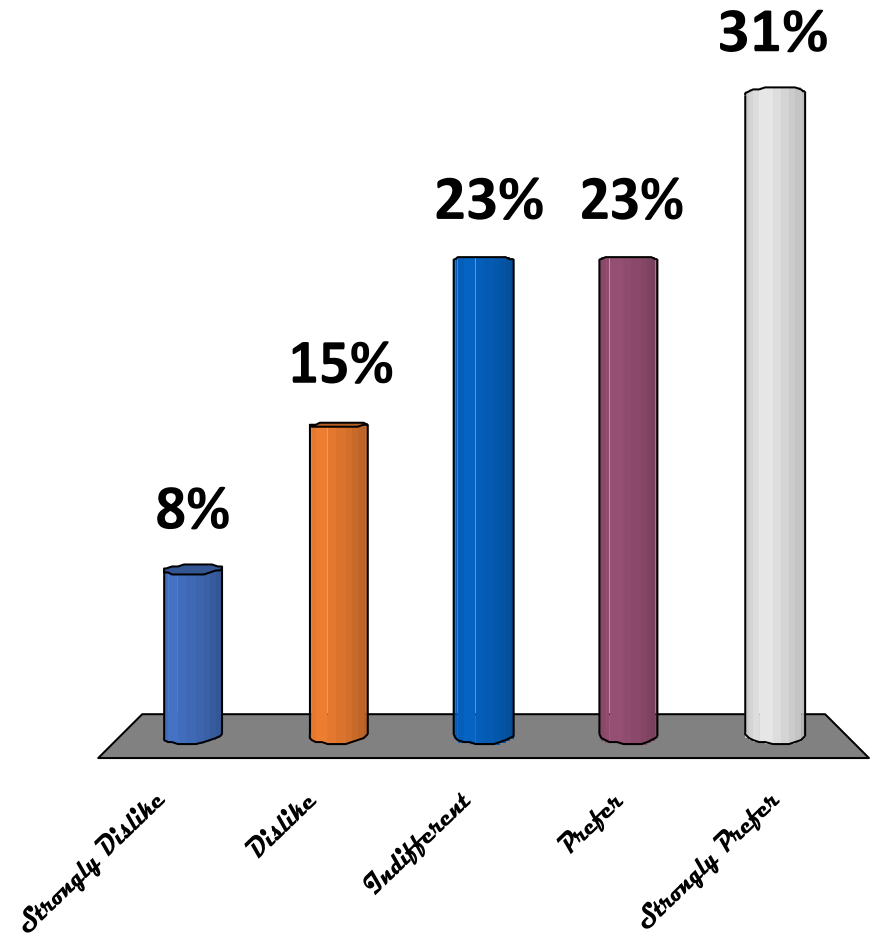


Business Sign Styles | Hanging Under Canopy



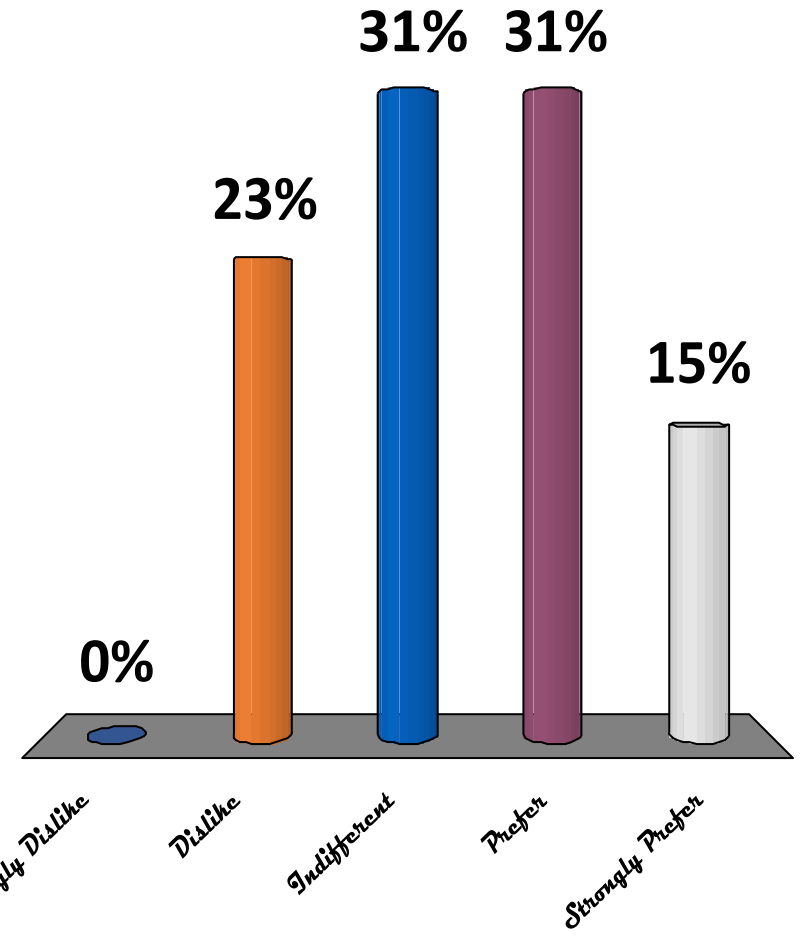
Mean = 3.54

Item 14.



Business Sign Styles | Wall Signs

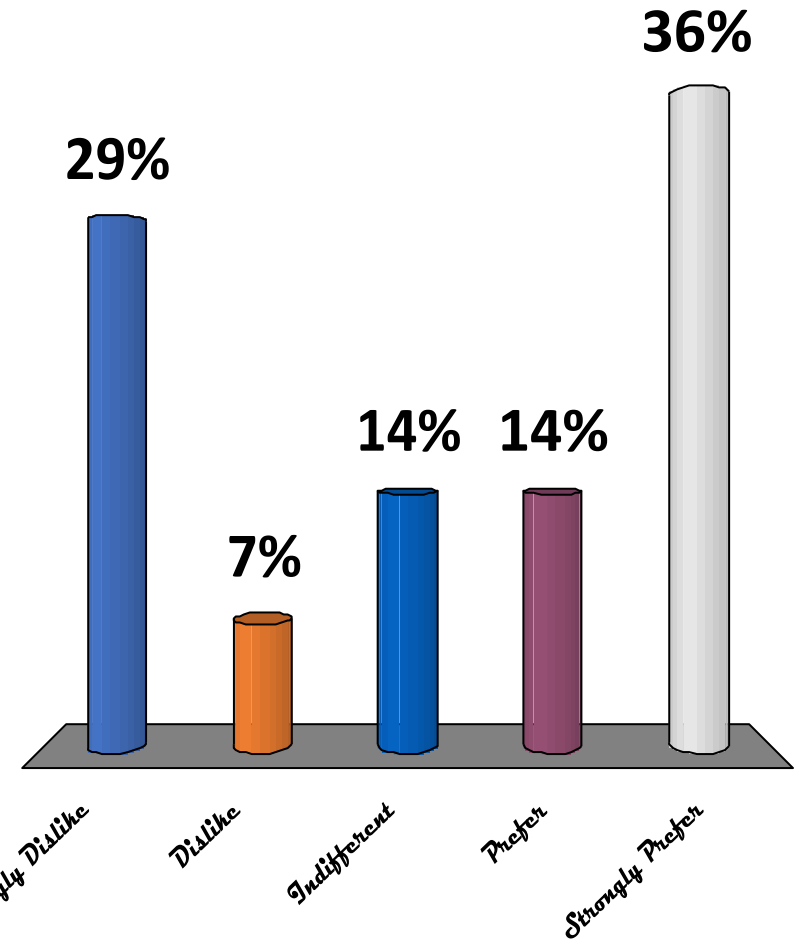
Mean = 3.38 Item 14.



Business Sign Styles | Window Signs



Mean = 3.21 Item 14.



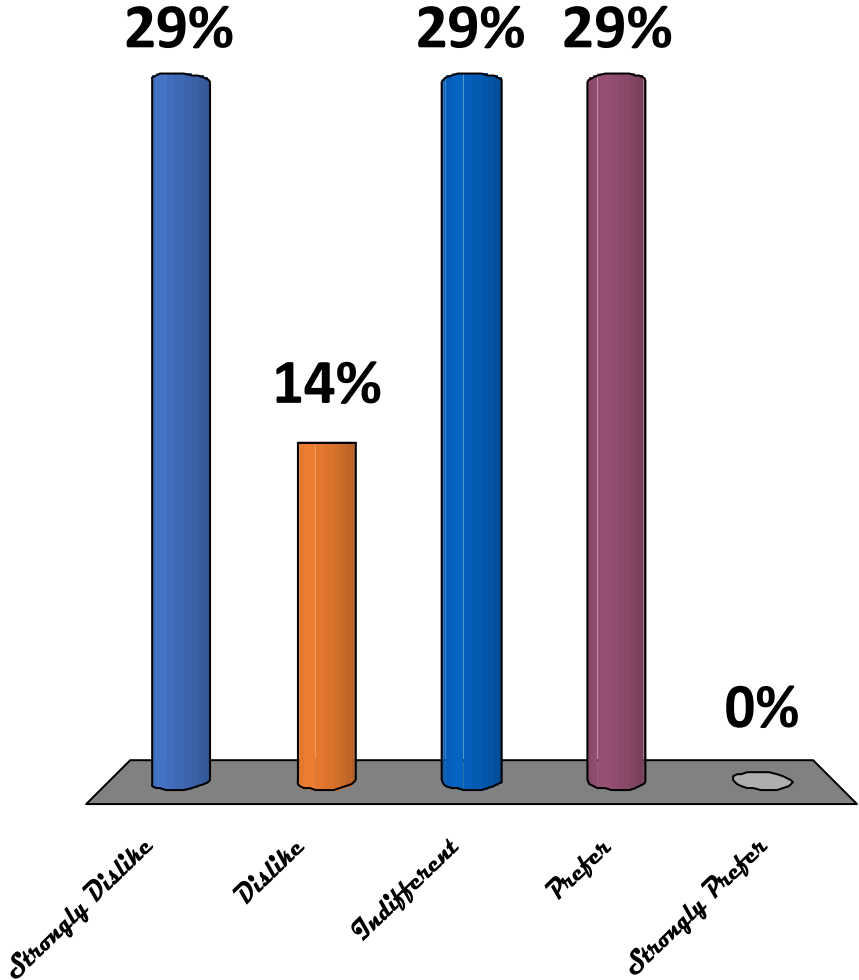
Business Sign Styles | Awning/Canopy Signs



Prefer



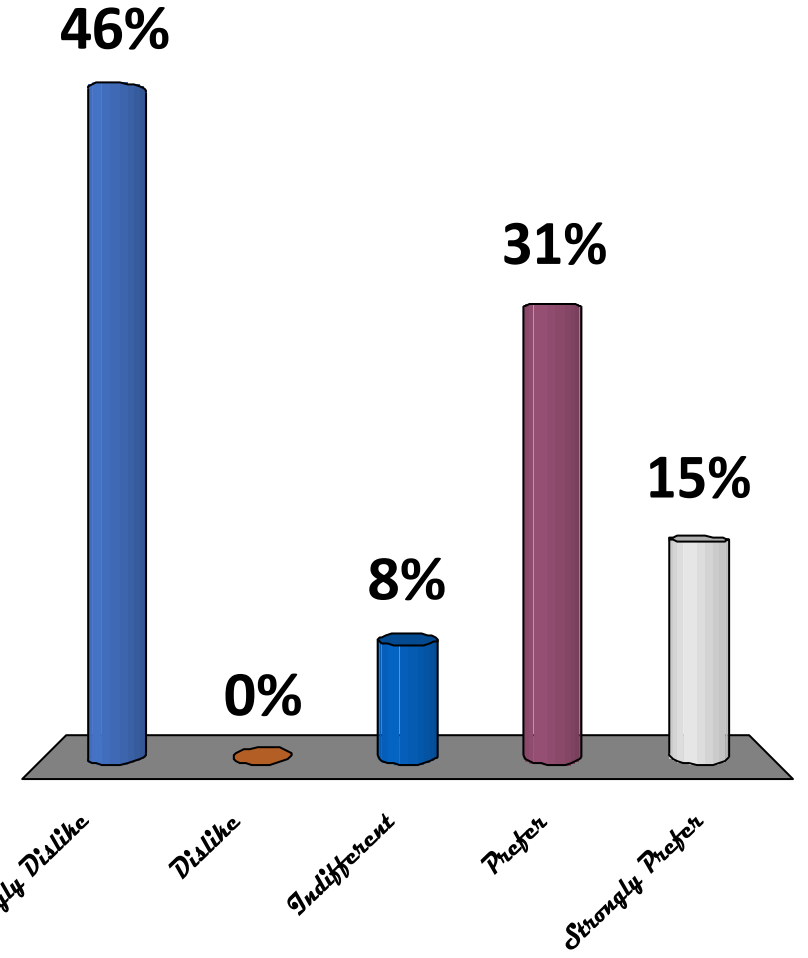
Mean = 2.57 Item 14.



Business Sign Styles | Figurative Signs

Mean = 2.69

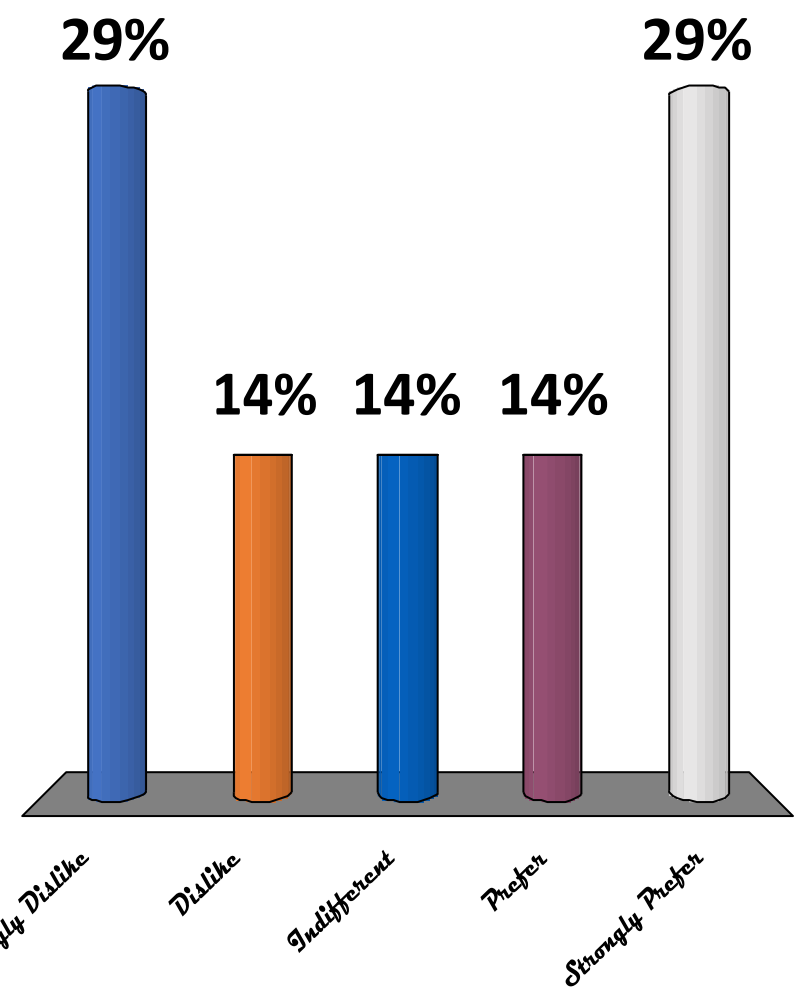
Item 14.



Business Sign Styles | A-Frame Sidewalk Signs (*Temporary*)



Mean = 3.00 Item 14.



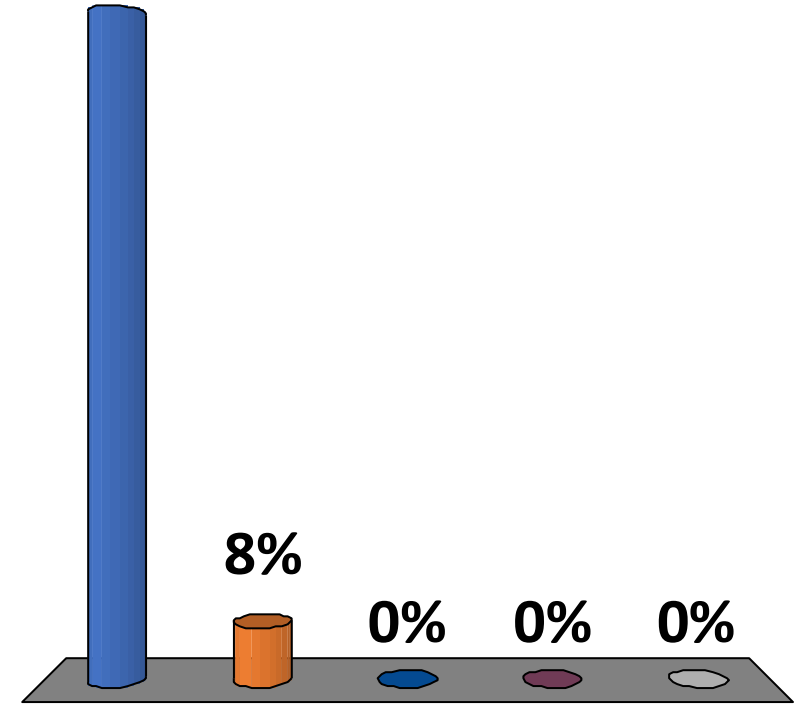
Business Sign Styles | Wooden Post Signs (Temporary)

Mean = 1.08

Item 14.



92%

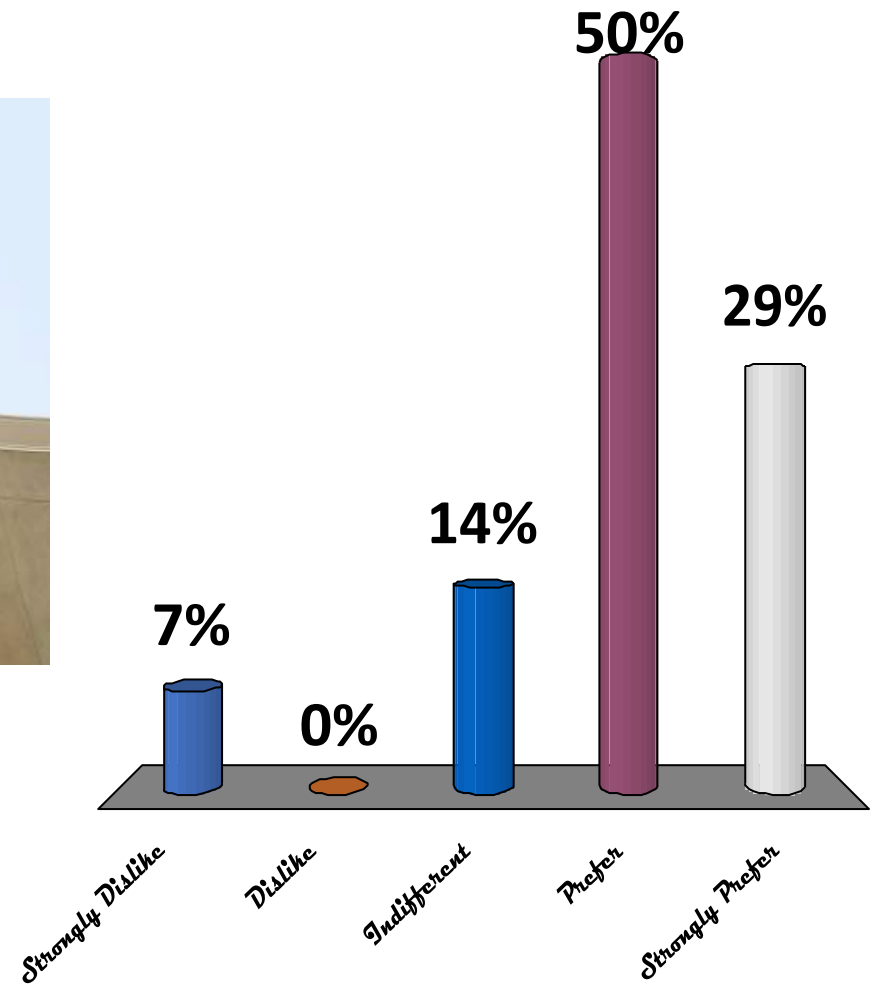


Business Sign Styles | Painted Murals



Mean = 3.93

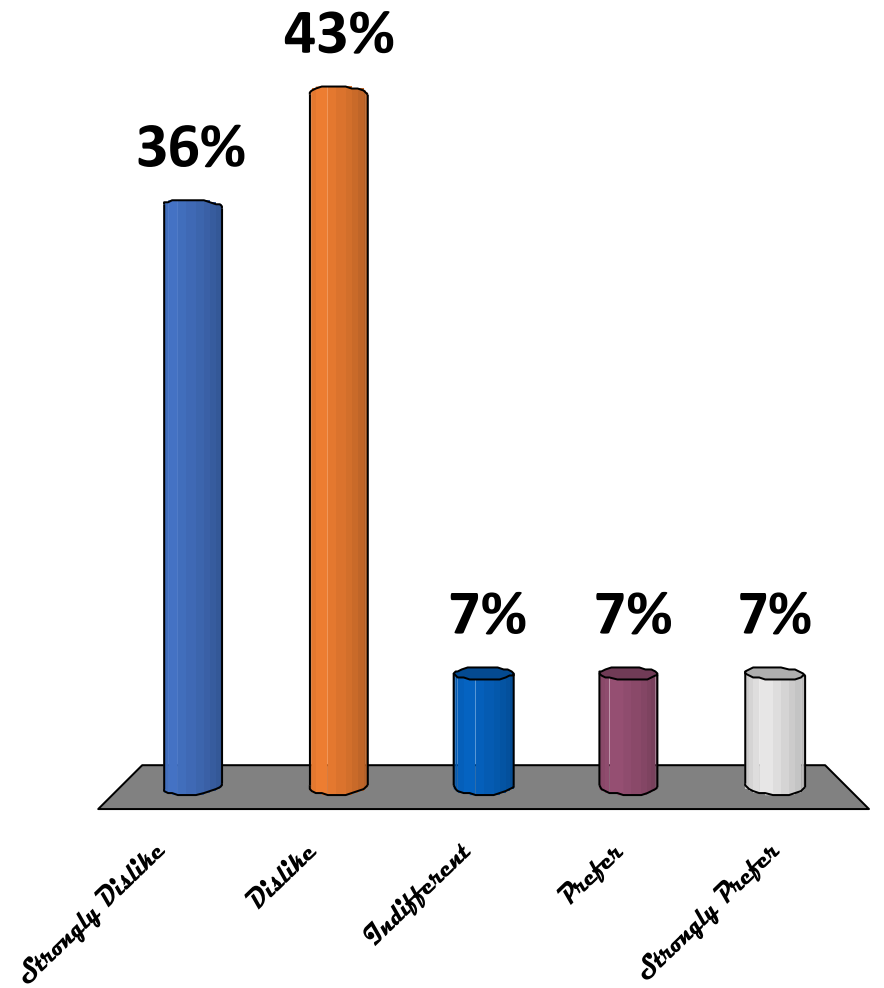
Item 14.



Business Sign Styles | Monument Signs



Mean = 2.07 Item 14.

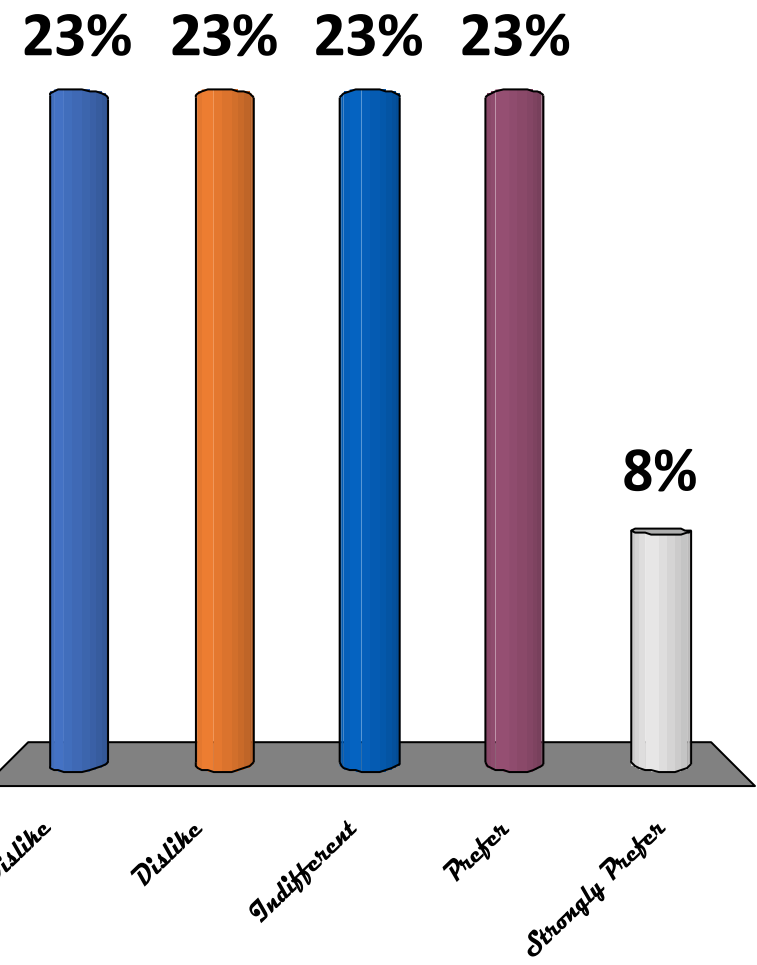


Business Sign Styles | Cabinet Sign



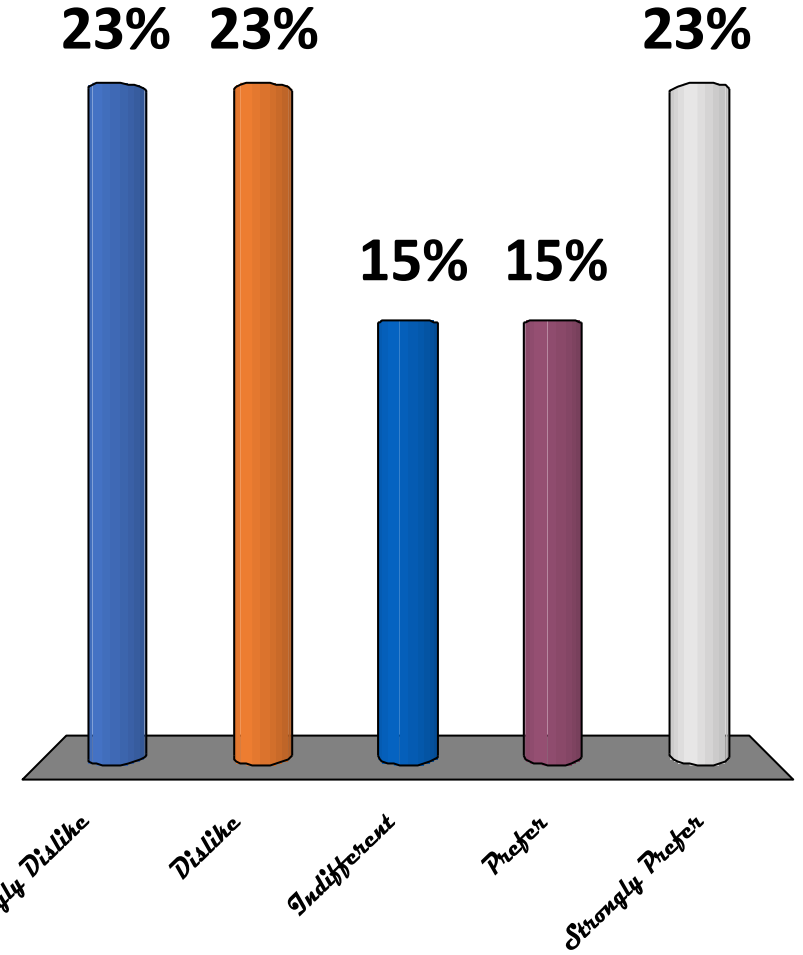
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Item 14.



Business Sign Styles | Lit Box Sign

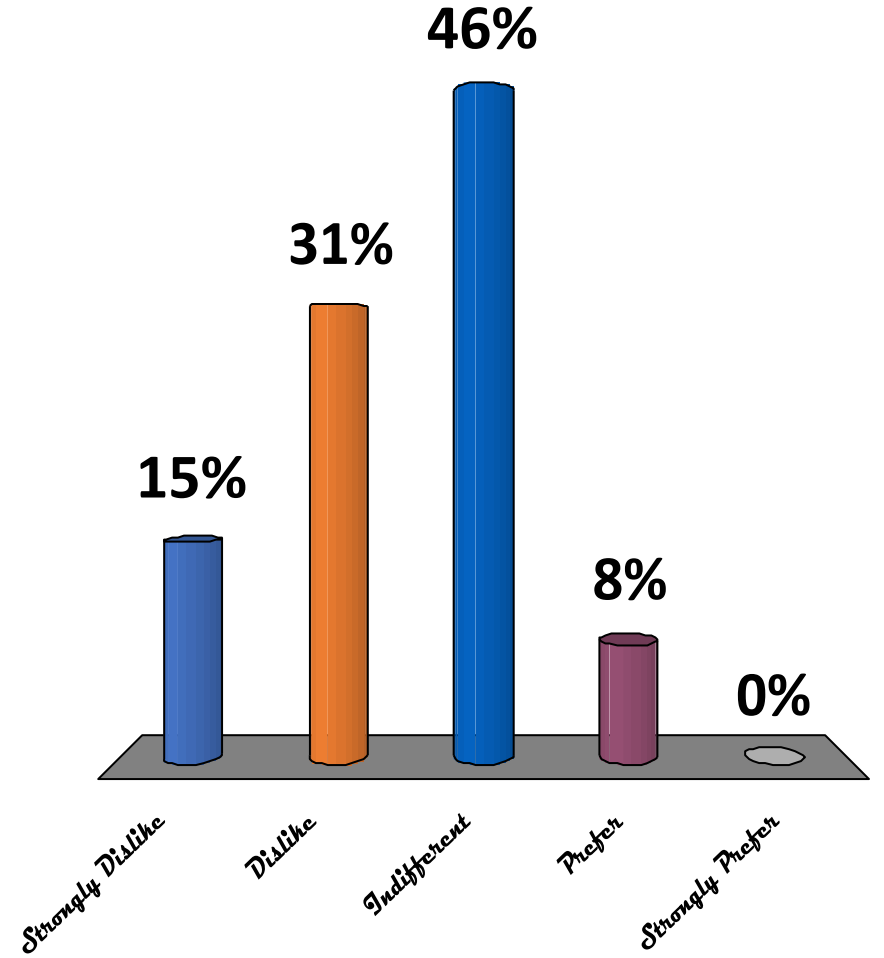
Mean = 2.92 Item 14.



Business Sign Styles | Projected Lighting



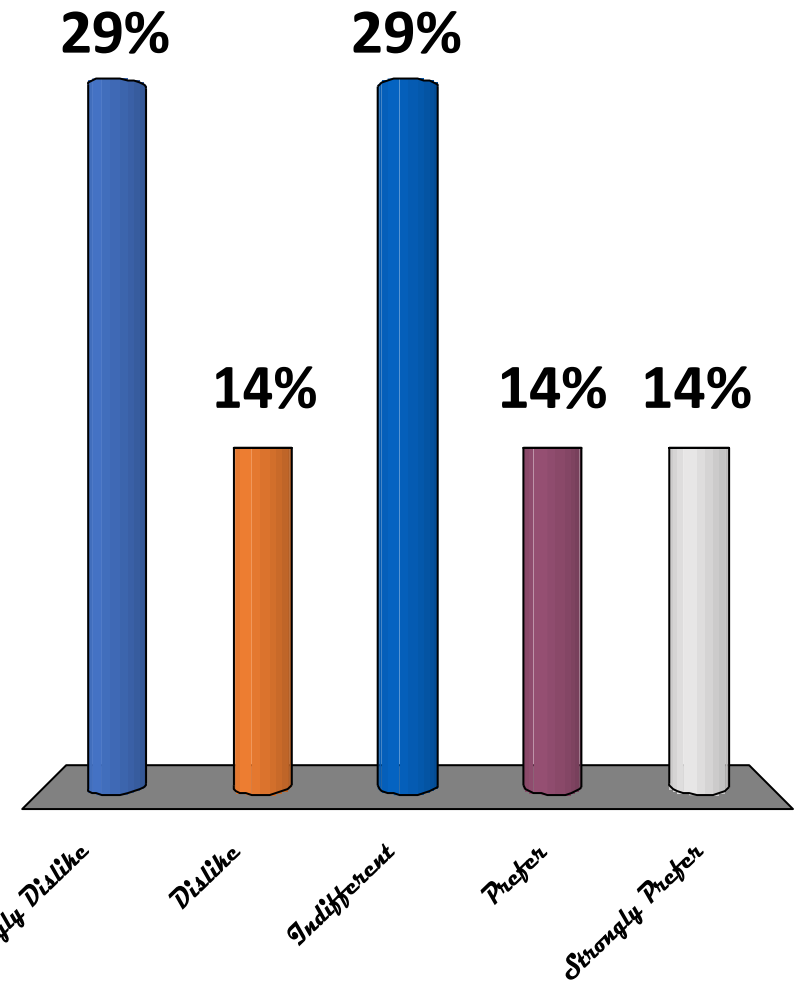
Mean = 2.46 Item 14.



Business Sign Styles | Neon



Mean = 2.71 Item 14.

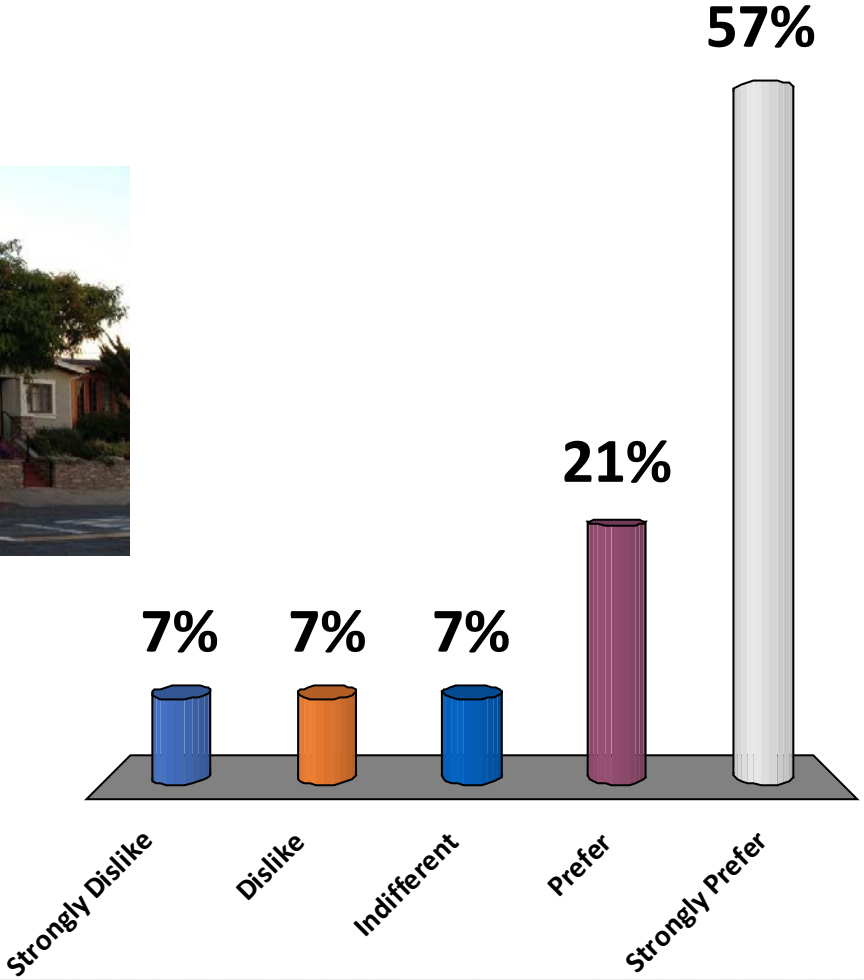


Outdoor Dining

Outdoor Dining | Storefront Adjacent (Sidewalk)

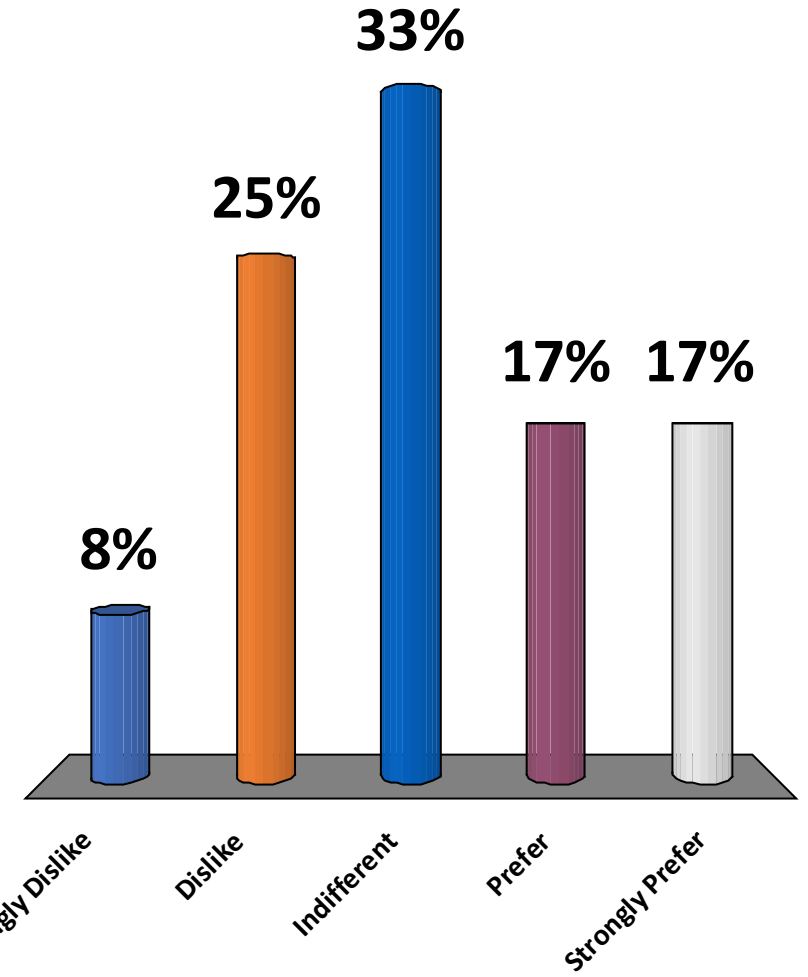
Mean = 4.14

Item 14.



Outdoor Dining | Street Adjacent (Sidewalk)

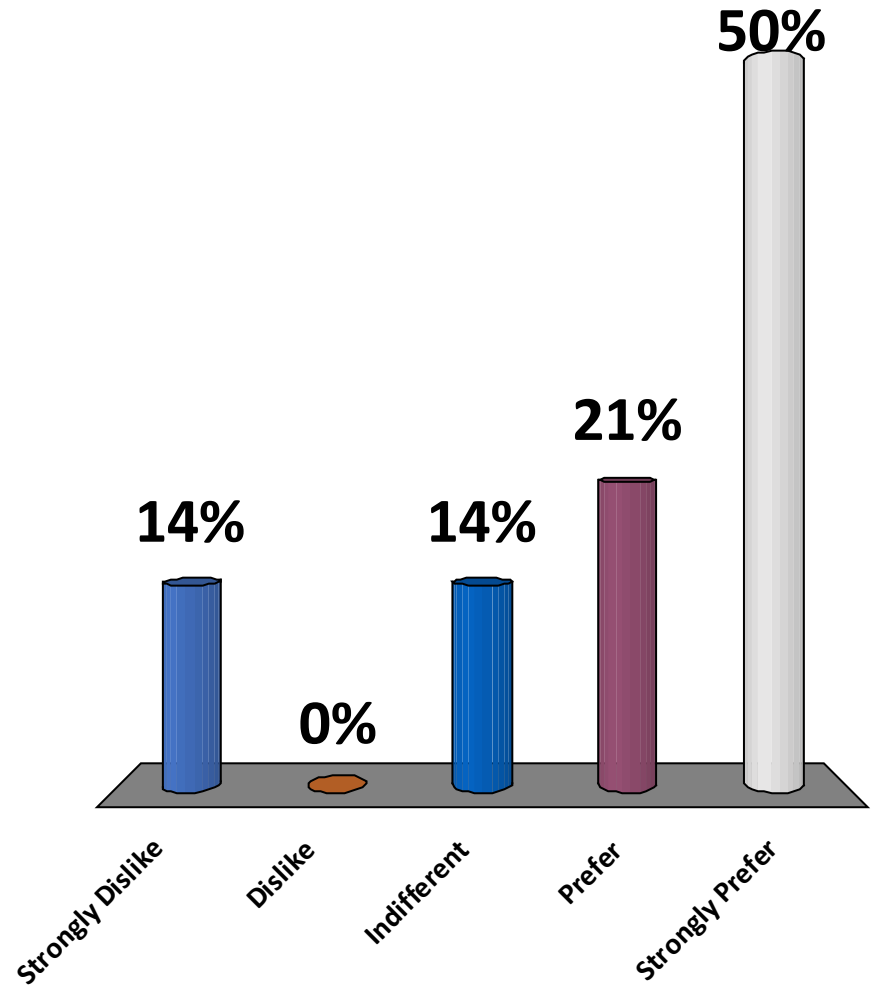
Mean = 3.08 Item 14.



Outdoor Dining | Alley/Building Sides/Paseo

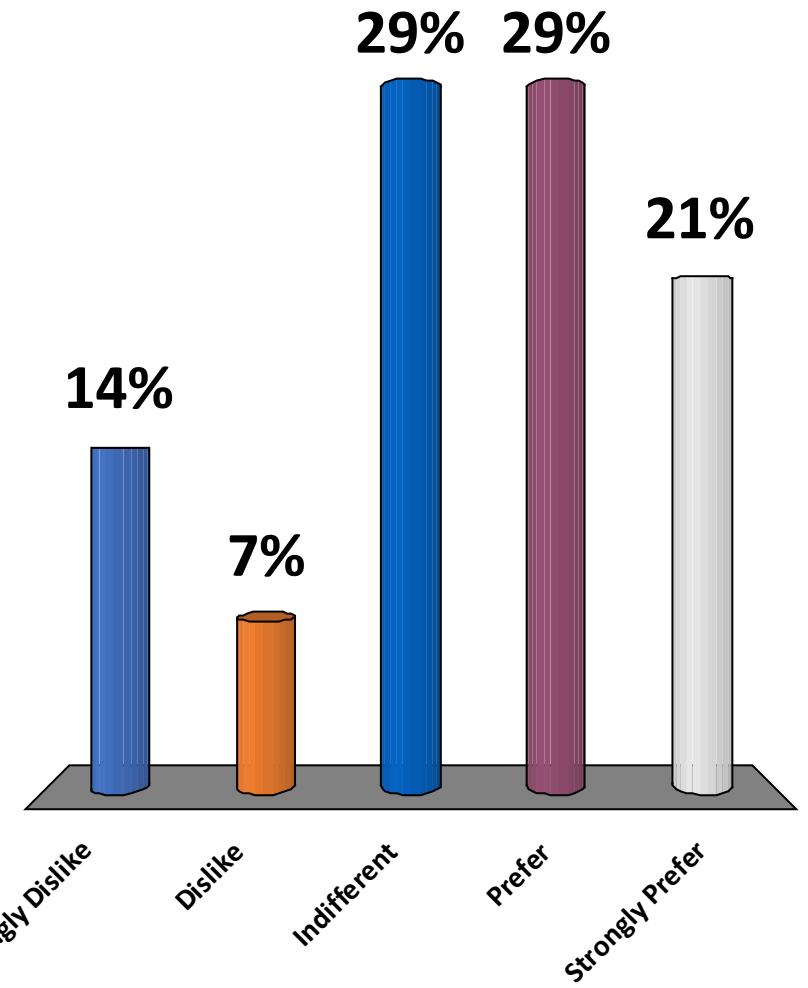


Mean = 3.93 Item 14.



Outdoor Dining Furniture | Metal

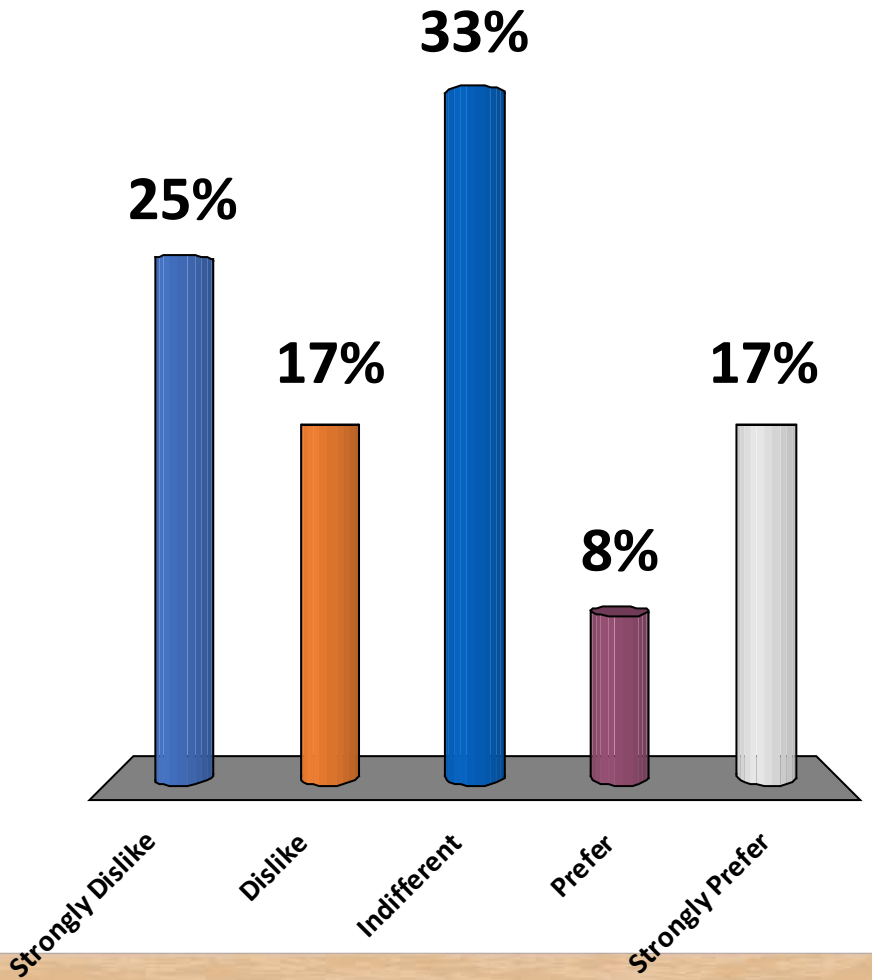
Mean = 3.36 Item 14.



Outdoor Dining Furniture | Wicker

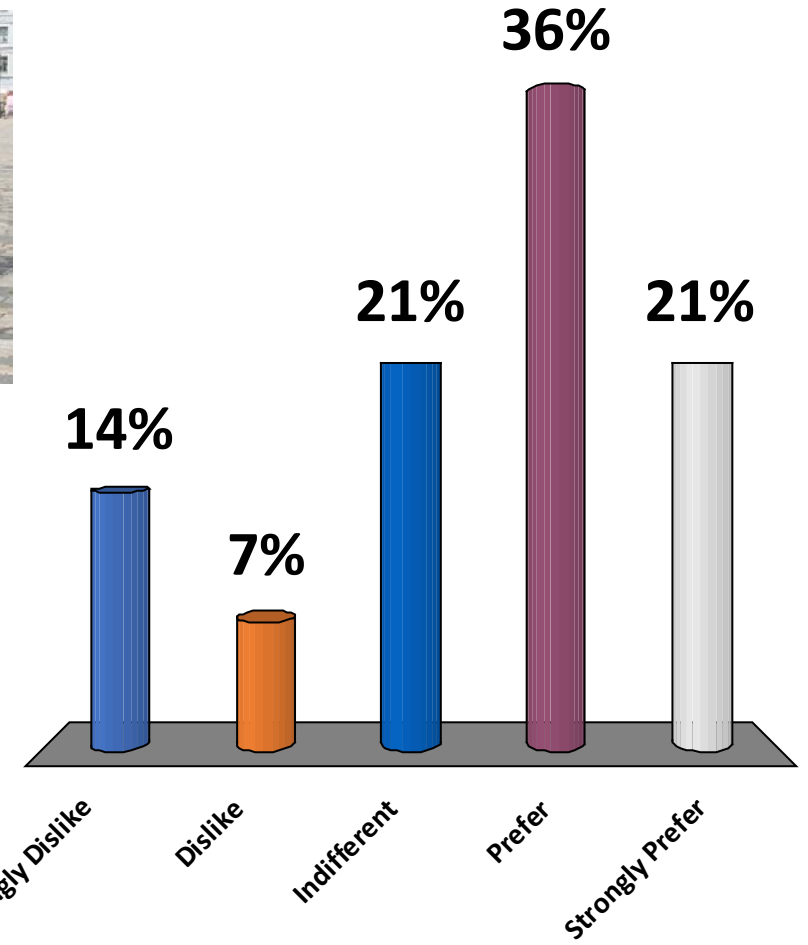
Mean = 2.75

Item 14.



Outdoor Dining Furniture | Wood

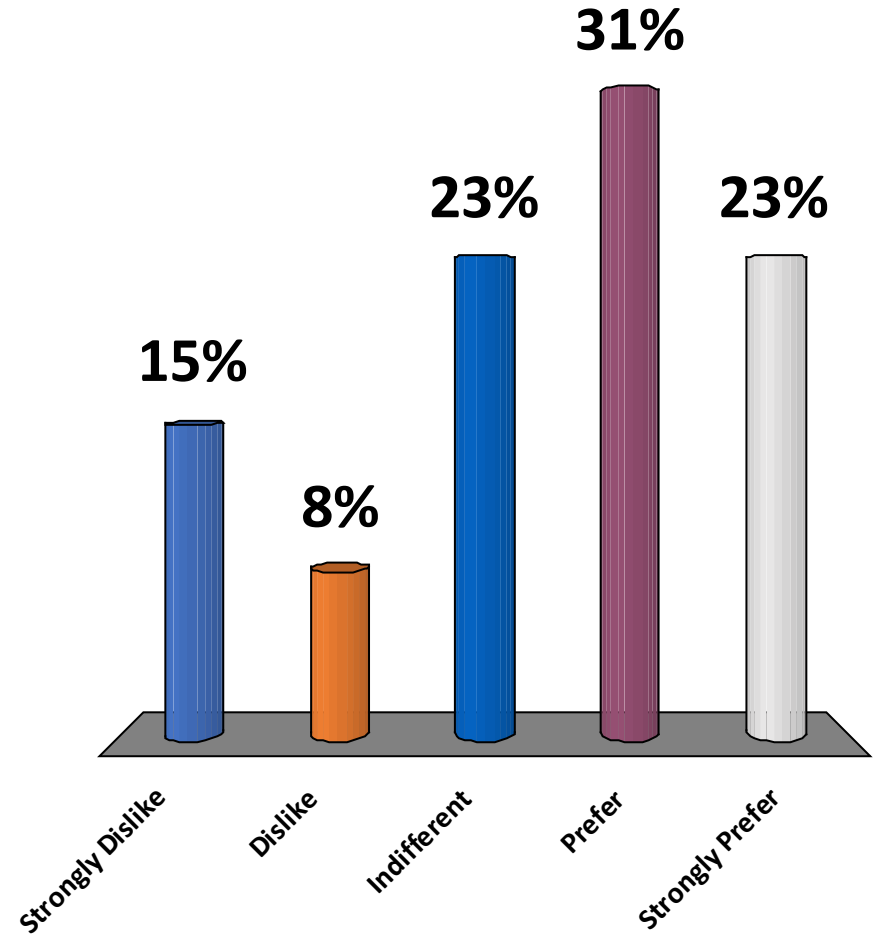
Mean = 3.43 Item 14.



Outdoor Dining Furniture | Concrete

Mean = 3.38

Item 14.



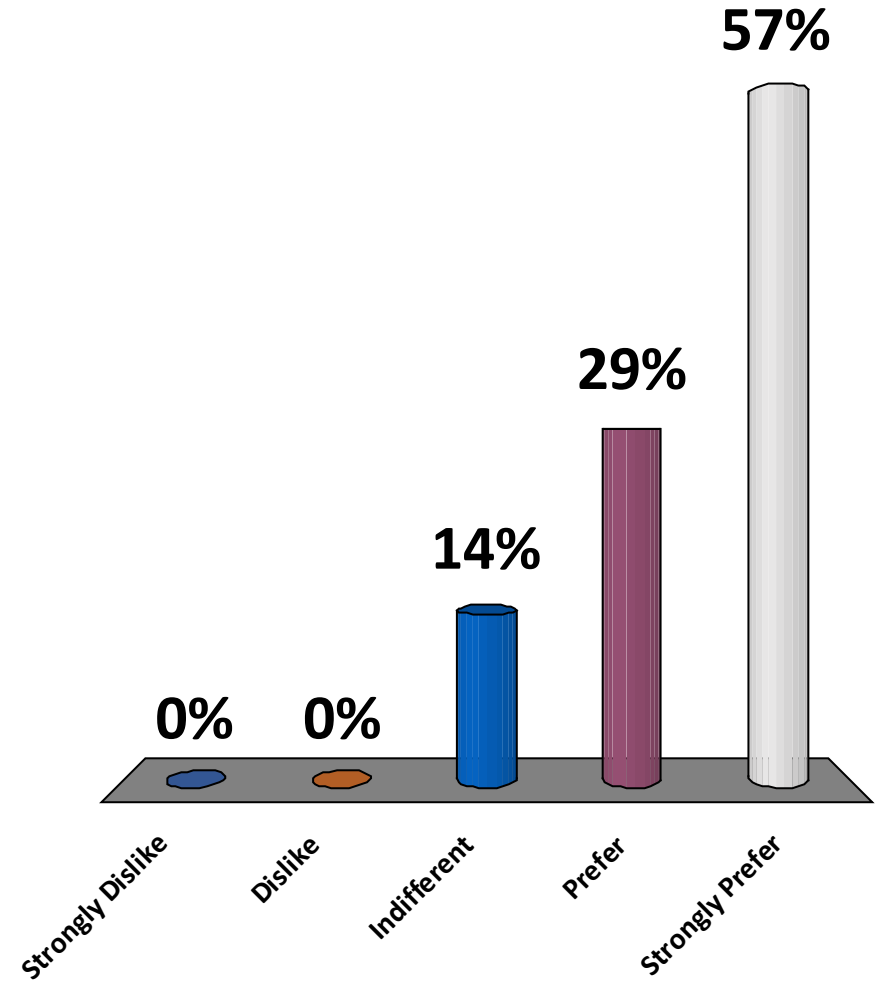
Gateways

Gateway | Horizontal Feature

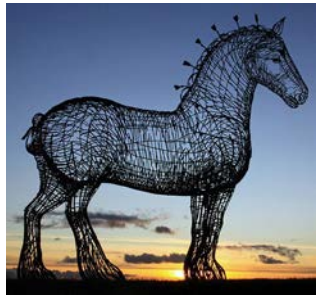


Mean = 4.43

Item 14.

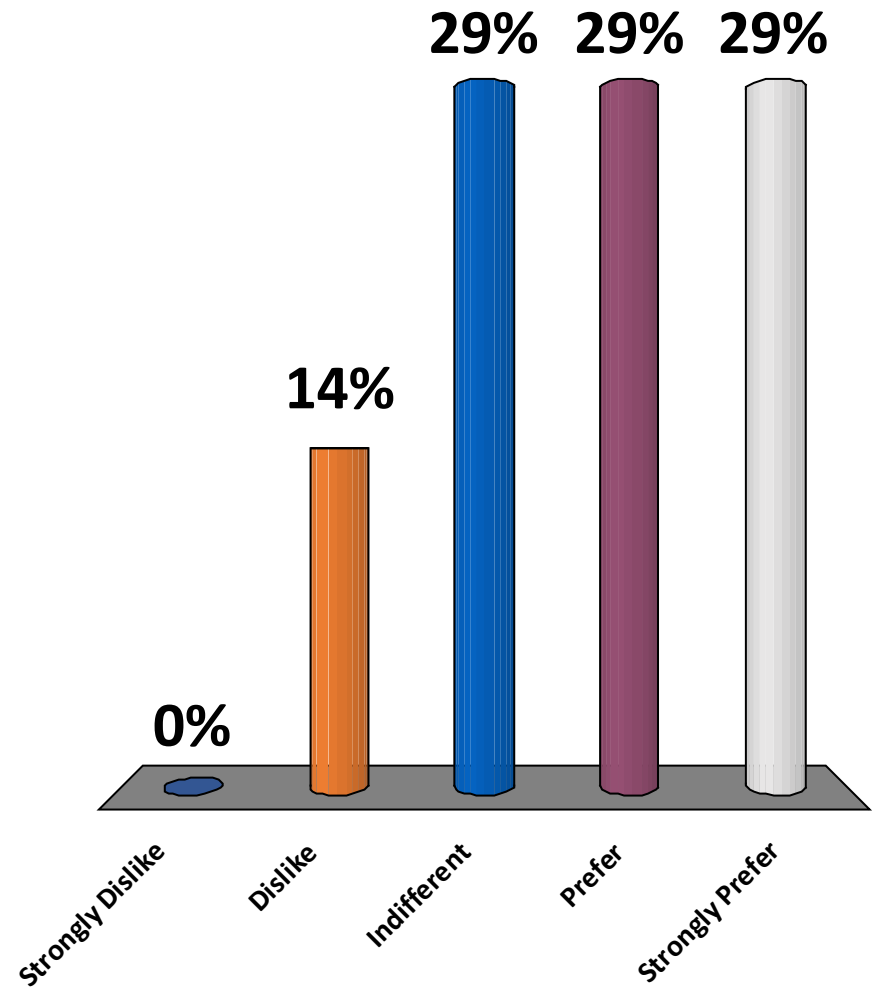


Gateway | Vertical Feature



Mean = 3.71

Item 14.

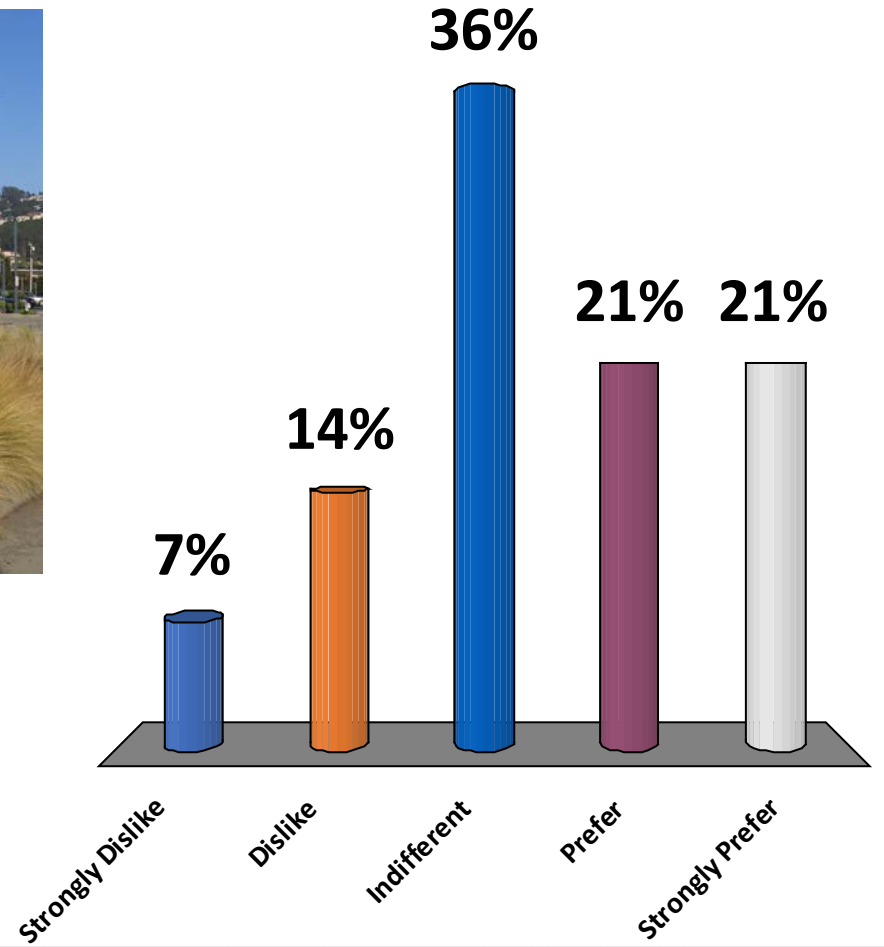


Gateway | Welcome Sign



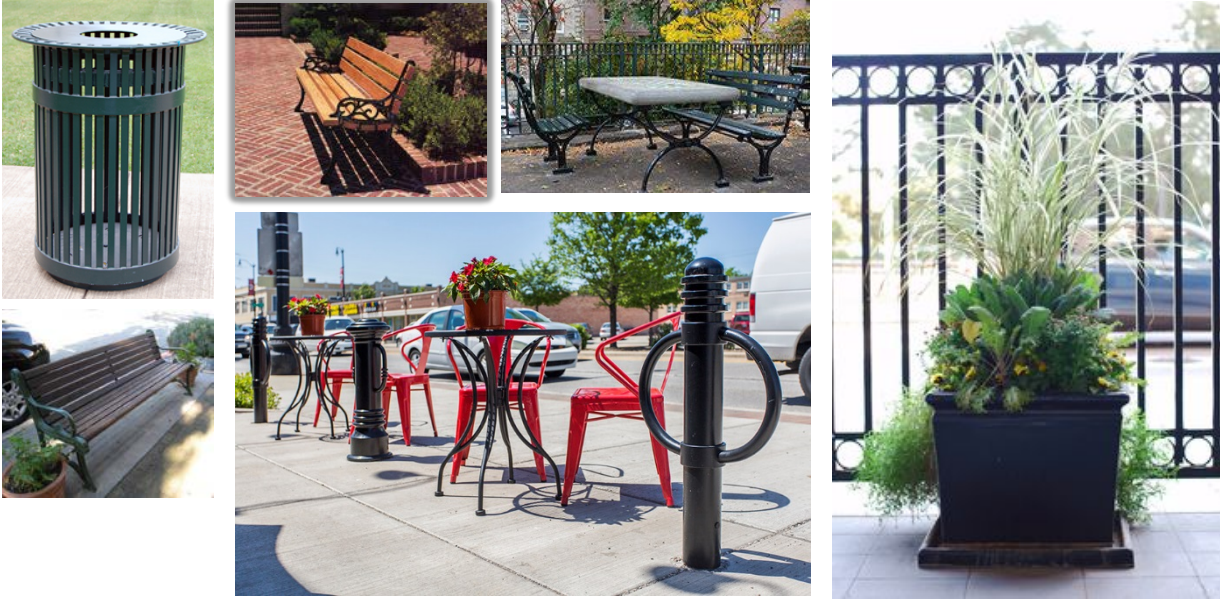
Mean = 3.36

Item 14.

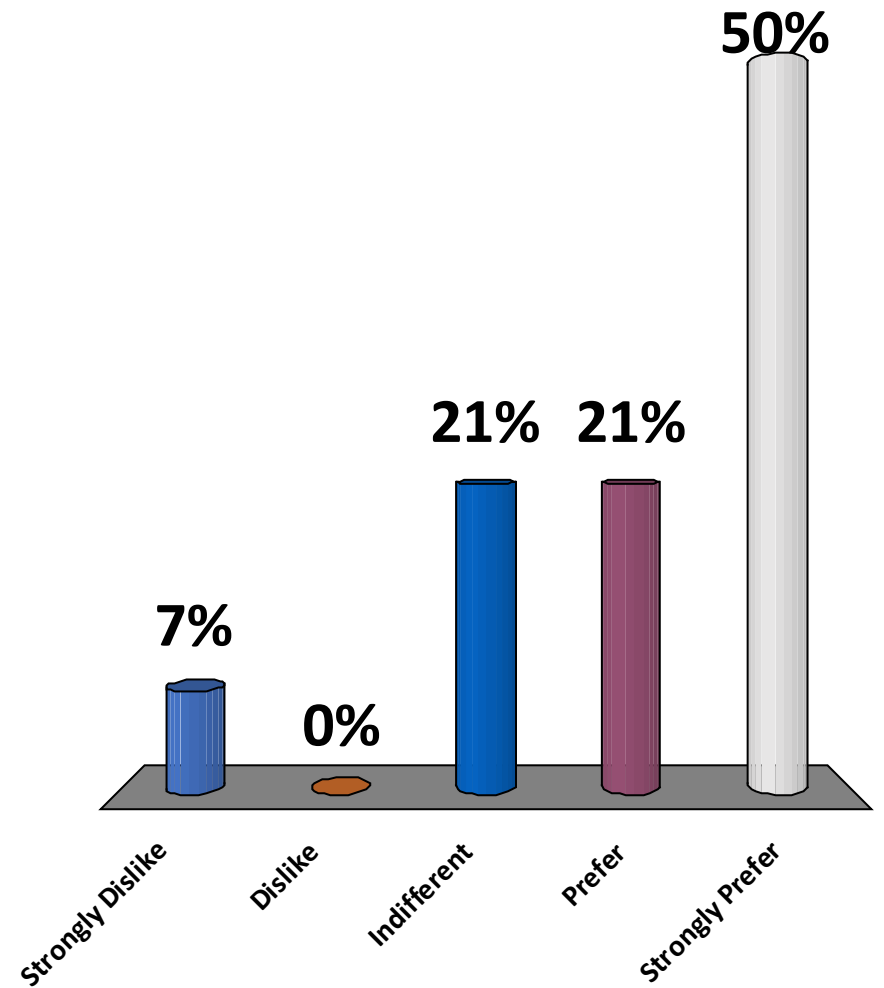


Furnishing Styles

Furnishing Styles | Traditional/Historic



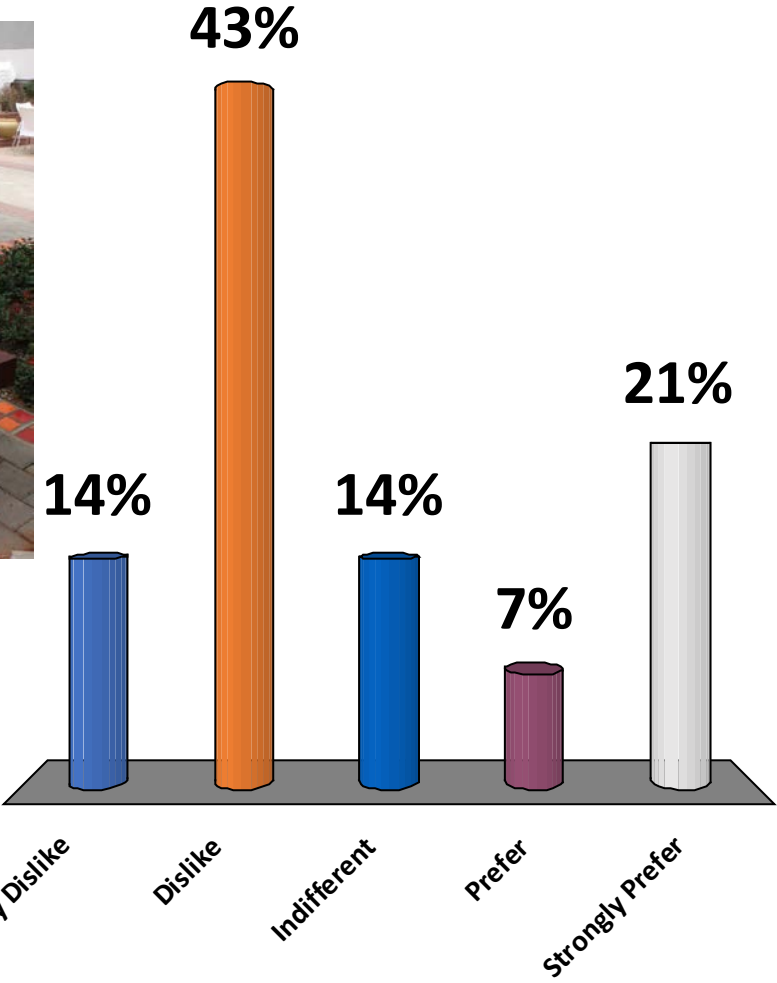
Mean = 4.07 Item 14.



Furnishing Styles | Contemporary

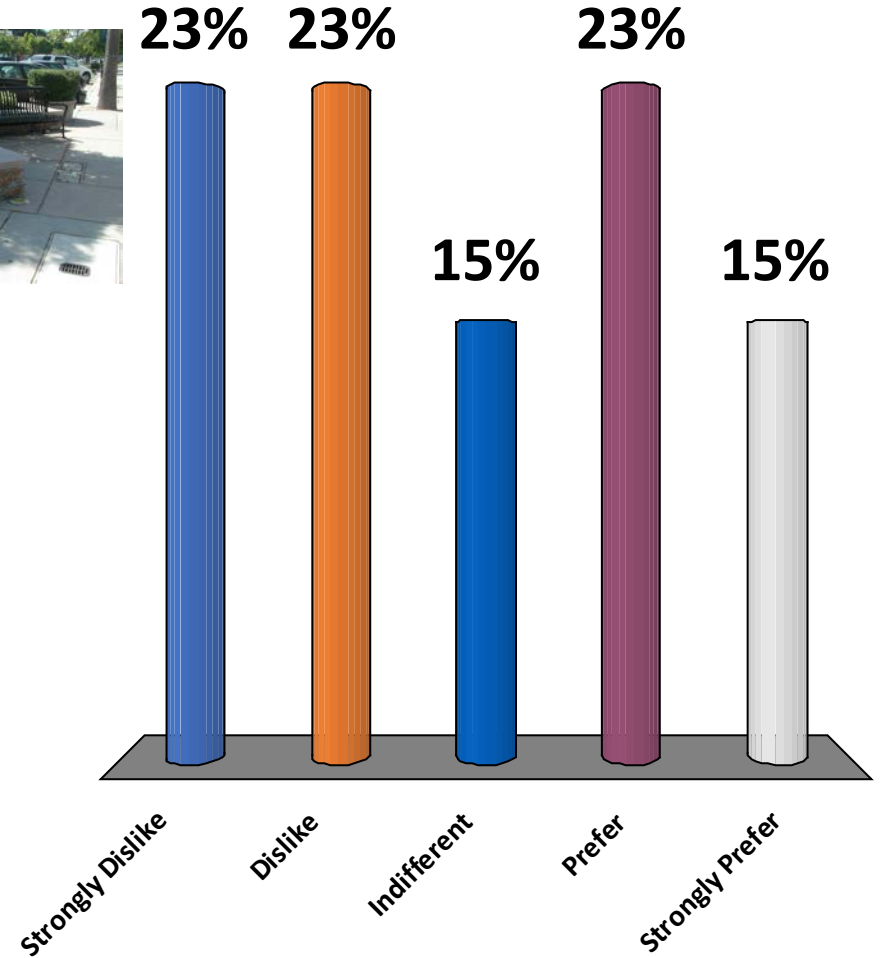
Item 14.

Mean = 2.79



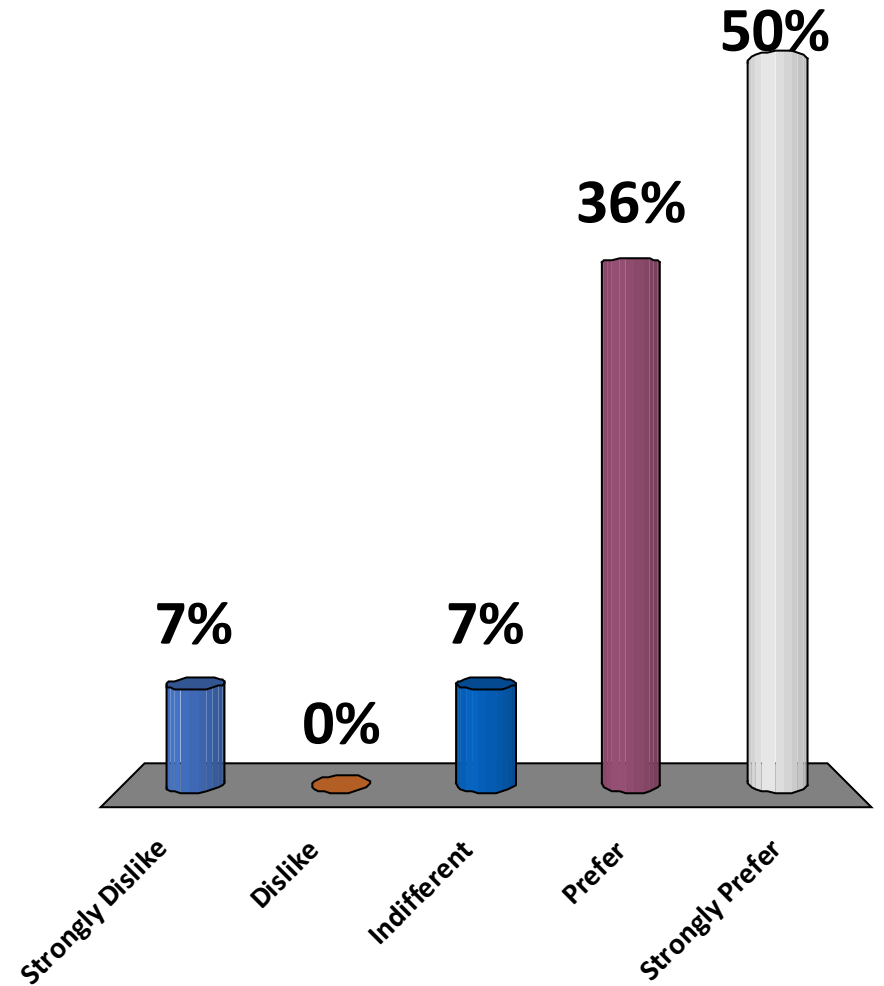
Furnishing Styles | Rustic

Mean = 2.85 Item 14.



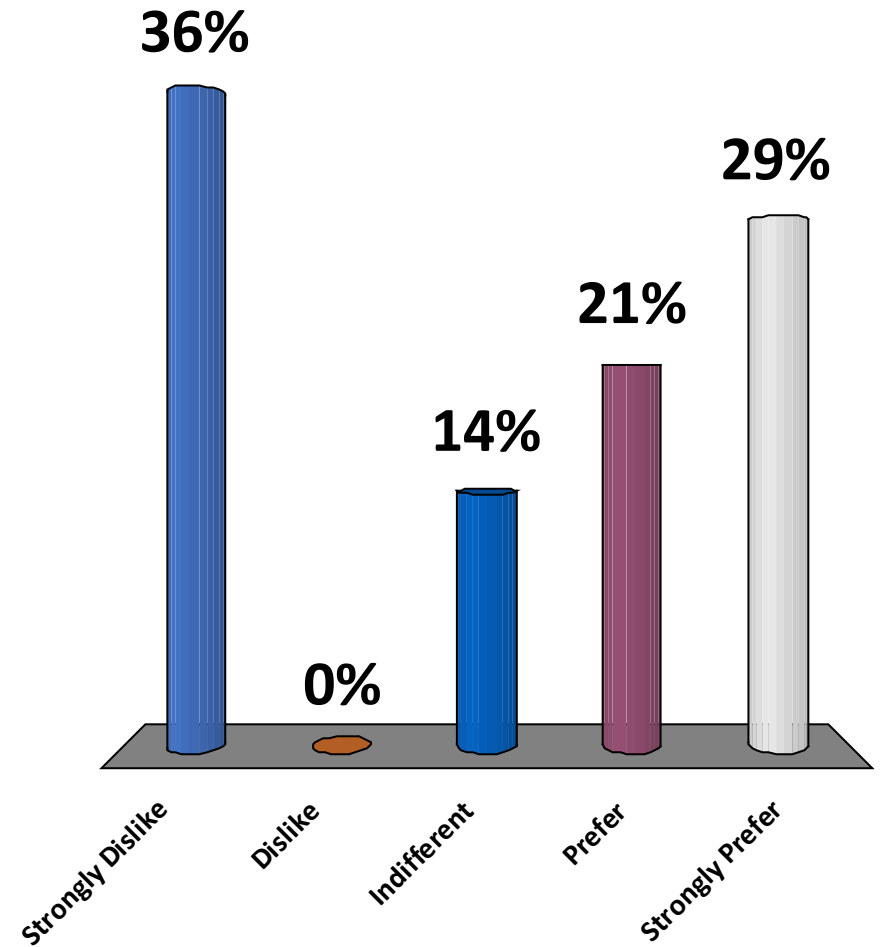
Furnishing Styles | Spanish Colonial

Mean = 4.21 Item 14.



Furnishing Styles | Custom

Mean = 3.07 Item 14.

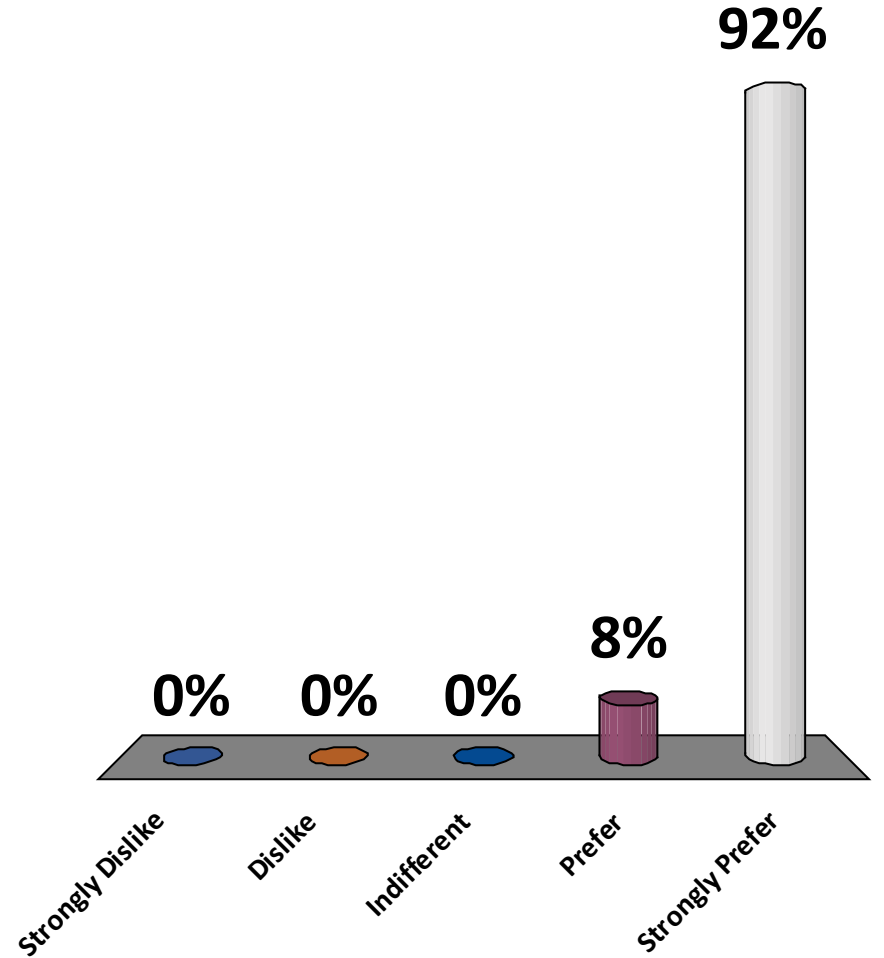


Shade Structures

Shade Structures | Covered Arcade

Mean = 4.92

Item 14.

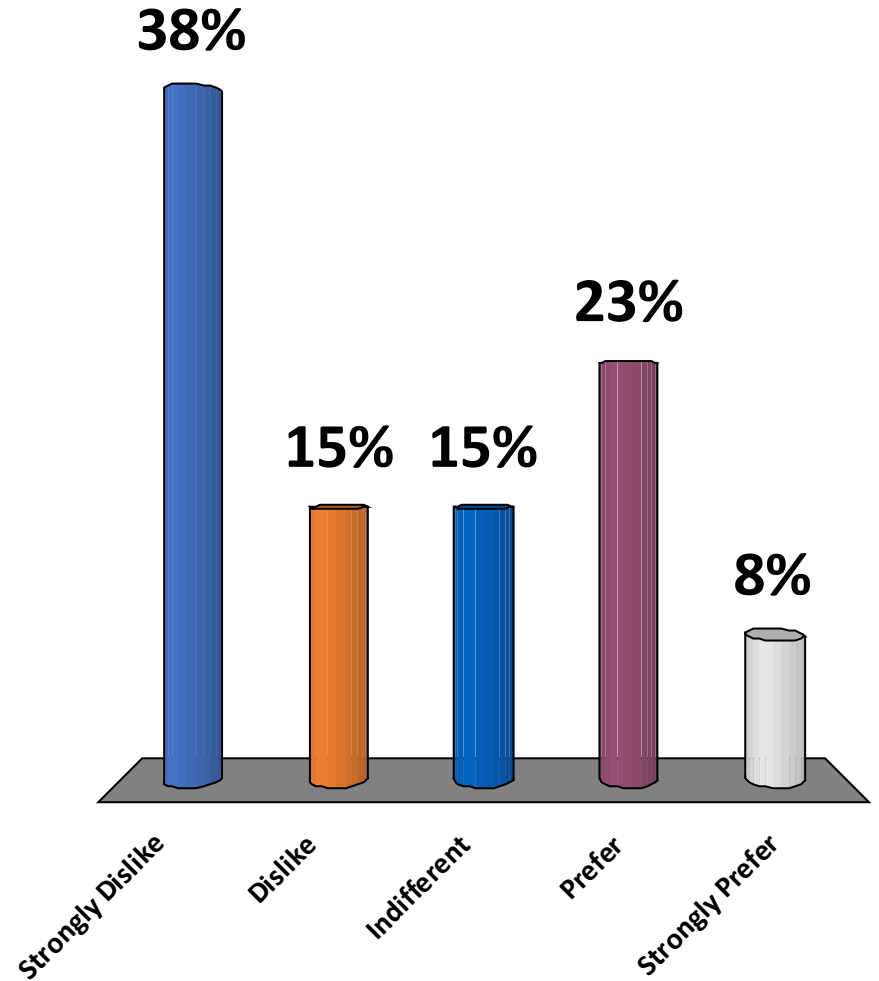


Shade Structures | Awnings and Canopies



Mean = 2.46

Item 14.



Shade Structures | Open Trellis

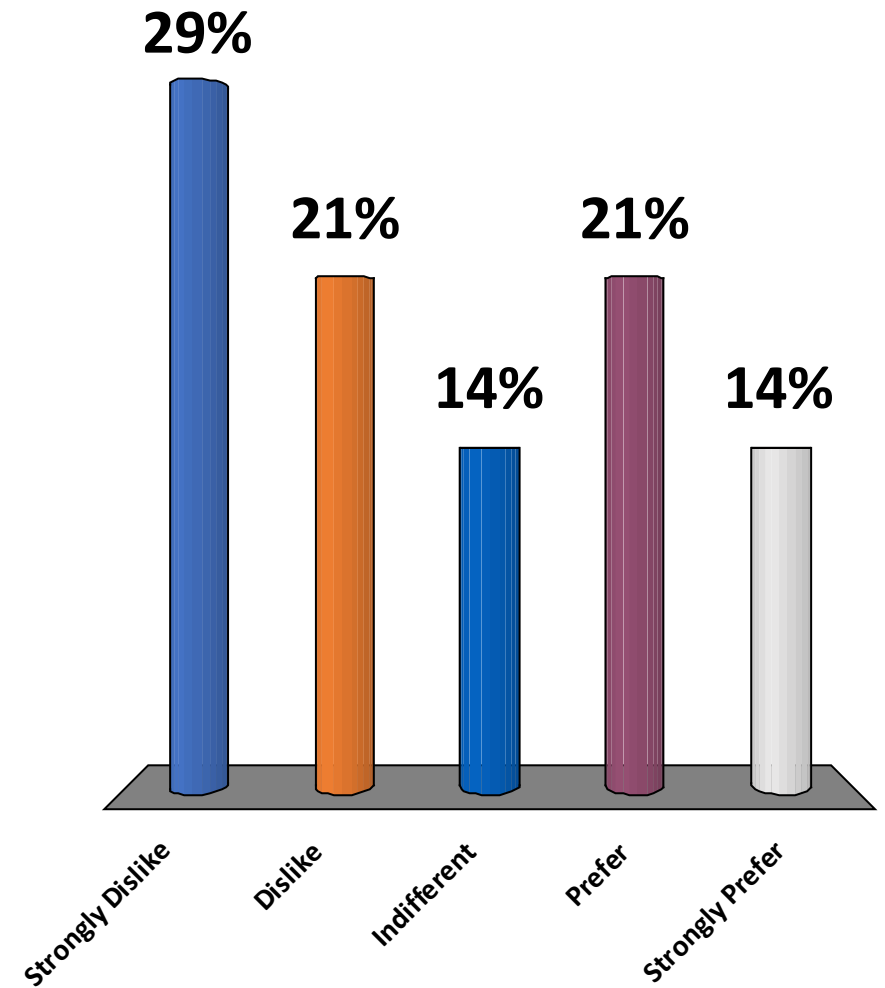


1 2 3 4 5

STRONGLY DISLIKE ← INDIFFERENT → STRONGLY PREFER



Mean = 2.71 Item 14.

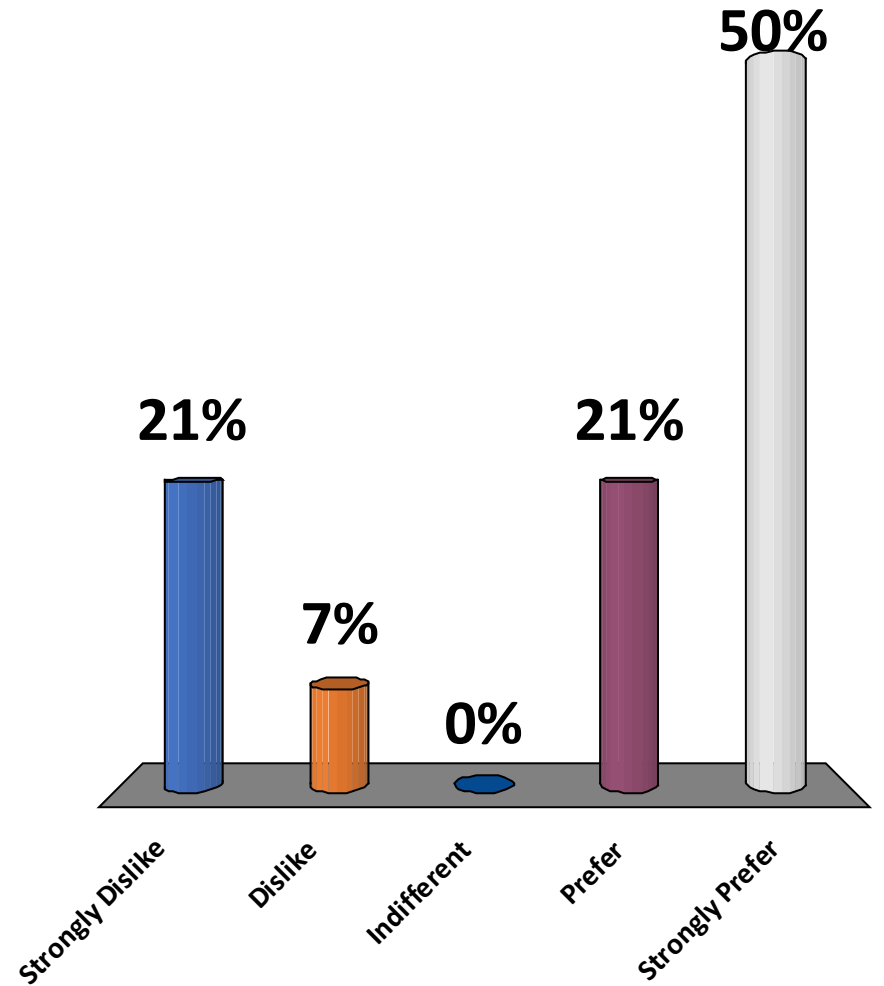


Public Art

Public Art | Wall Murals 1

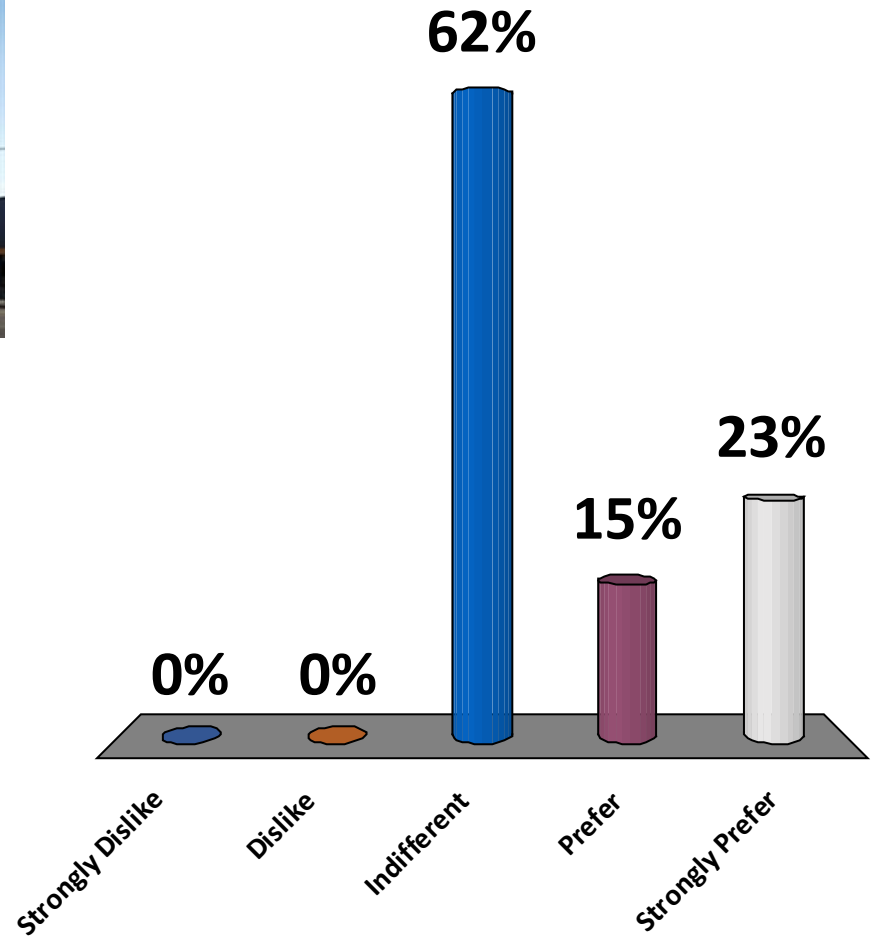


Mean = 3.71 Item 14.



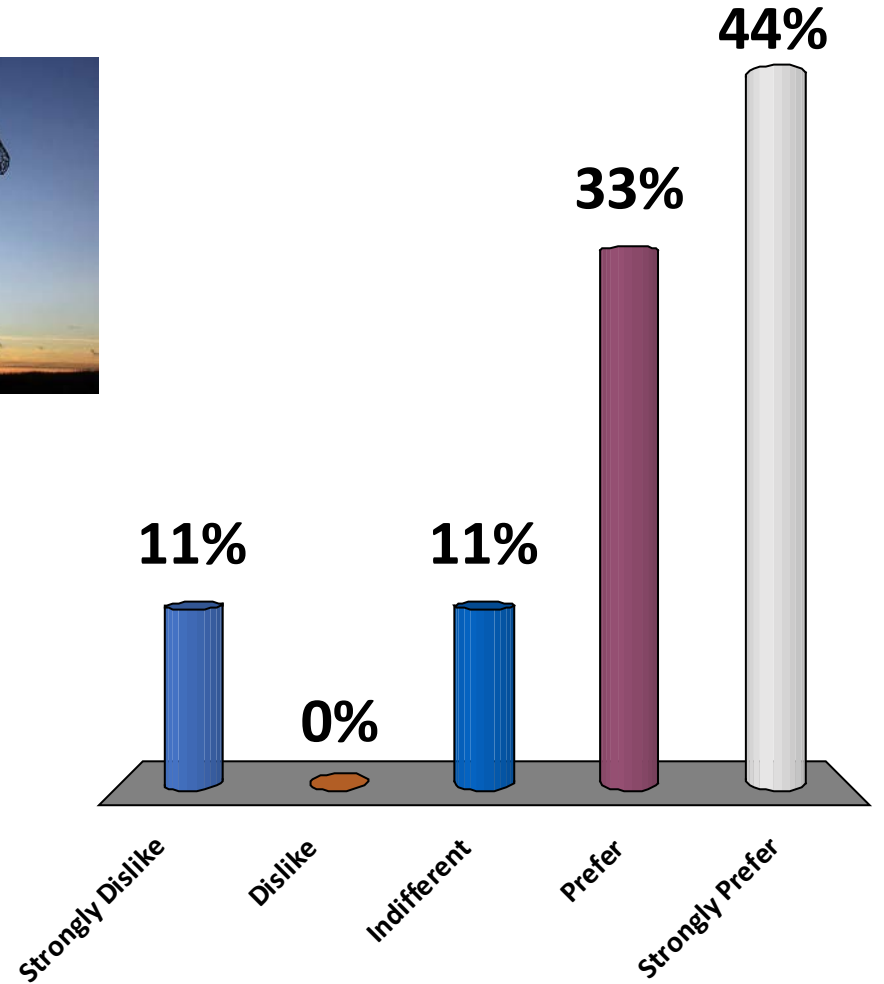
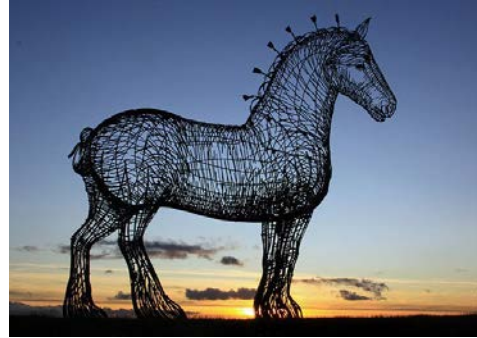
Public Art | Wall Murals 2

Mean = 3.62 Item 14.



Public Art | Sculpture

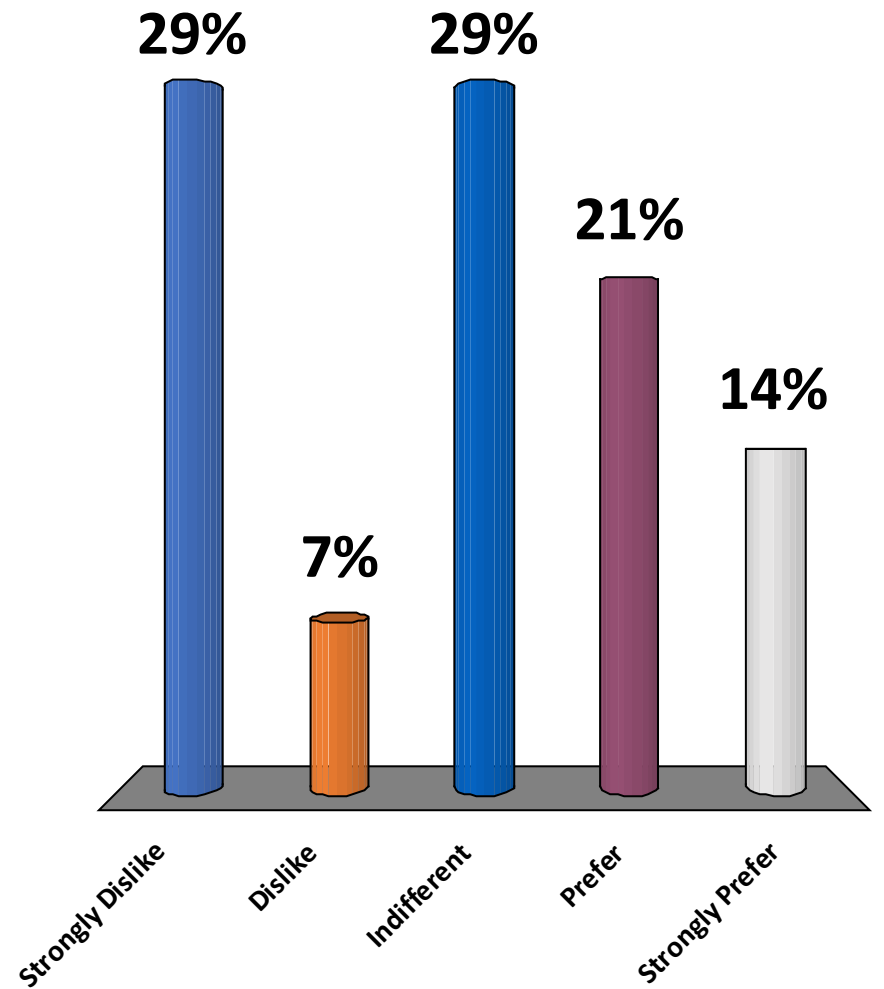
Mean = 4.00 Item 14.



Public Art | Kinetic/Interactive Sculpture



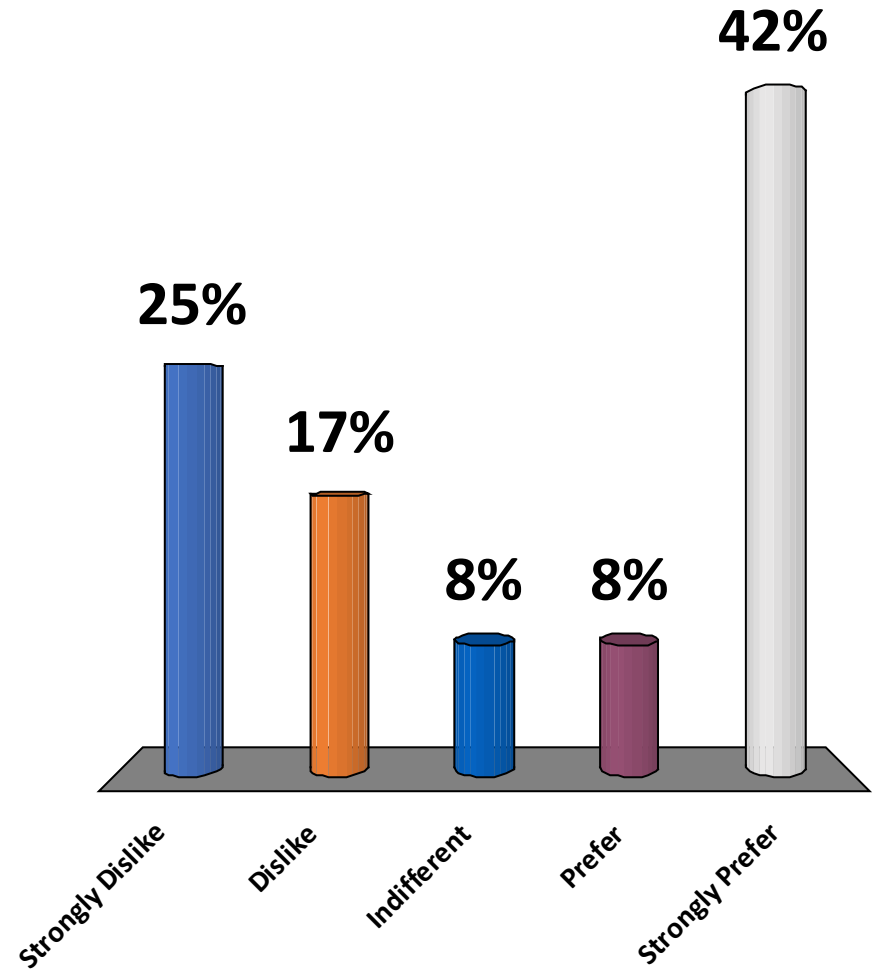
Mean = 2.86 Item 14.



Public Art | Pavement Details



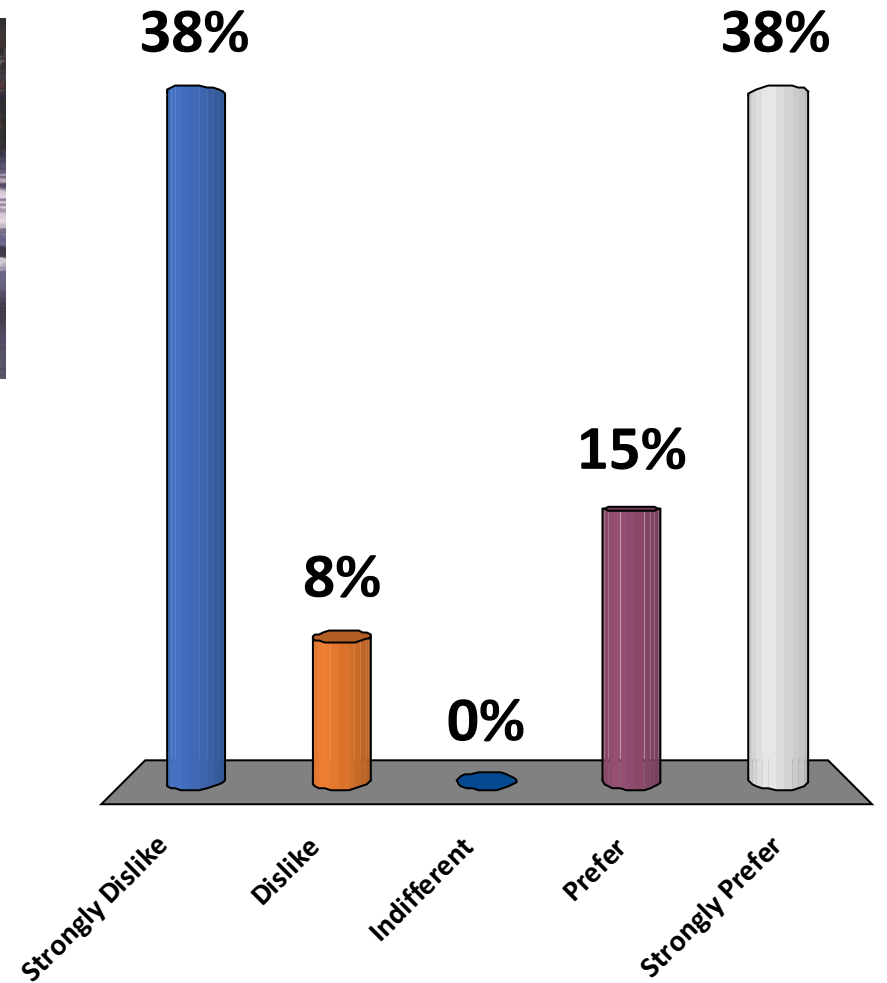
Mean = 3.25 Item 14.



Public Art | Seating

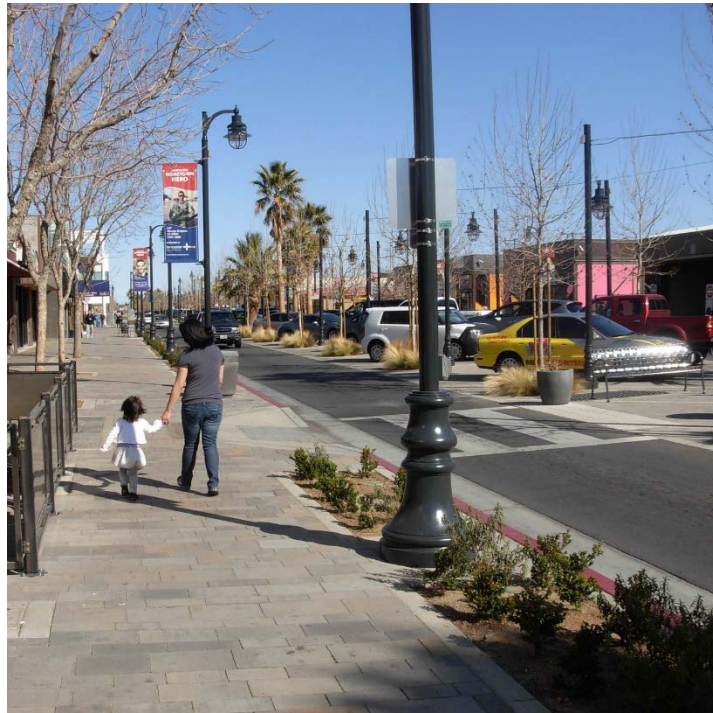


Mean = 3.08 Item 14.

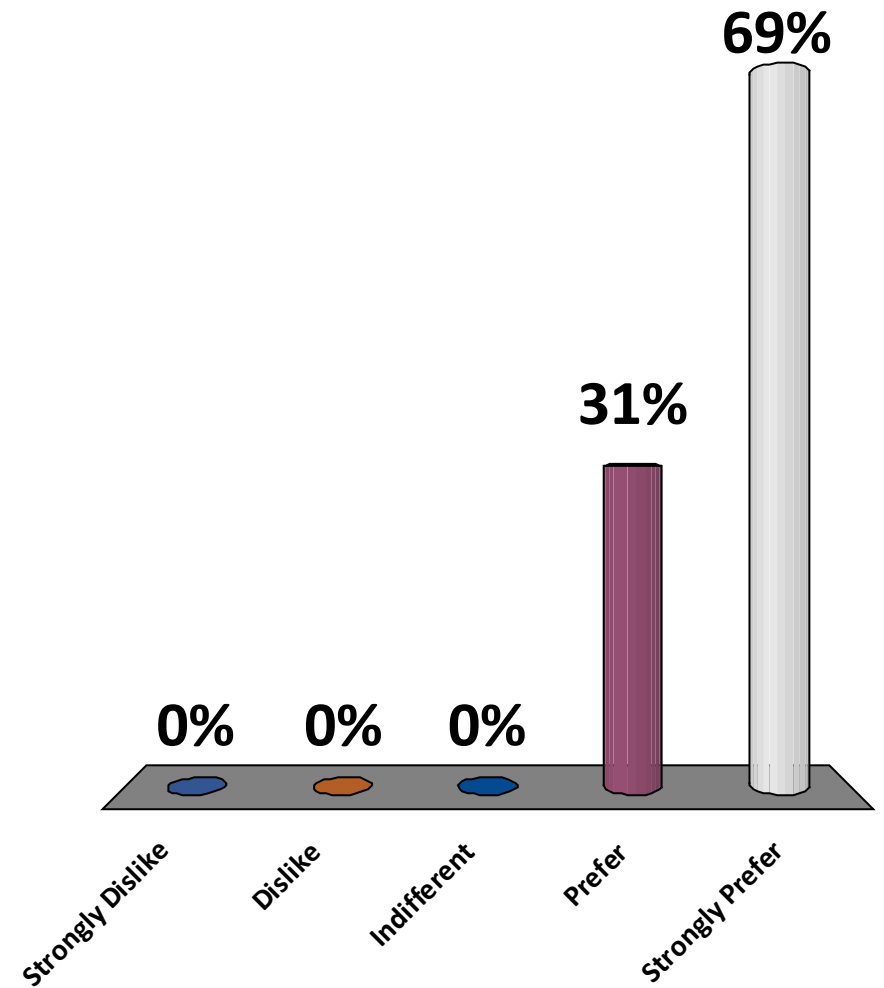


Pavement Types

Street Pavement | Pavers



Mean = 4.69 Item 14.

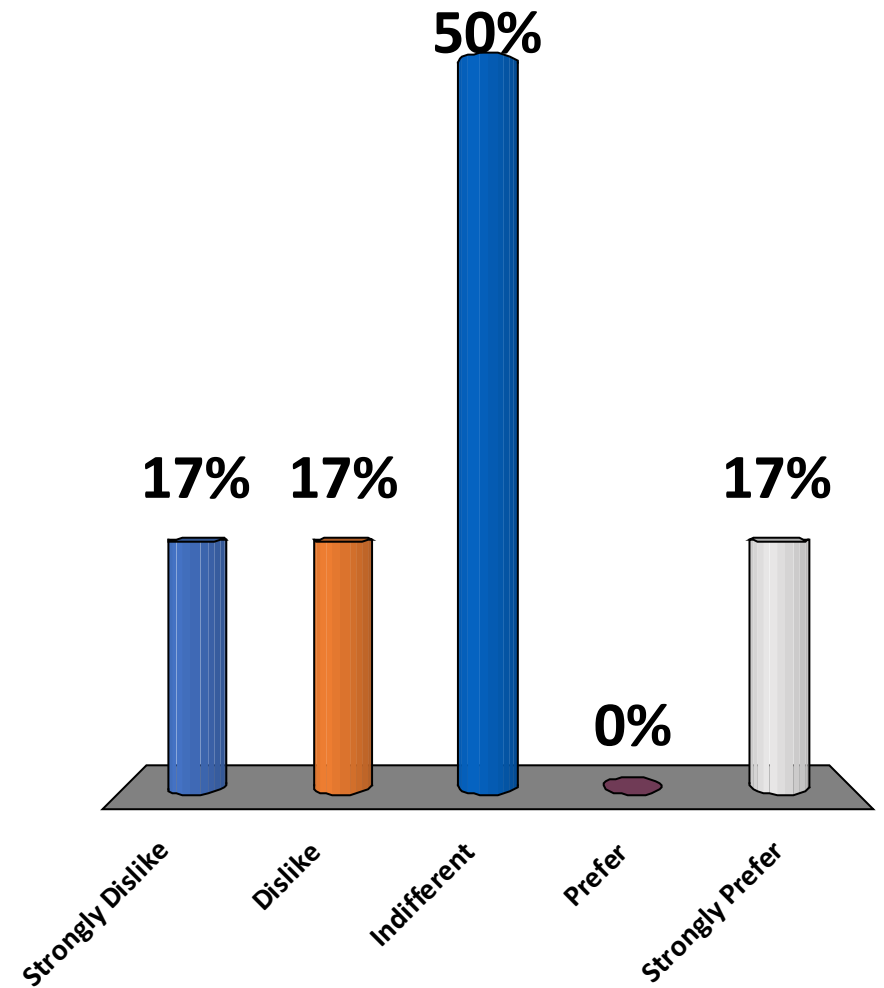


Street Pavement | Concrete



Mean = 2.83

Item 14.

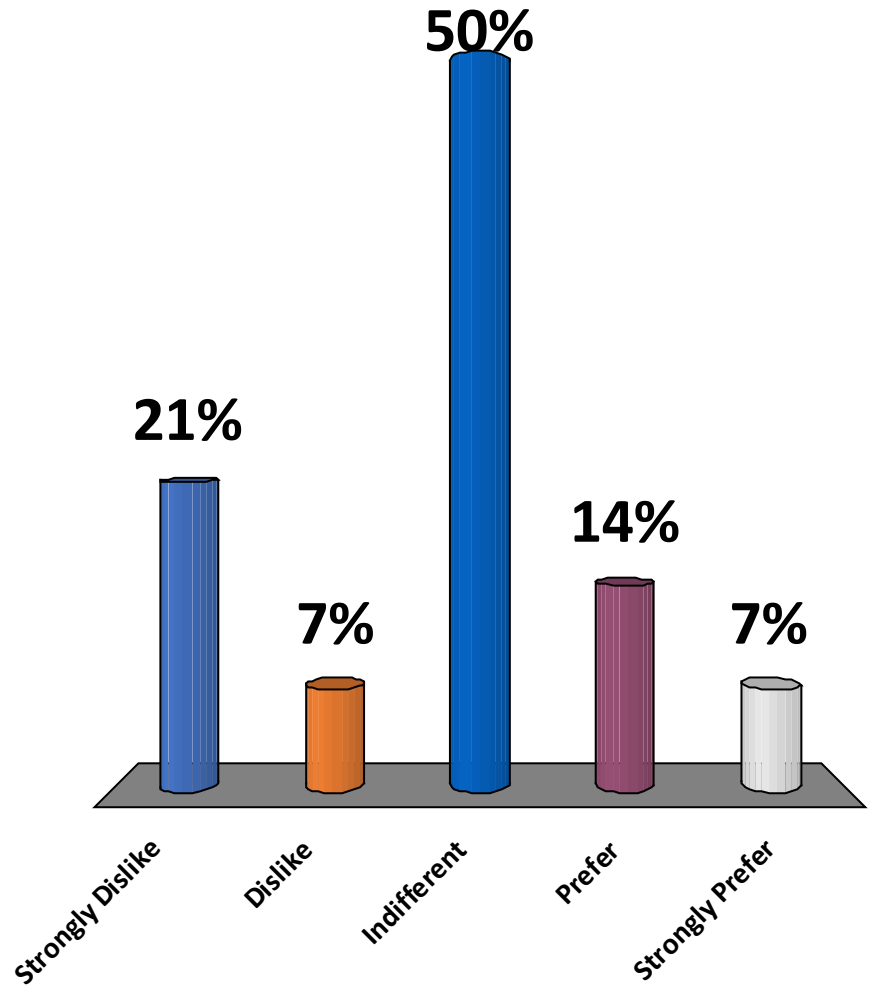


Street Pavement | Asphalt



Mean = 2.79

Item 14.

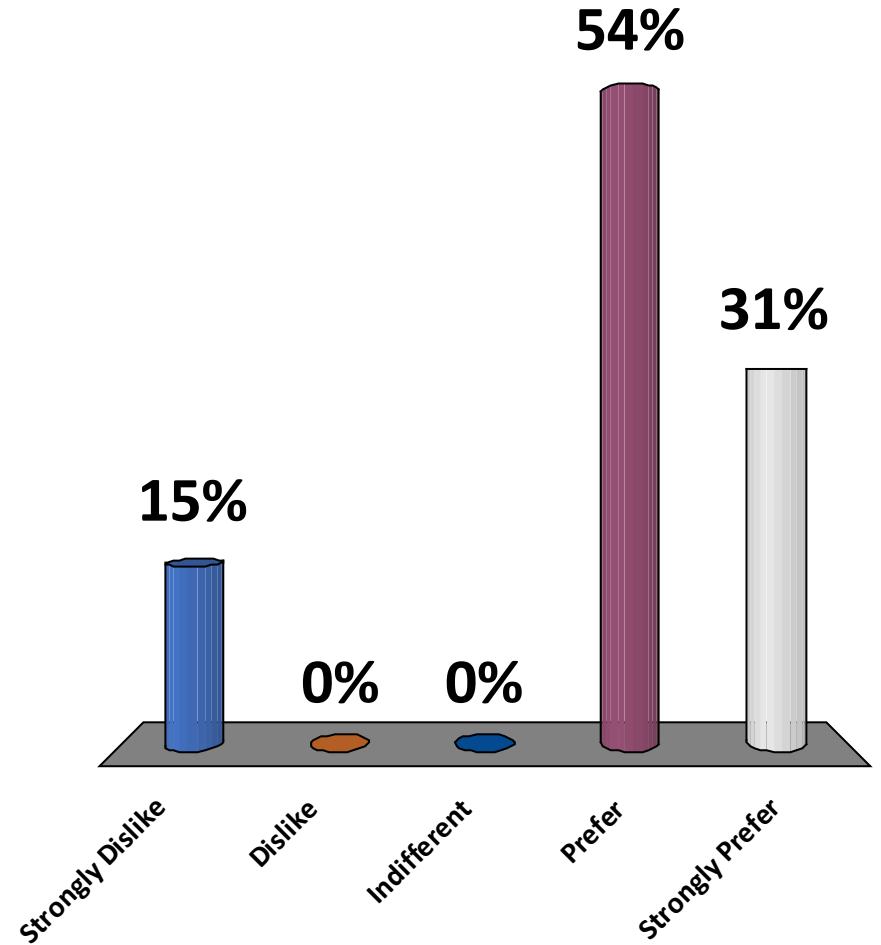


Street Pavement | Stamped Concrete



Mean = 3.85

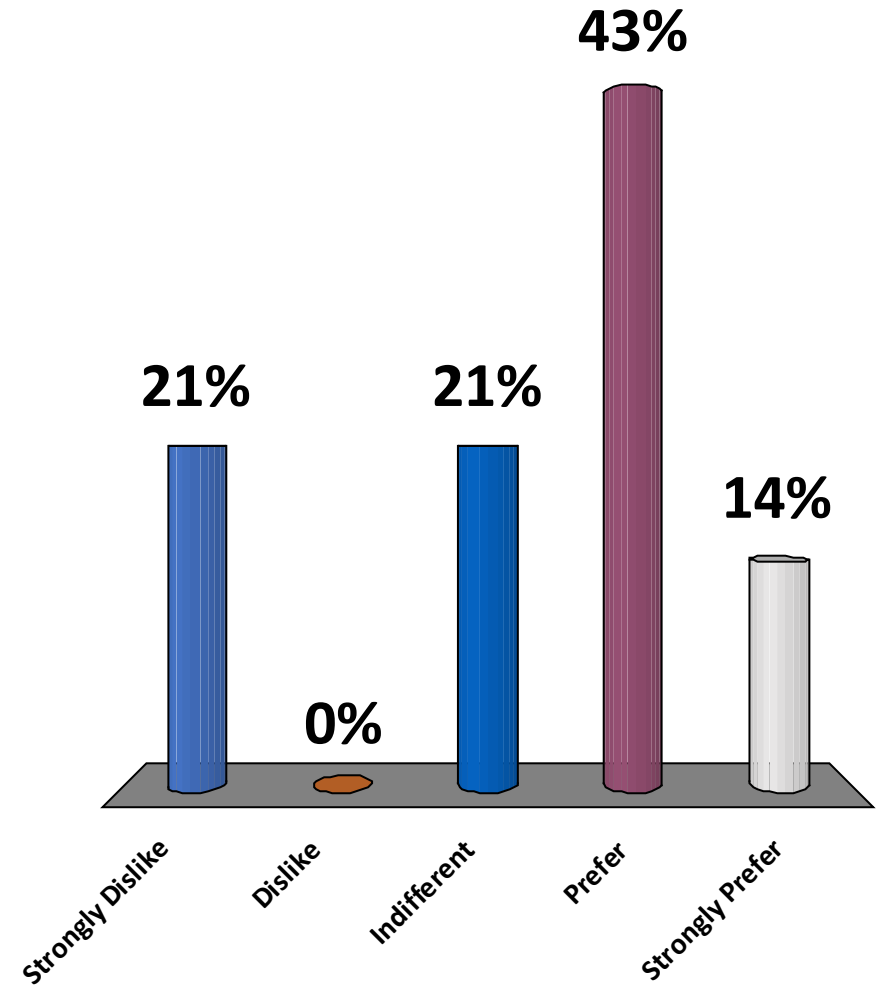
Item 14.



Street Pavement | Stamped Asphalt



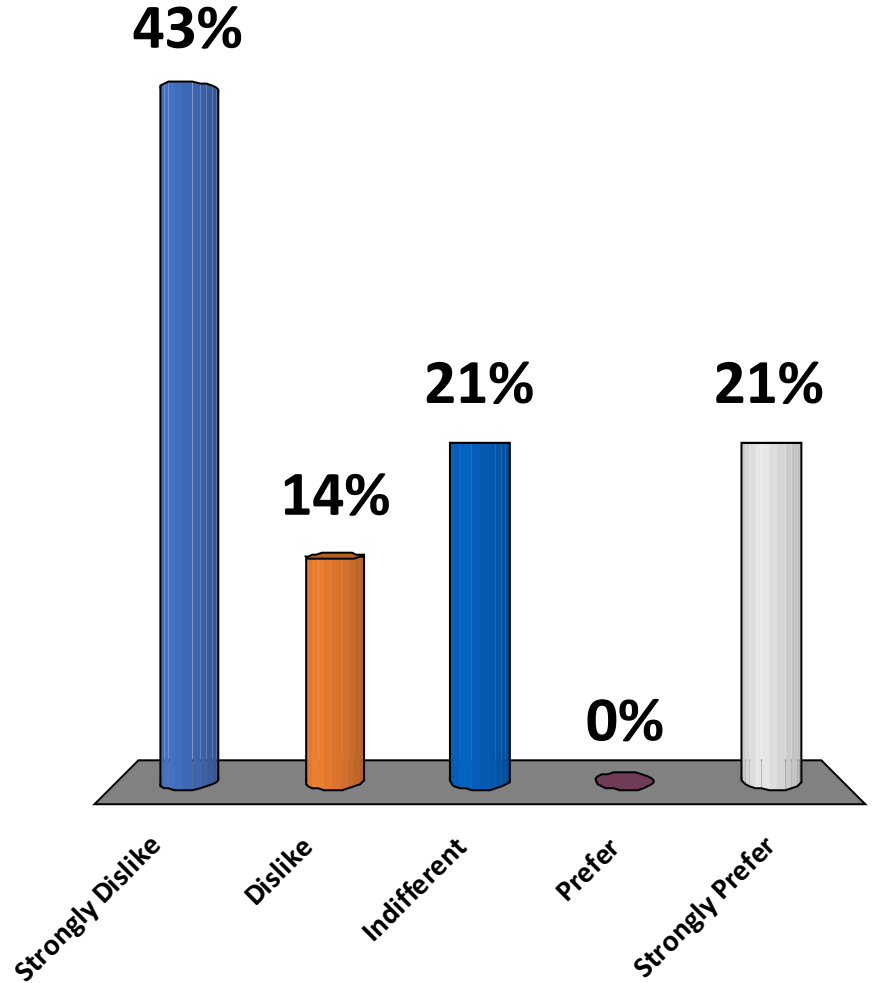
Mean = 3.29 Item 14.



Street Pavement | Street Print

Item 14.

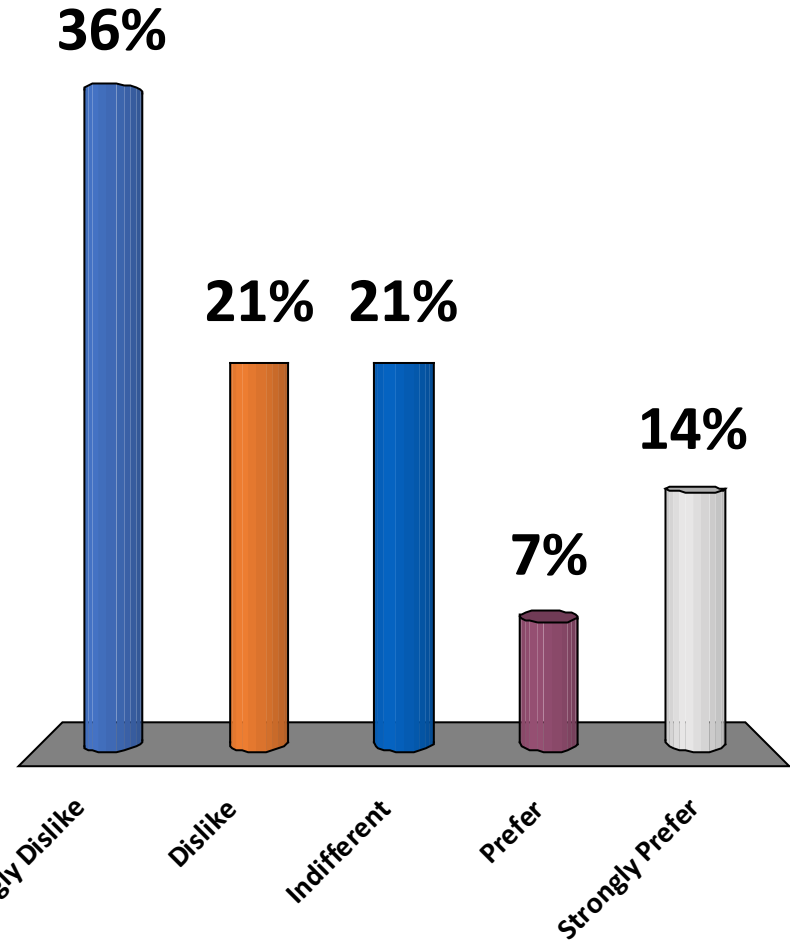
Mean = 2.43



Street Pavement | Painted/Mural

Mean = 2.43

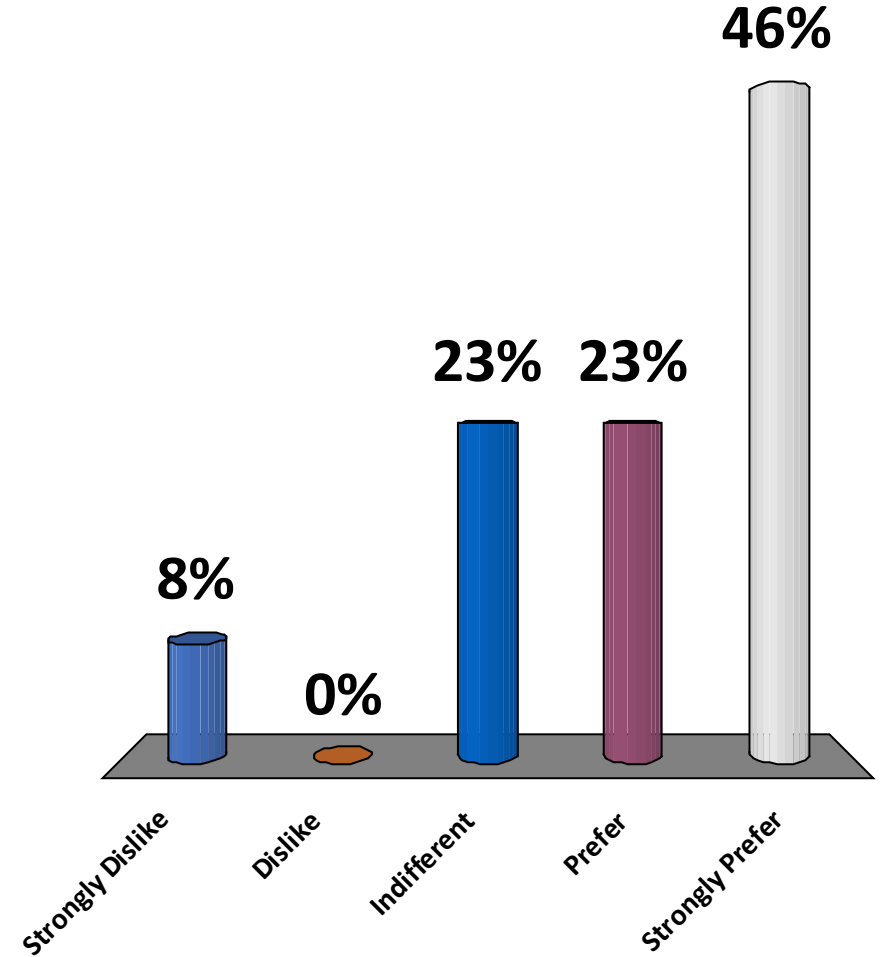
Item 14.



On-Street Parking

On-Street Parking | Parallel

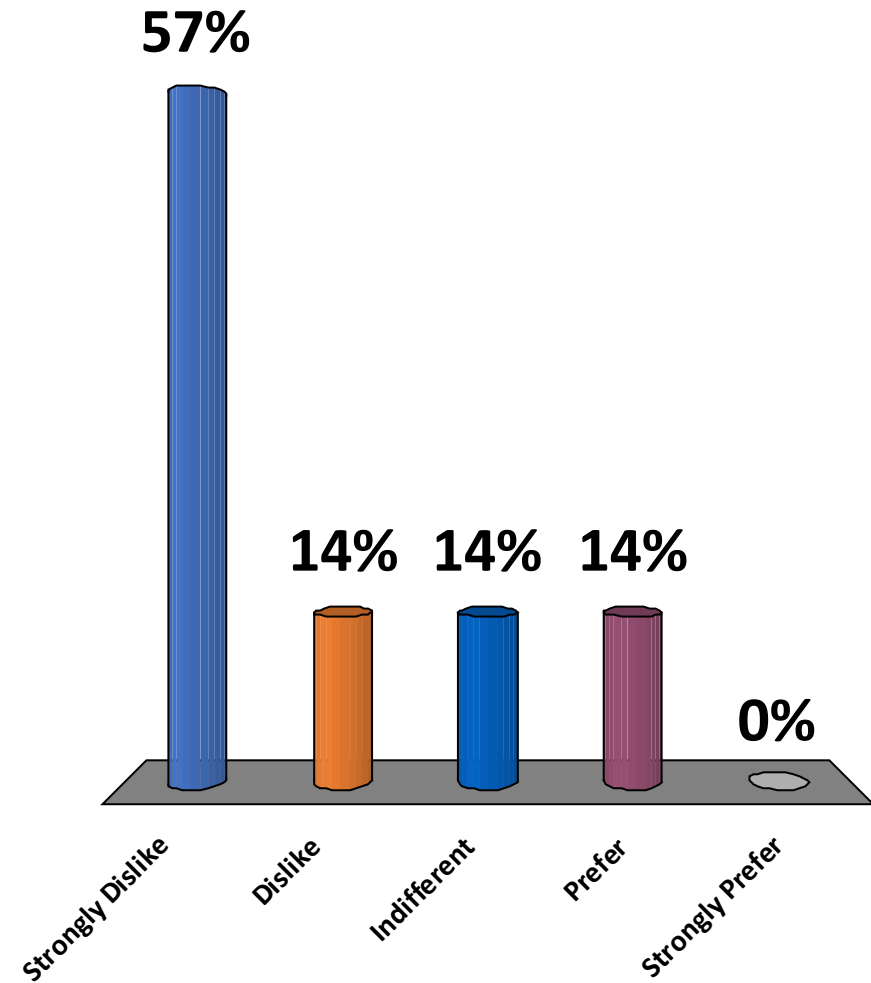
Mean = 4.00 Item 14.



On-Street Parking | Reverse/Back-In Angled

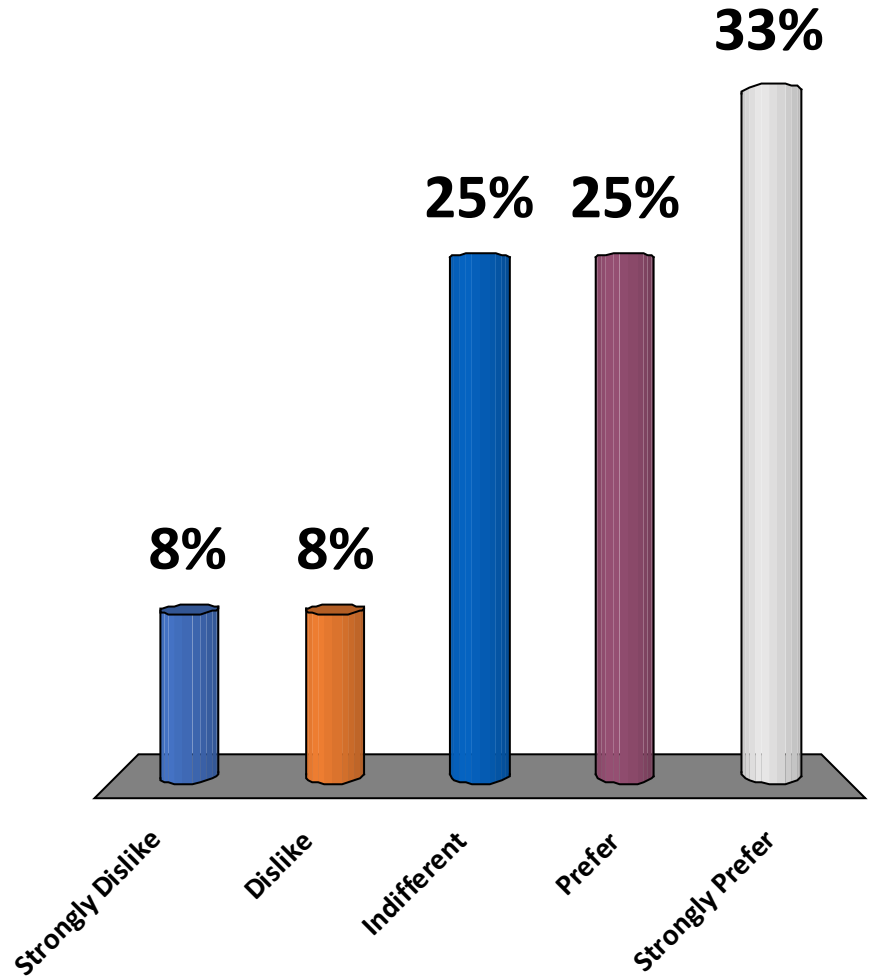


Mean = 1.86 Item 14.



On-Street Parking | Angled

Mean = 3.67 Item 14.

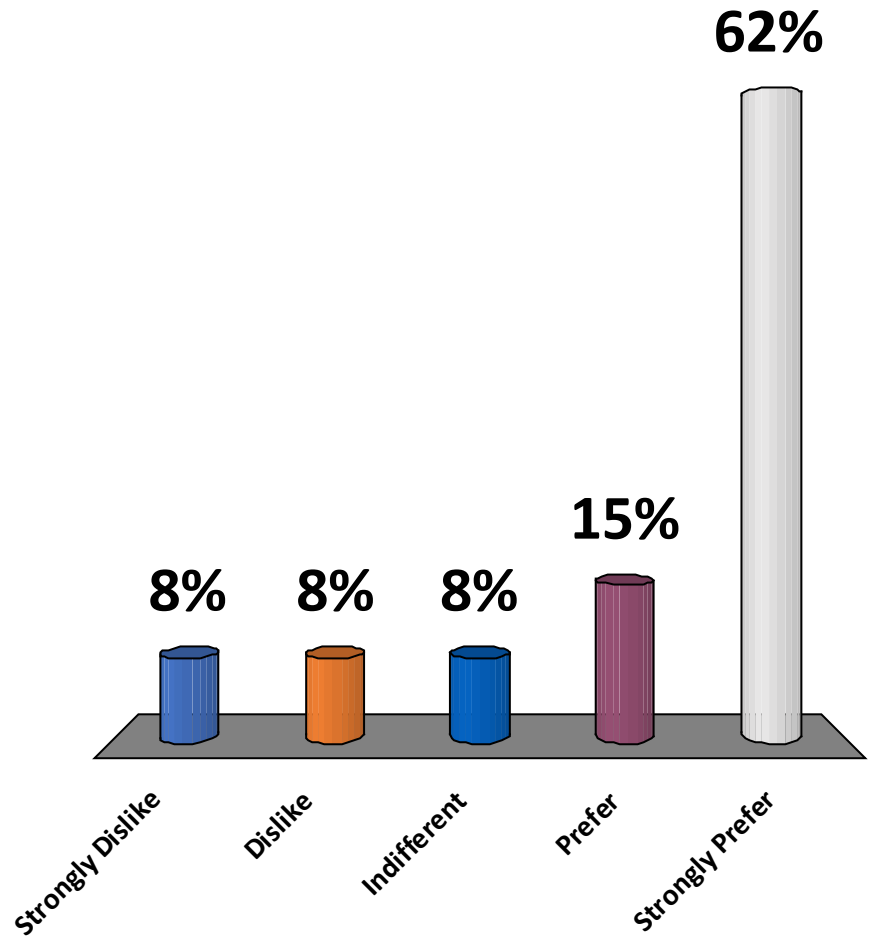


Light Poles

Light Poles & Bollards | 6th Street

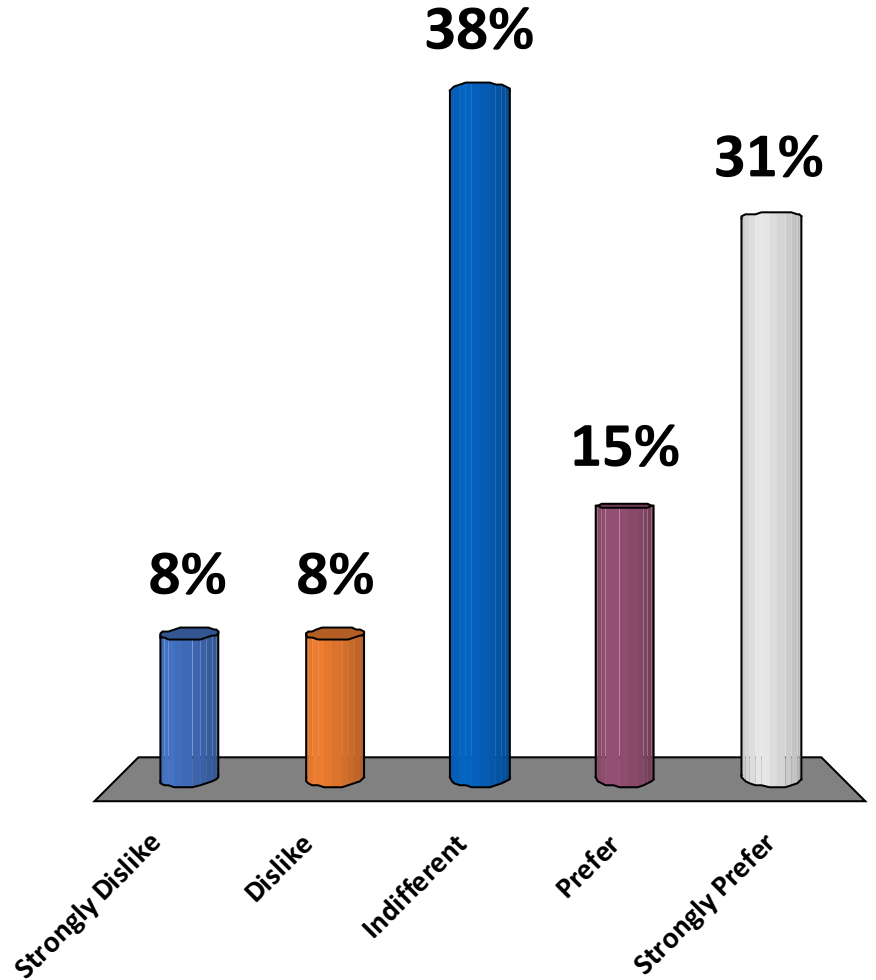
Mean = 4.15

Item 14.

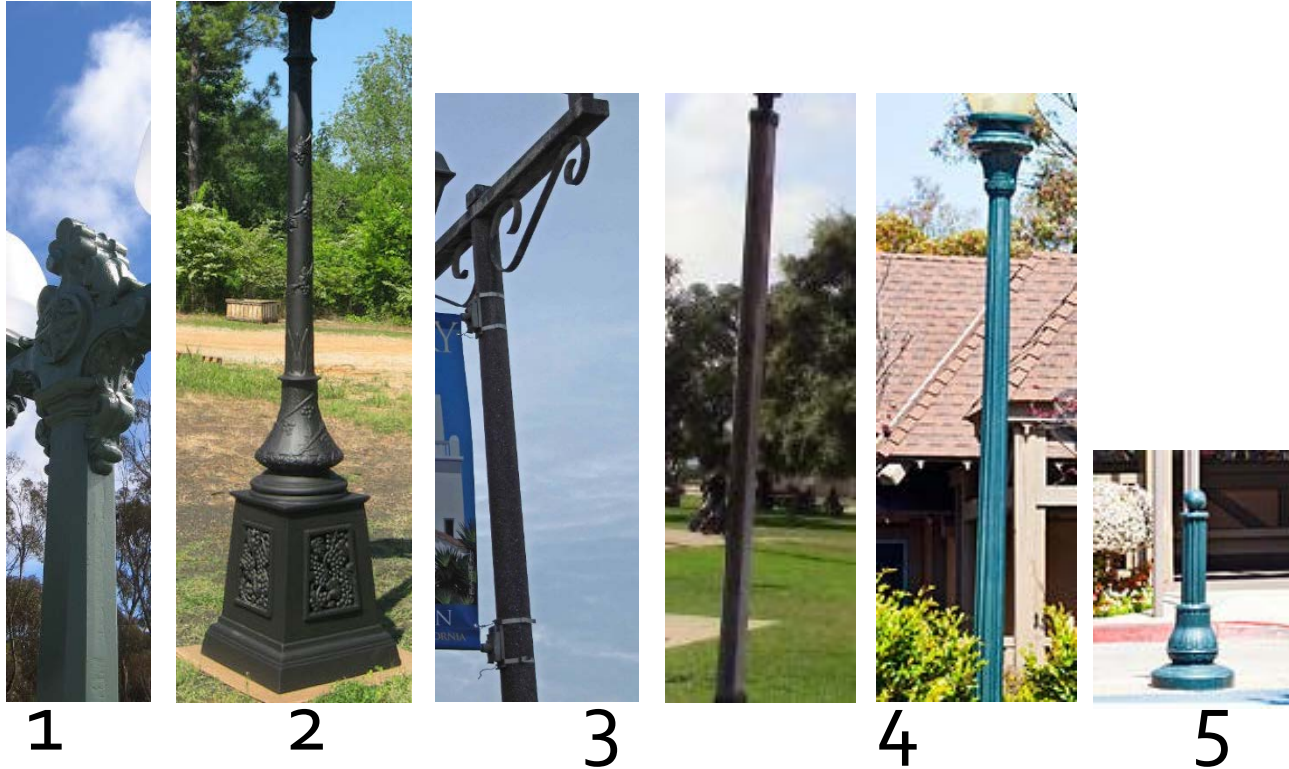


Light Poles & Bollards | Traditional

Mean = 3.54 Item 14.

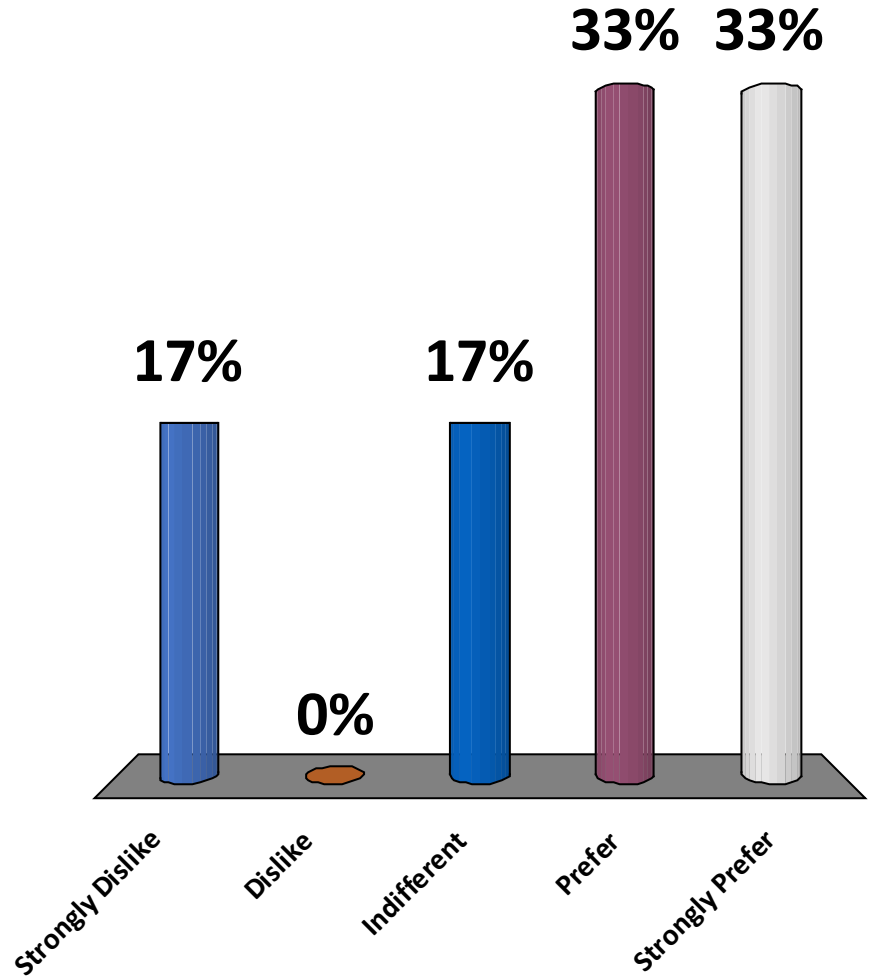


Light Poles & Bollards | Spanish Colonial



STRONGLY DISLIKE ← INDIFFERENT → STRONGLY PREFER

Mean = 3.67 Item 14.



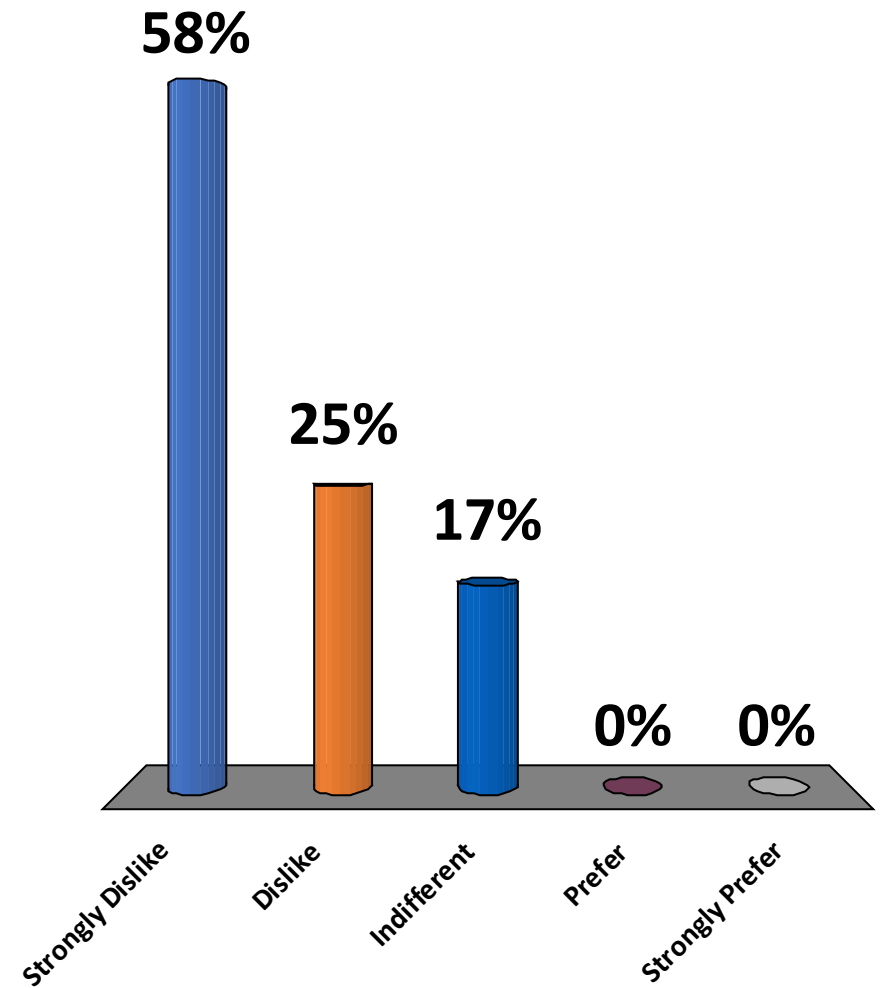
Light Poles & Bollards | Rustic



1 2 3 4 5

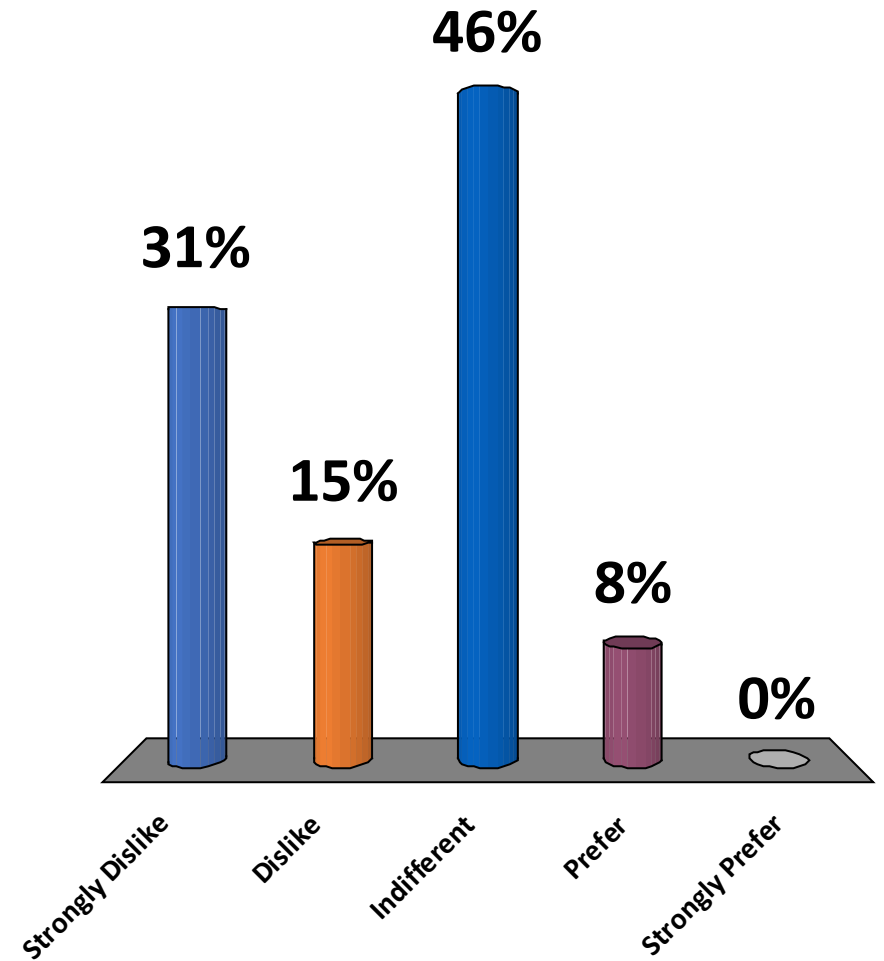
STRONGLY DISLIKE ← INDIFFERENT → STRONGLY PREFER

Mean = 1.58 Item 14.



Light Poles & Bollards | Contemporary

Mean = 2.31 Item 14.



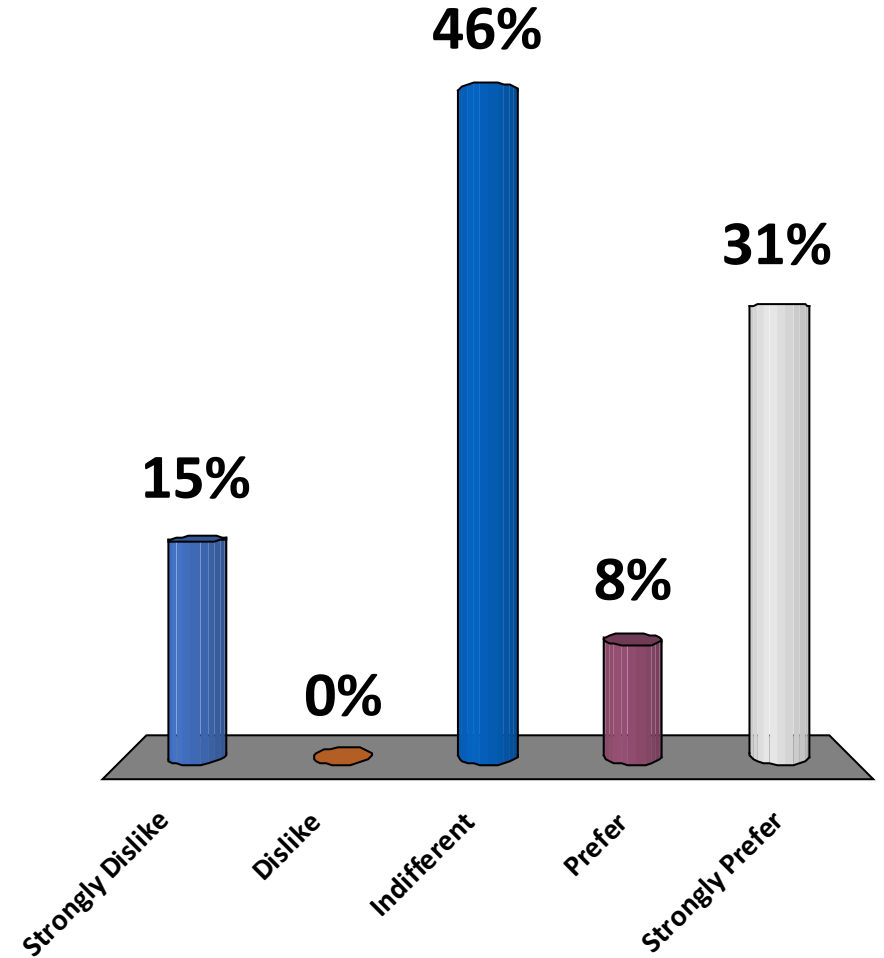
Luminaires

Luminaires | Acorn



Mean = 3.38

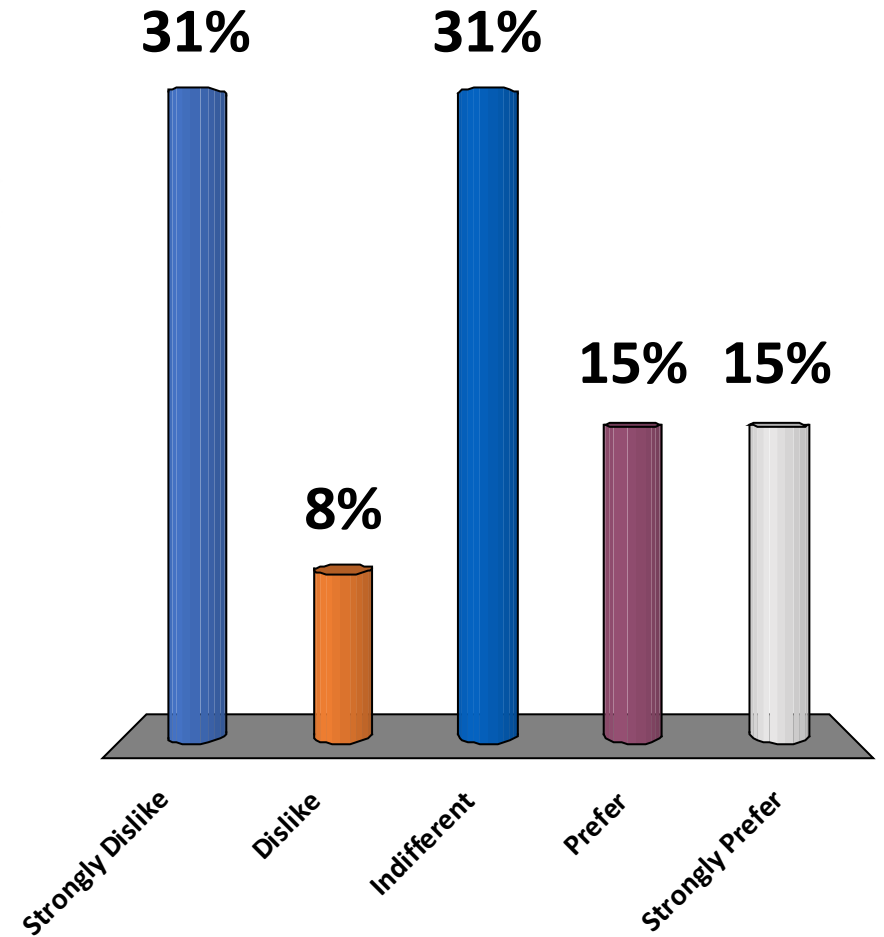
Item 14.



Luminaires | Globe



Mean = 2.77 Item 14.

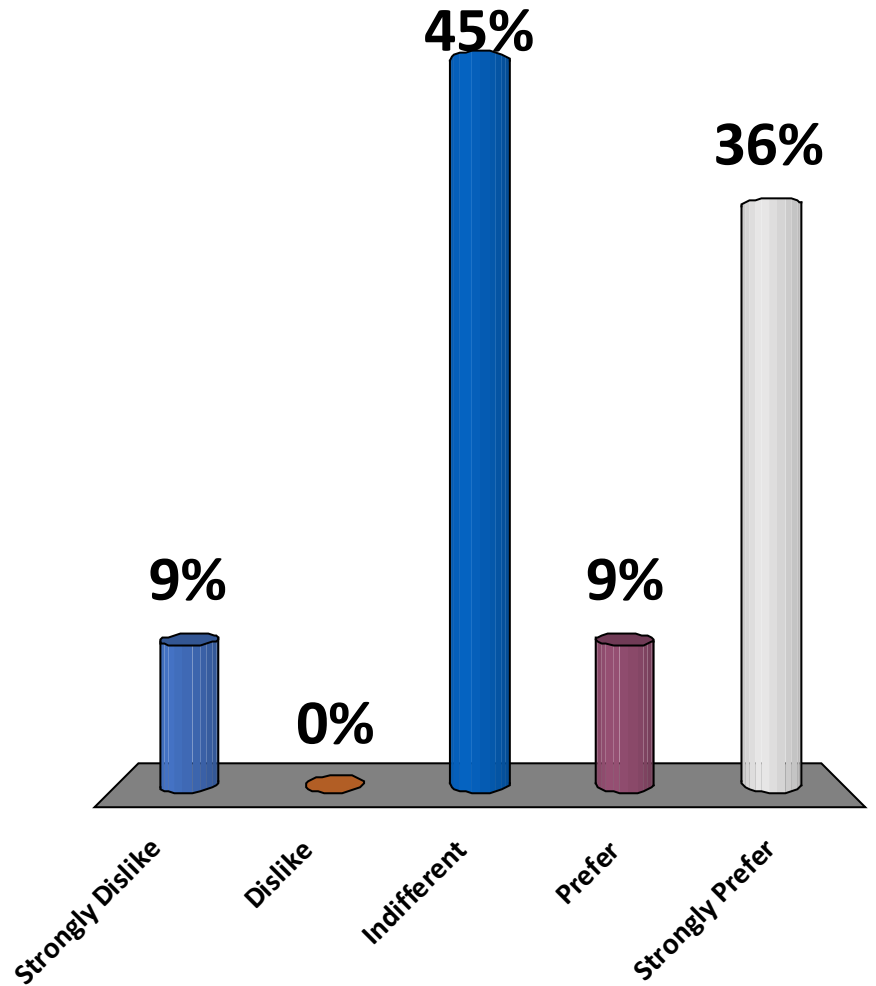


Luminaires | Lantern



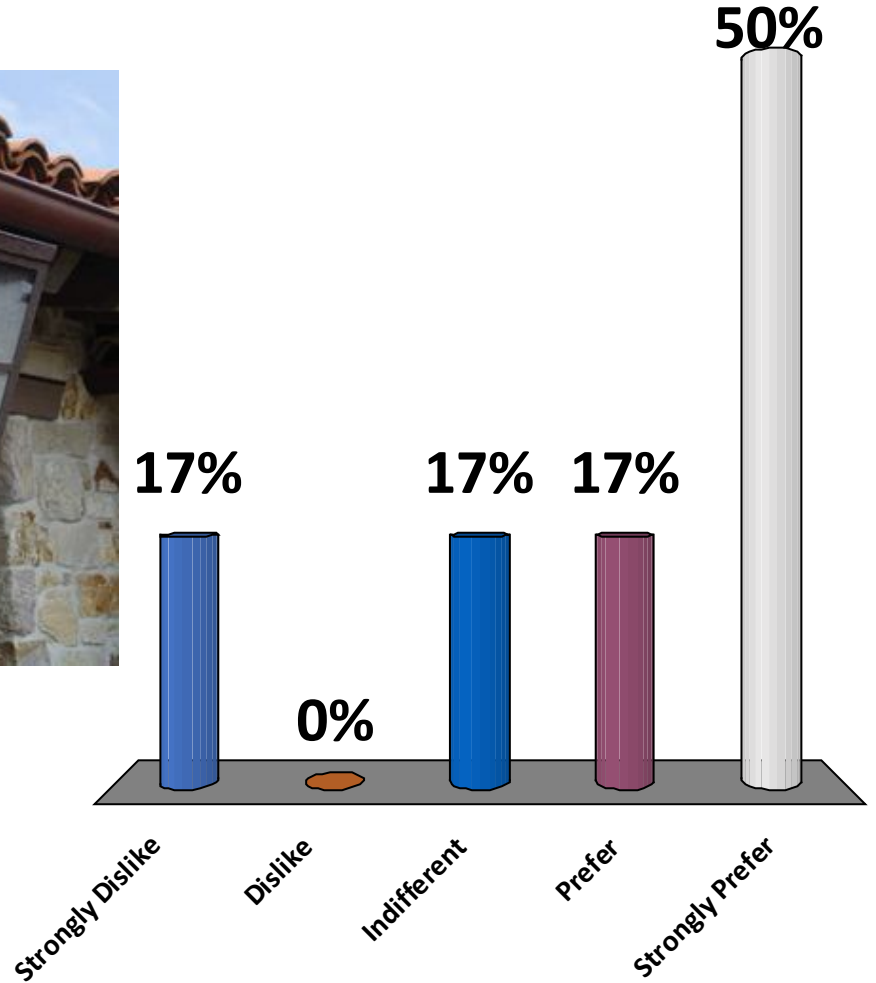
Mean = 3.64

Item 14.



Luminaires | Spanish Colonial

Mean = 3.83 Item 14.

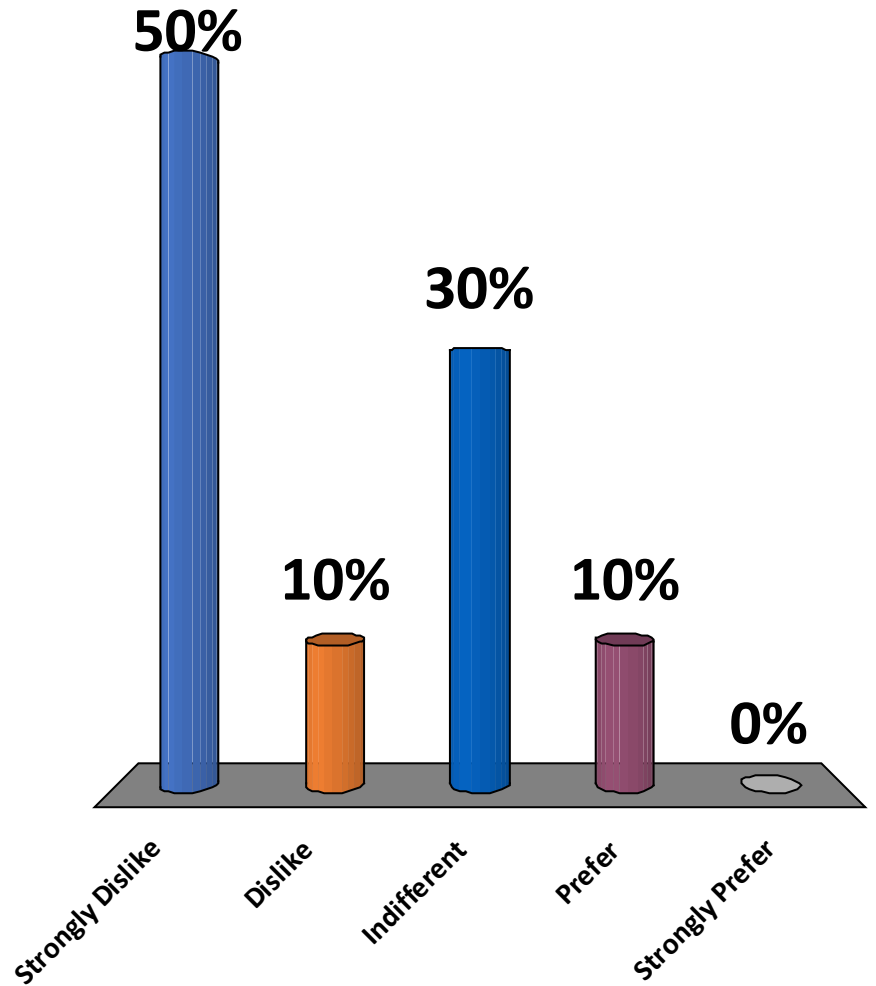


Luminaires | Contemporary



Mean = 2.00

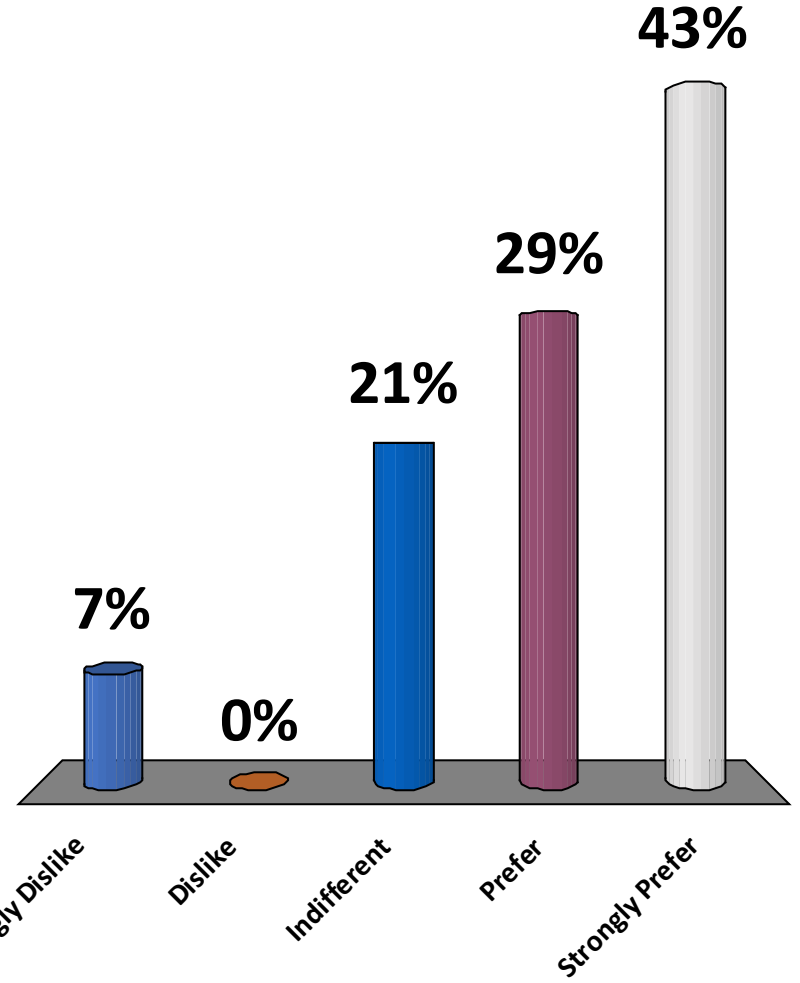
Item 14.



Traffic Calming

Traffic Calming | Bulb Outs

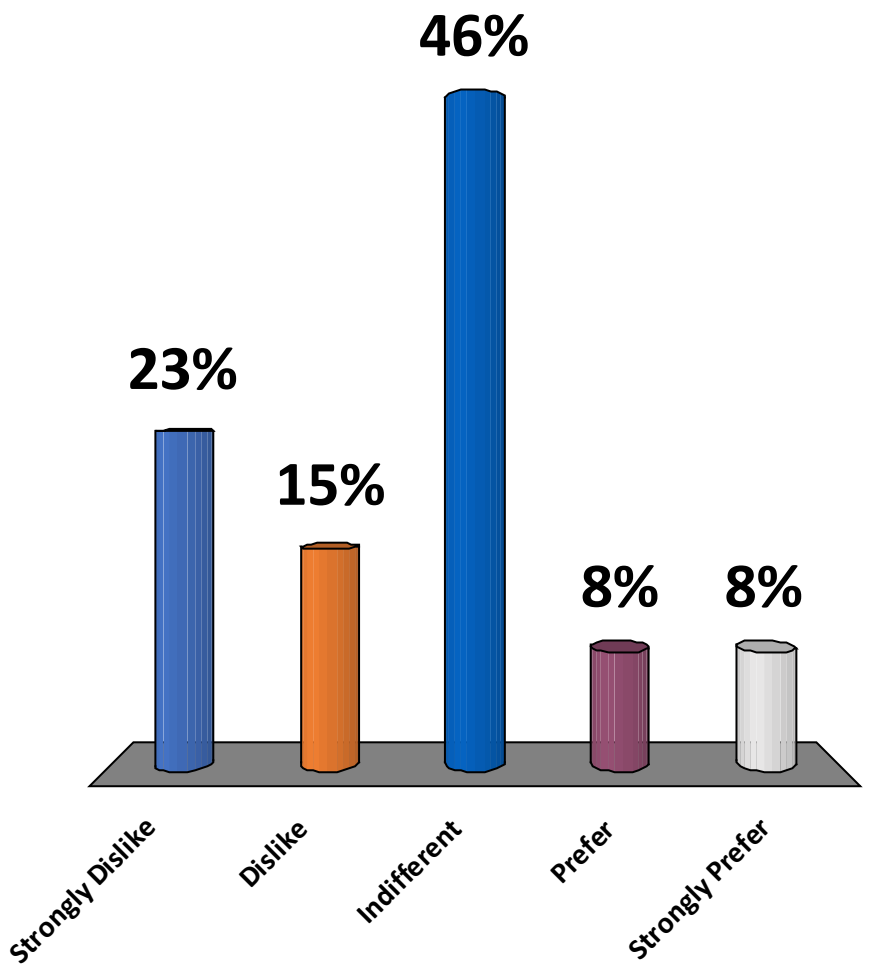
Mean = 4.00 Item 14.



Traffic Calming | Neck Downs

Mean = 2.62

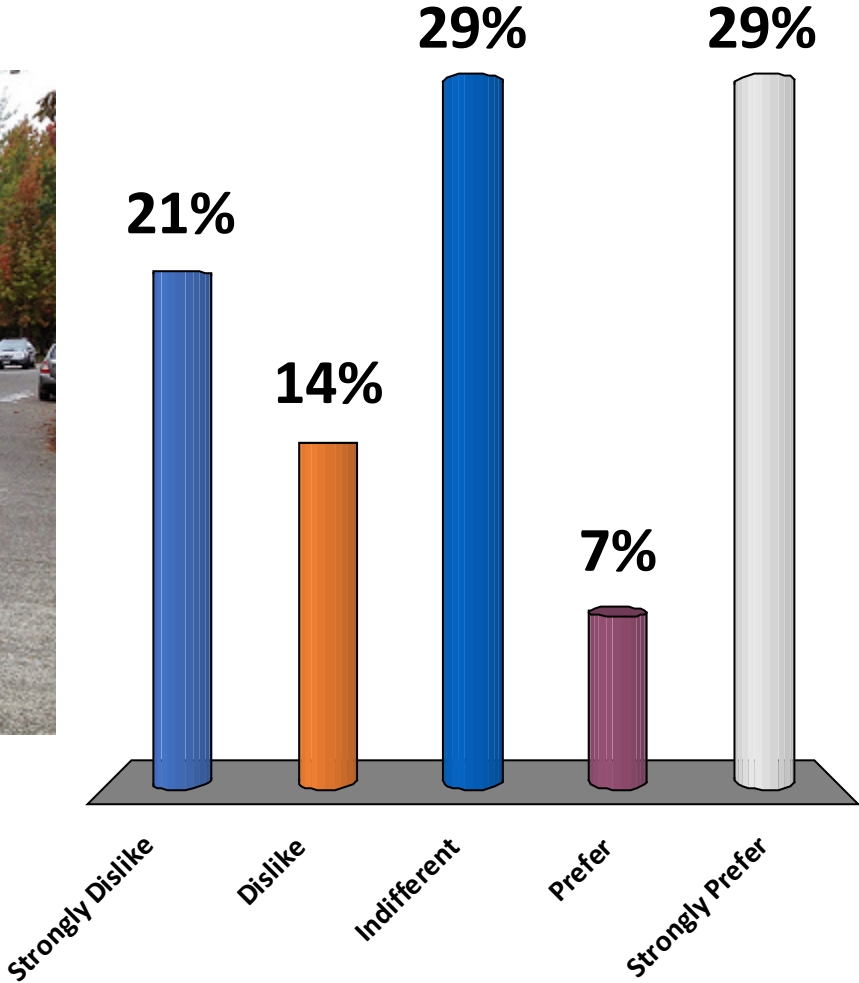
Item 14.



Traffic Calming | Mini-Roundabouts



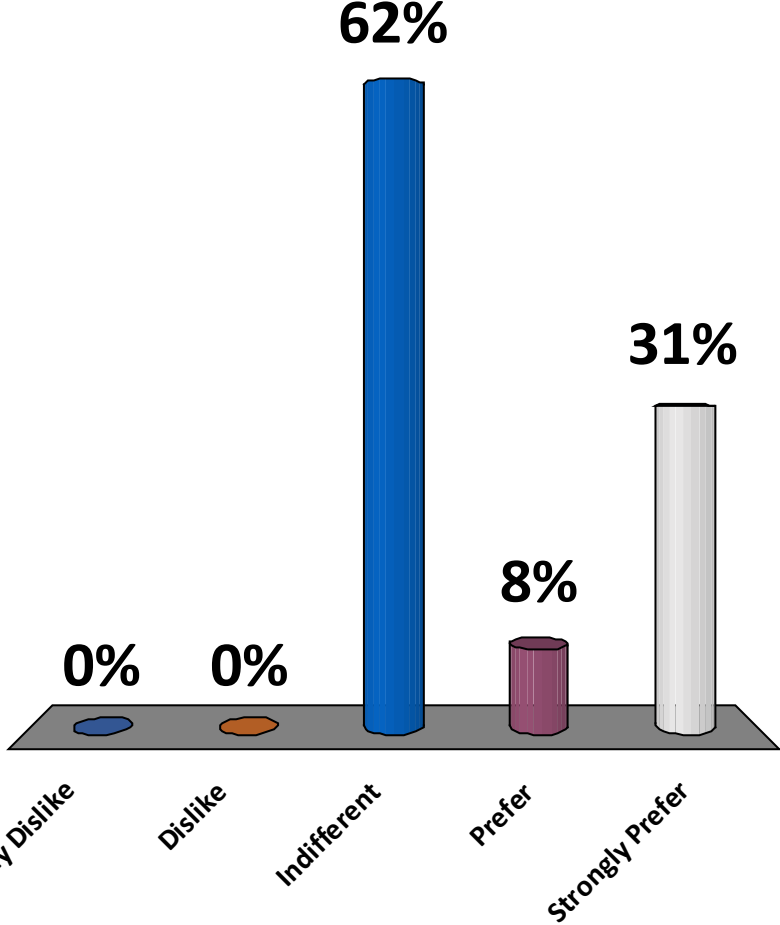
Mean = 3.07 Item 14.



Traffic Calming | 4 Way Stop

Mean = 3.69

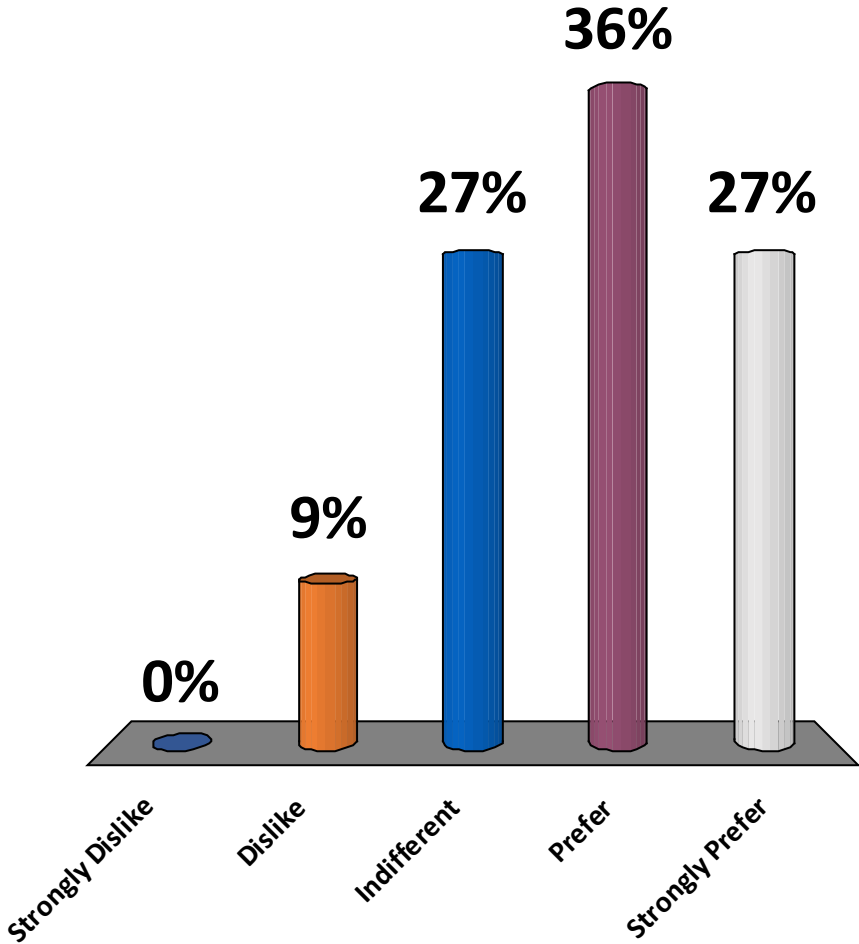
Item 14.



Traffic Calming | Raised Crosswalks

Mean = 3.82

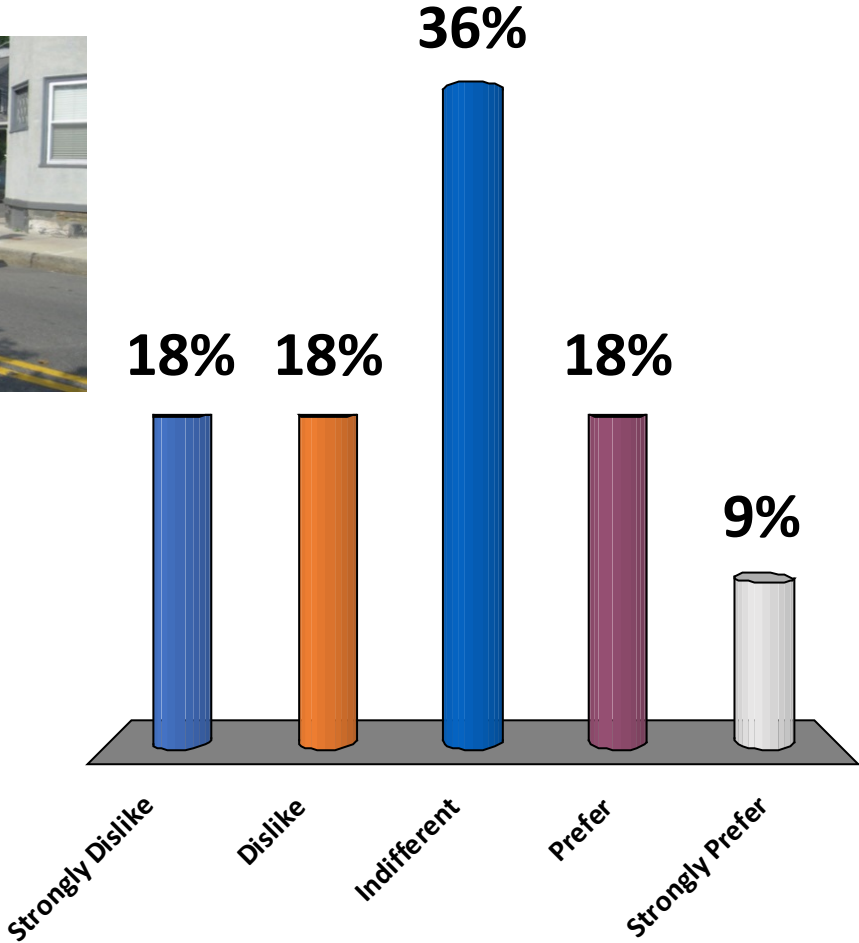
Item 14.



Traffic Calming | Raised Intersection

Mean = 2.82

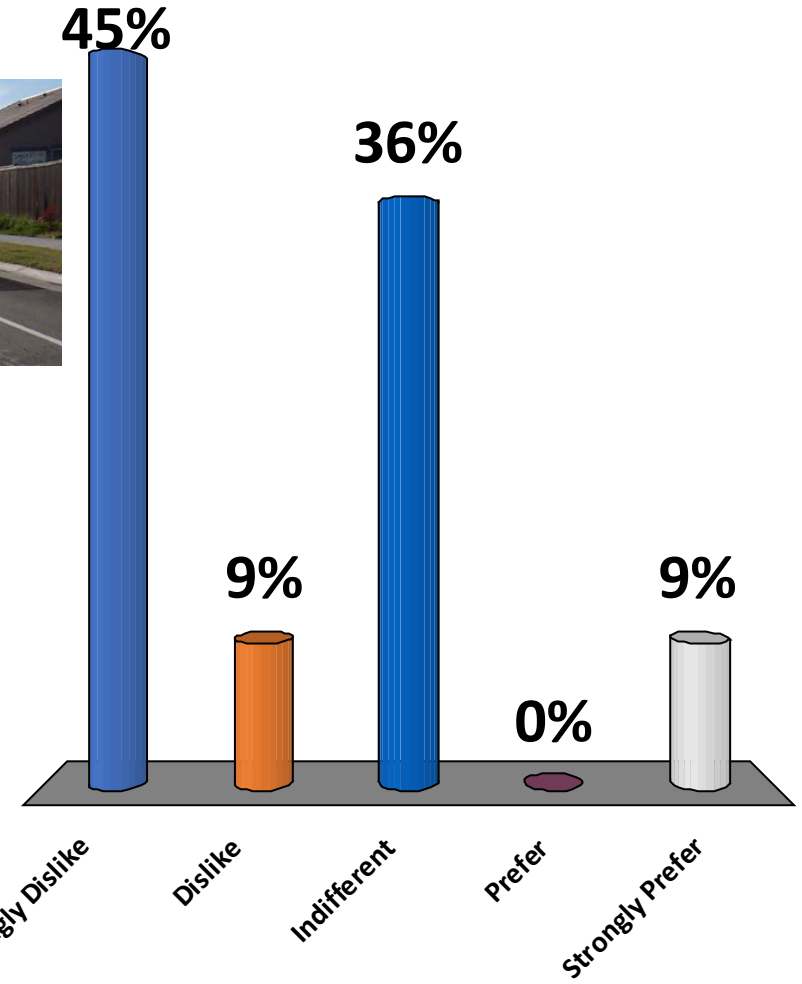
Item 14.



Traffic Calming | Speed Humps

Item 14.

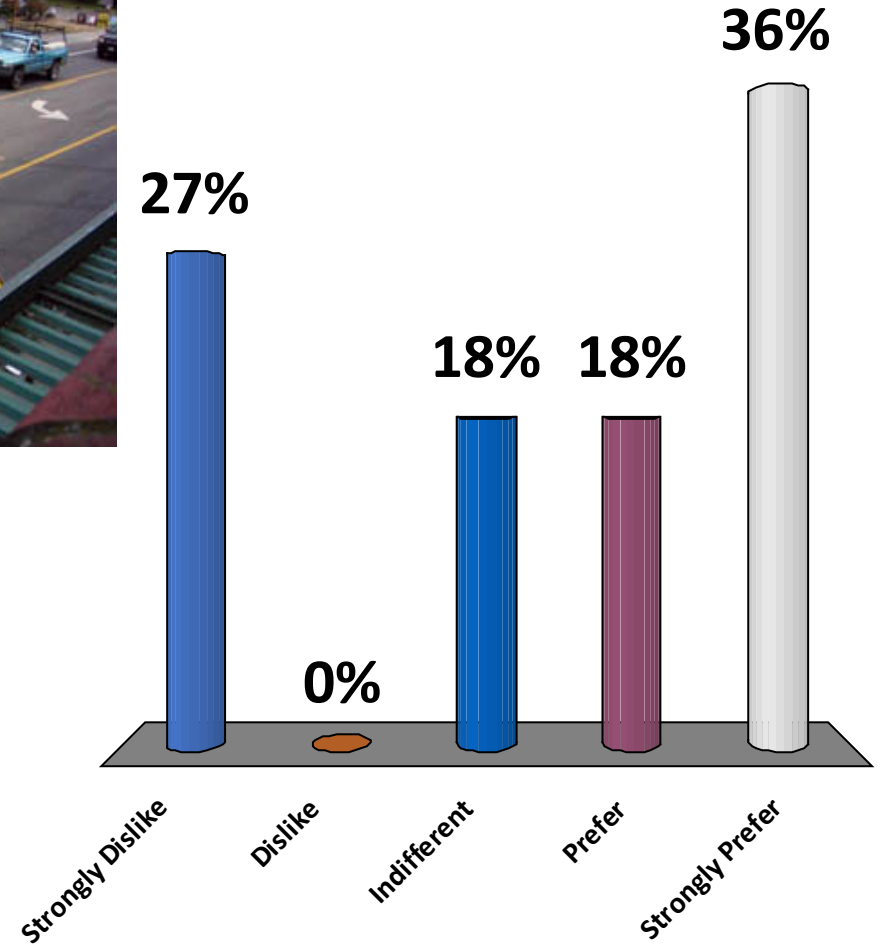
Mean = 2.18



Traffic Calming | Medians

Mean = 3.36

Item 14.

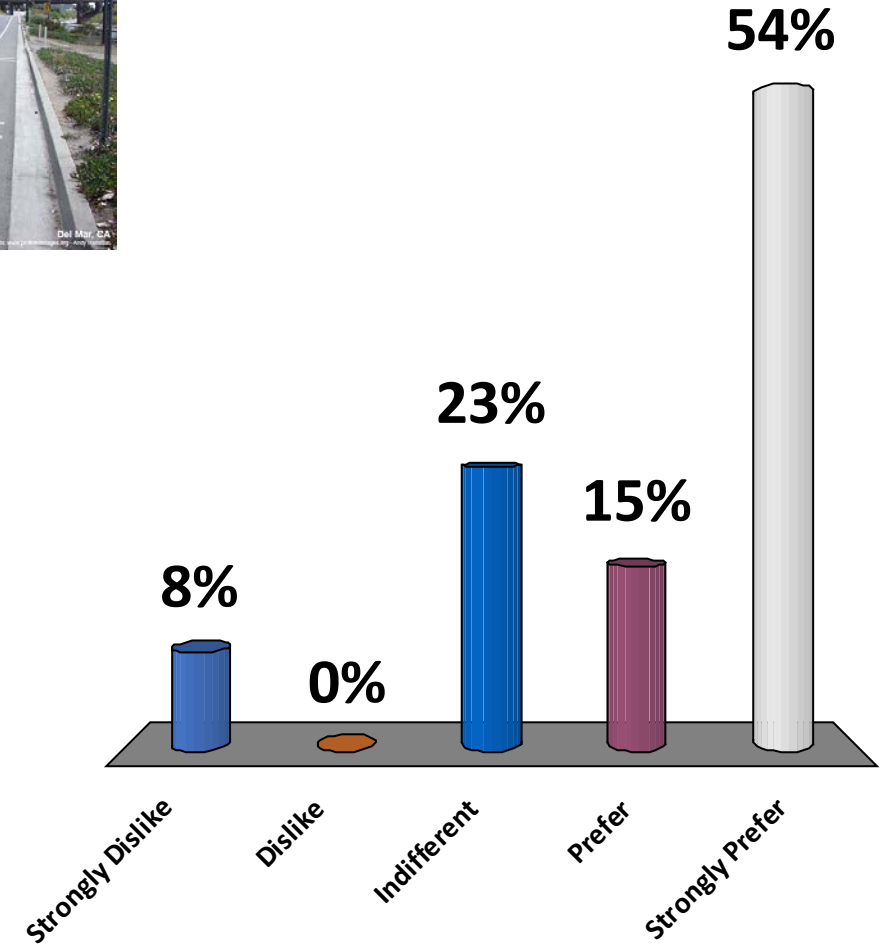


Bicycle Facilities

Bicycle Facilities | Bike Lane

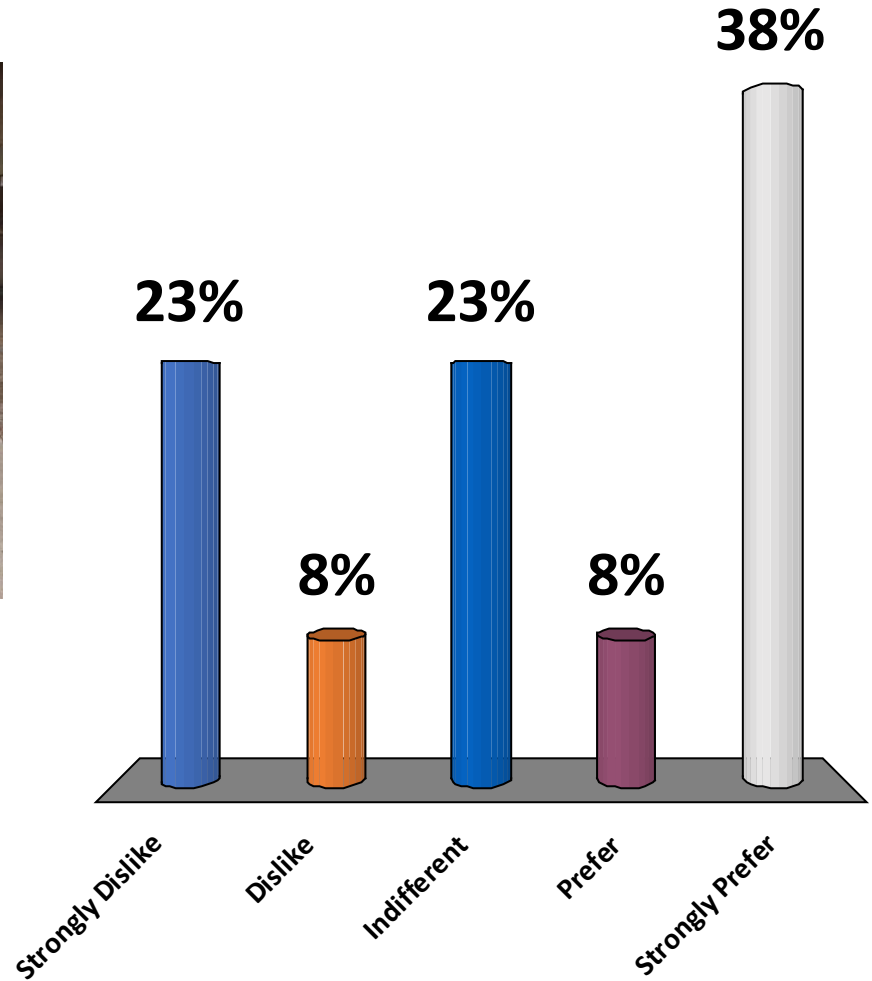
Mean = 4.08

Item 14.



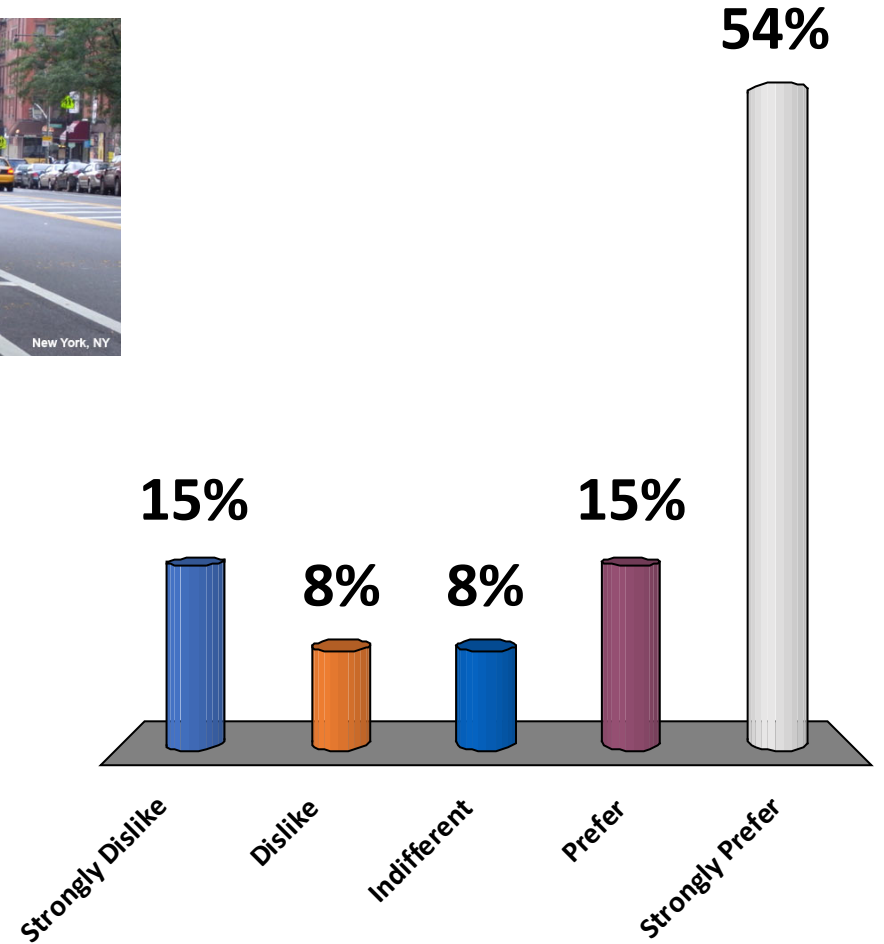
Bicycle Facilities | Green Bike Lane

Mean = 3.31 Item 14.



Bicycle Facilities | Buffered Bike Lane

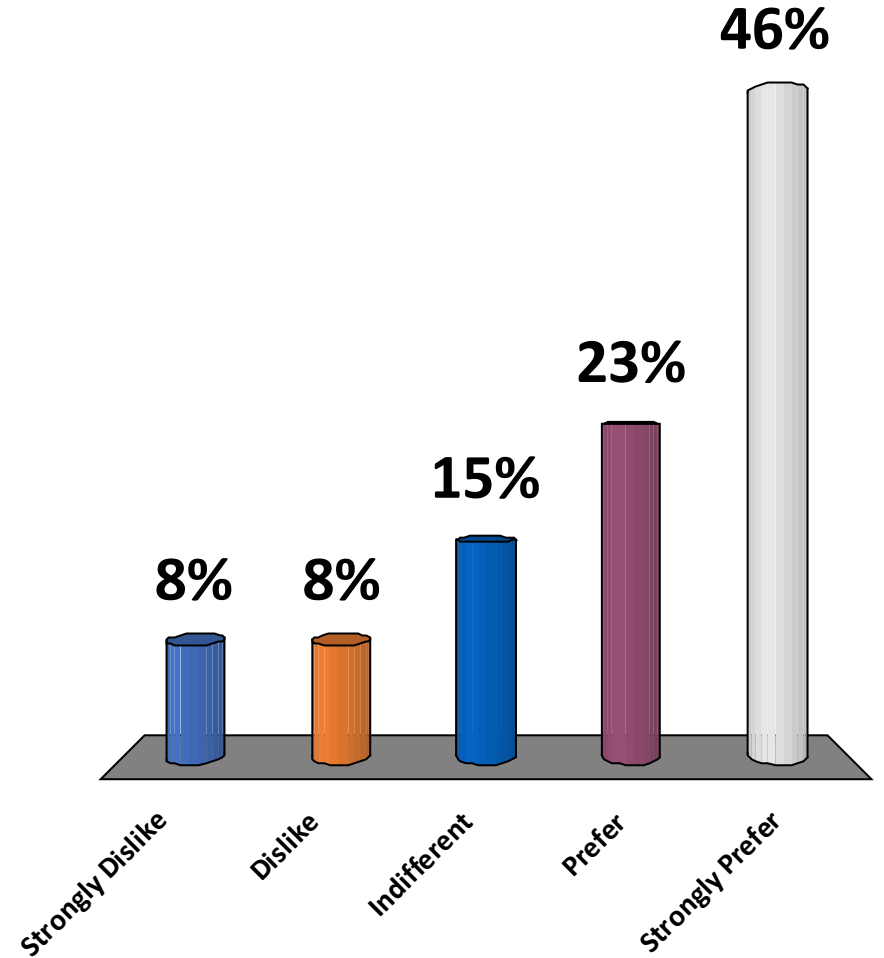
Mean = 3.85 Item 14.



Bicycle Facilities | Parking Protected Bike Lane/Cycle Track



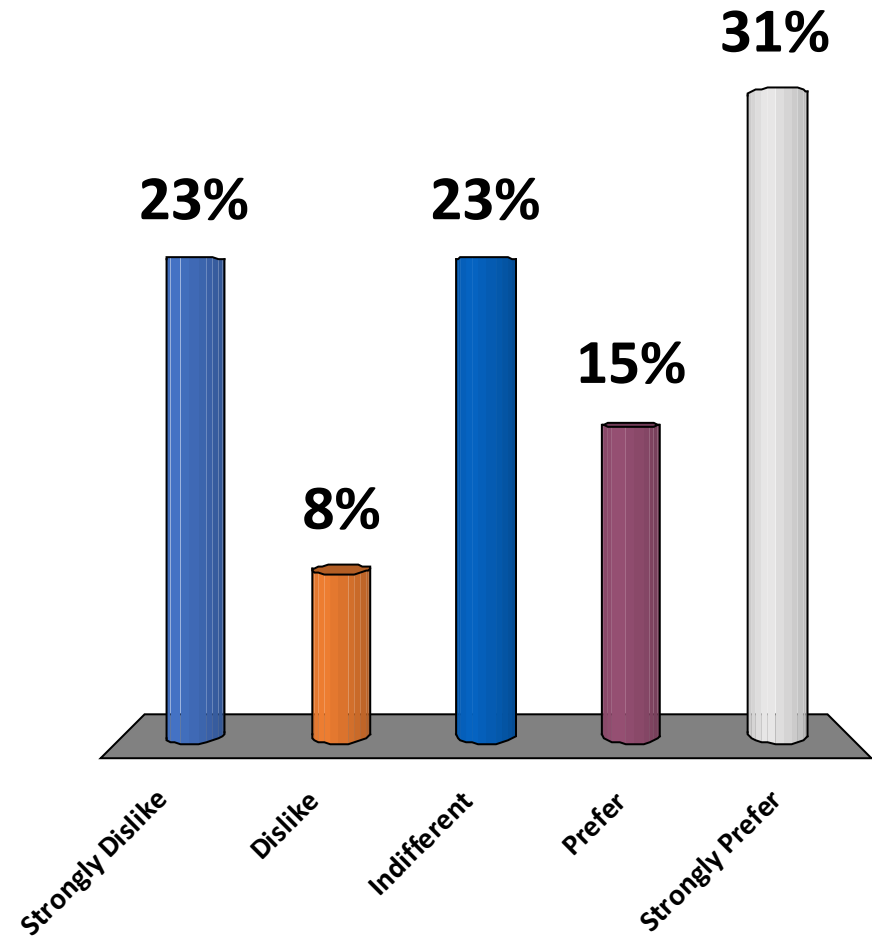
Mean = 3.92 Item 14.



Bicycle Facilities | Planter/Curb Protected Bike Lane/Cycle Track

Mean = 3.23

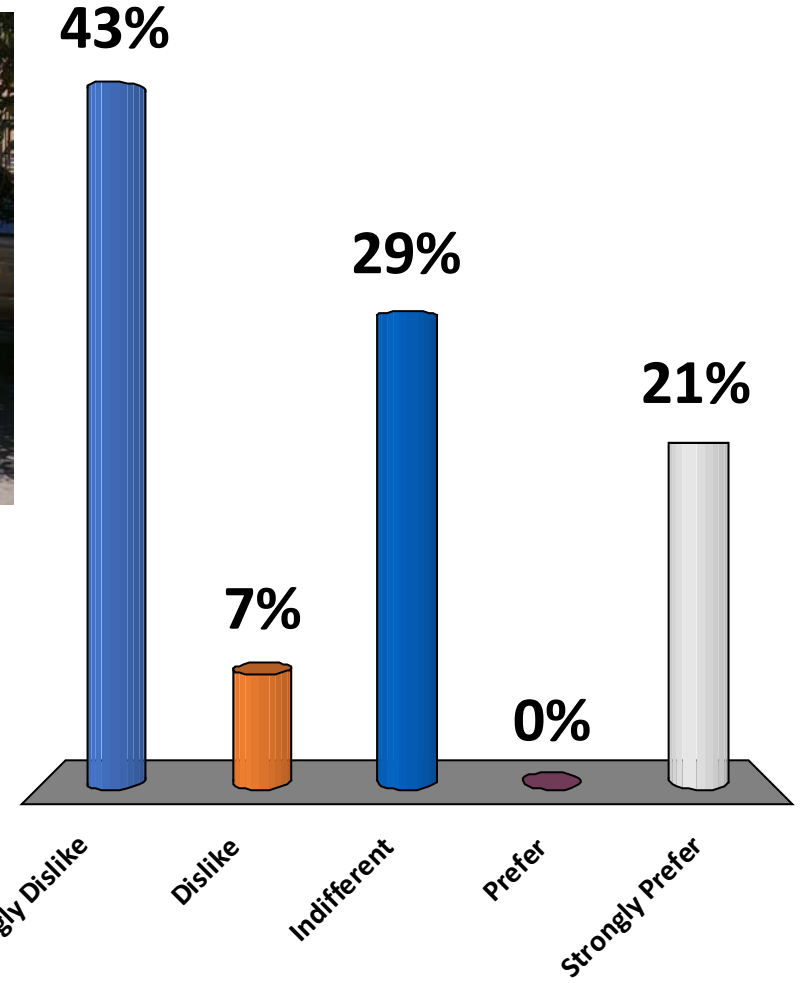
Item 14.



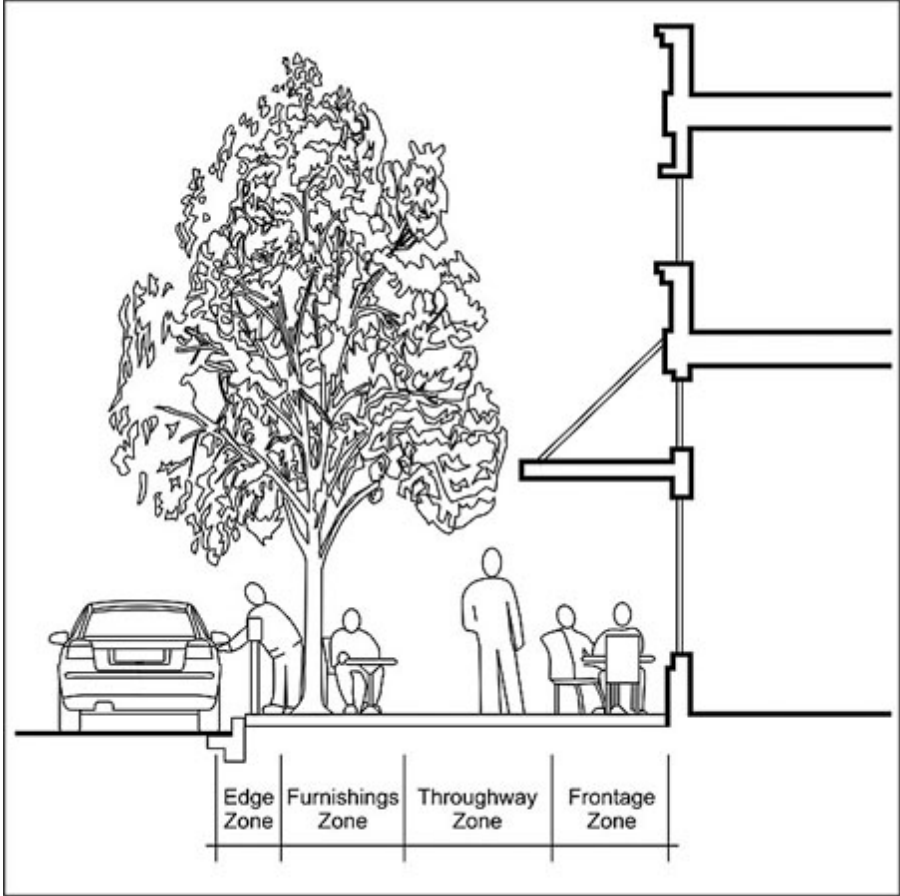
Bicycle Facilities | Sharrows

Item 14.

Mean = 2.50

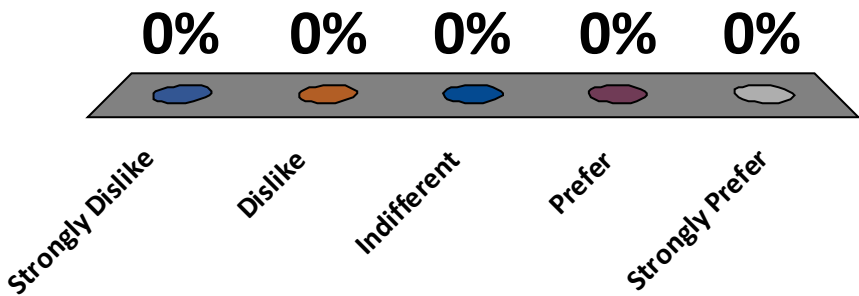


Components of a Sidewalk



Sidewalk Pavement

Sidewalk Pavement | Pavers



Sidewalk Pavement | Concrete

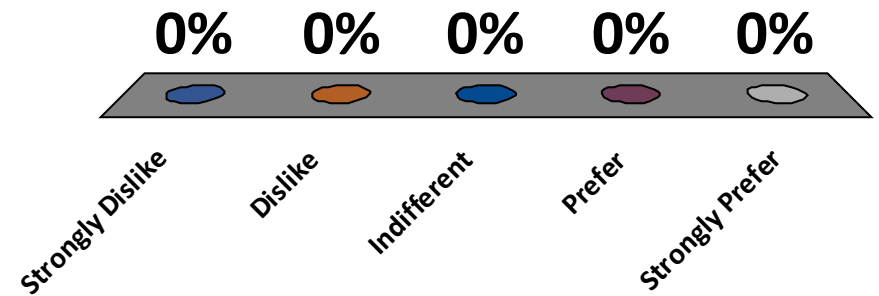


4. Prefer
5. Strongly Prefer



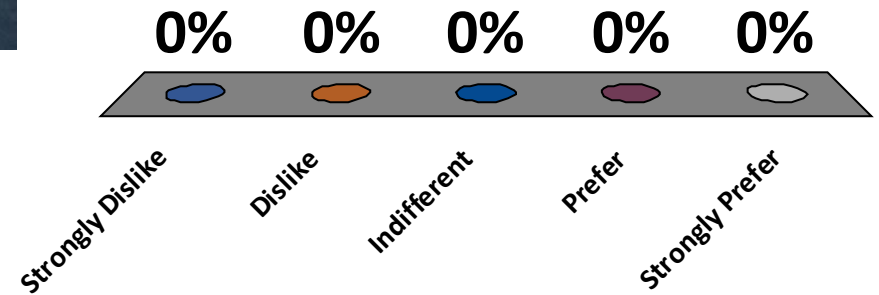
Mean =

Item 14.



Sidewalk Pavement | Stamped Concrete

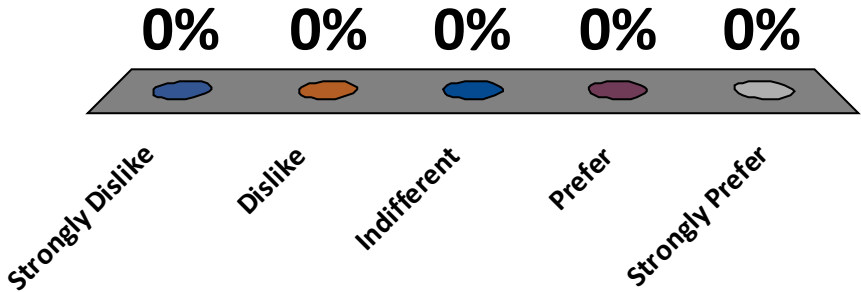
Mean = Item 14.



Sidewalk Pavement | Stamped Asphalt

Mean =

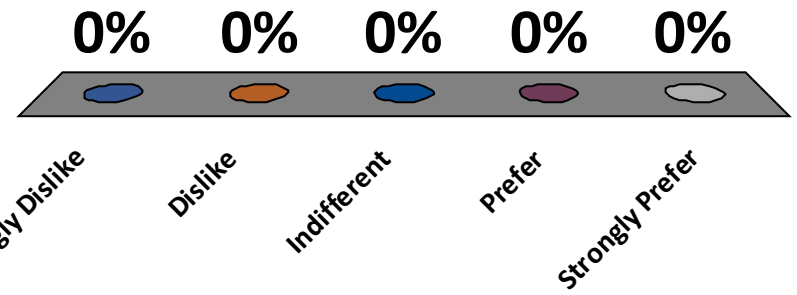
Item 14.



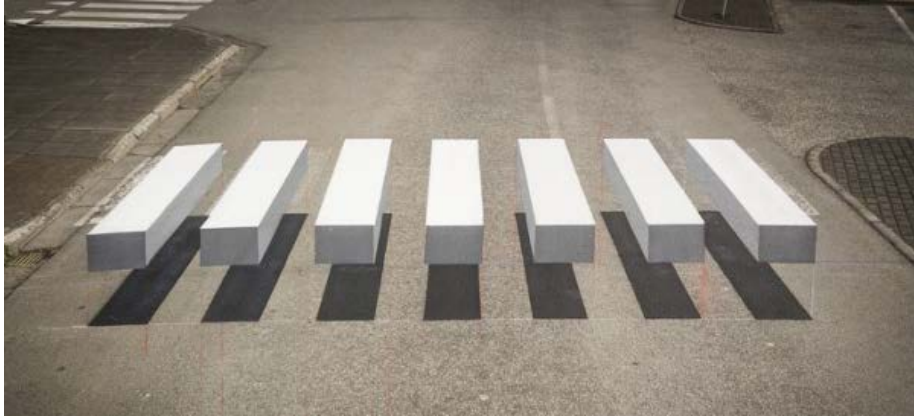
Sidewalk Pavement | Street Print

Mean =

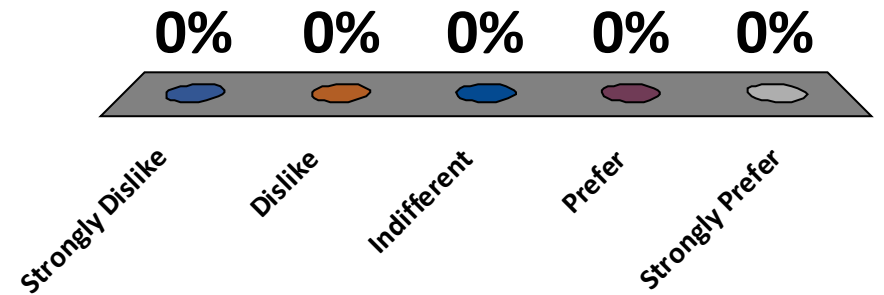
Item 14.



Sidewalk Pavement | Painted/Mural



Mean = Item 14.

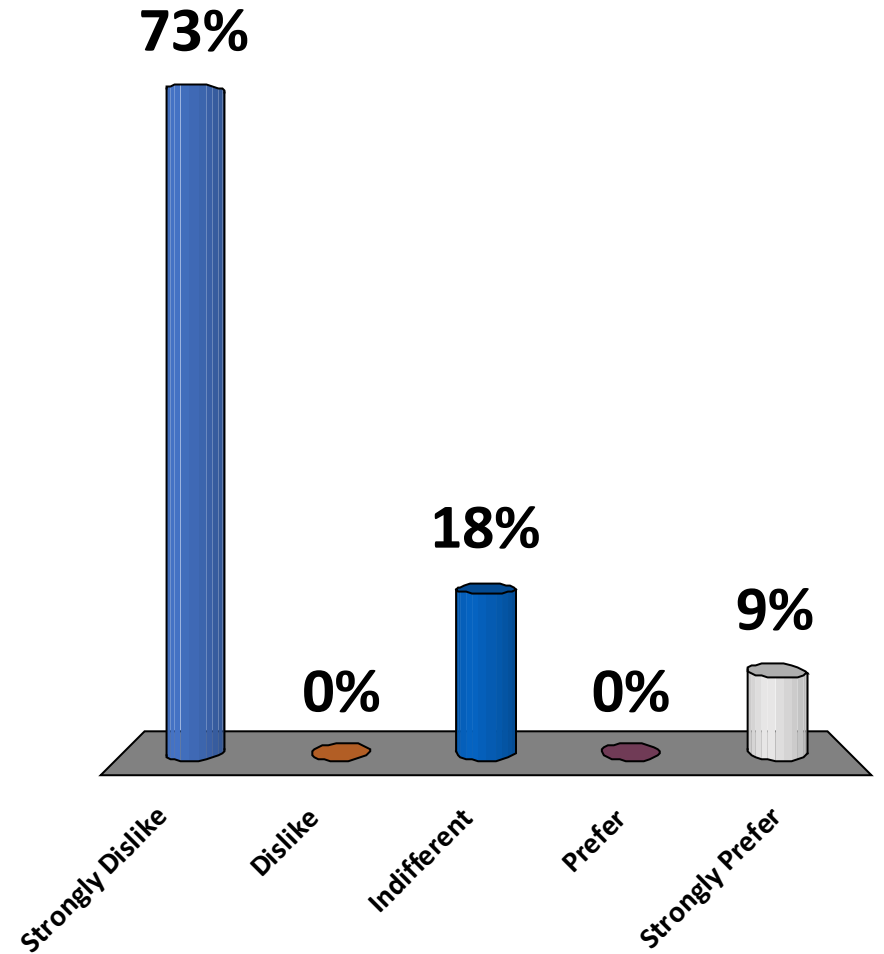


Bus Shelters

Bus Stop | Traditional (No Shelter)

Mean = 1.73

Item 14.

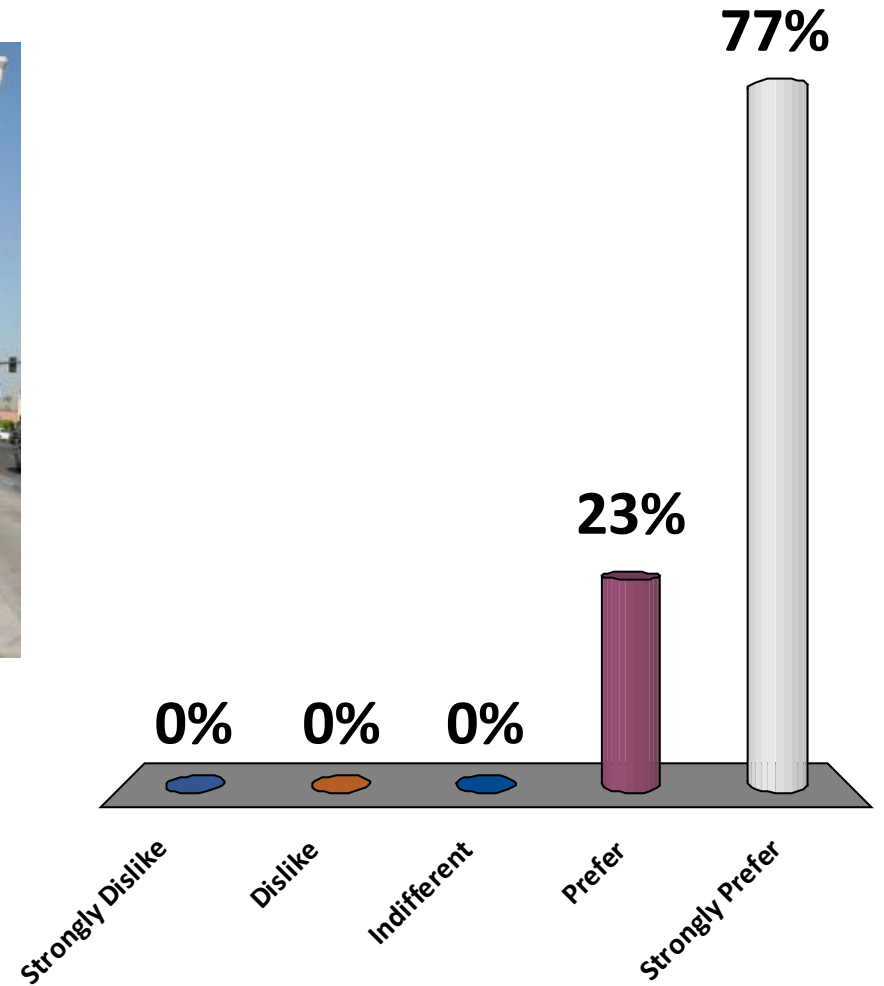


Bus Stop | Shelter



Mean = 4.77

Item 14.

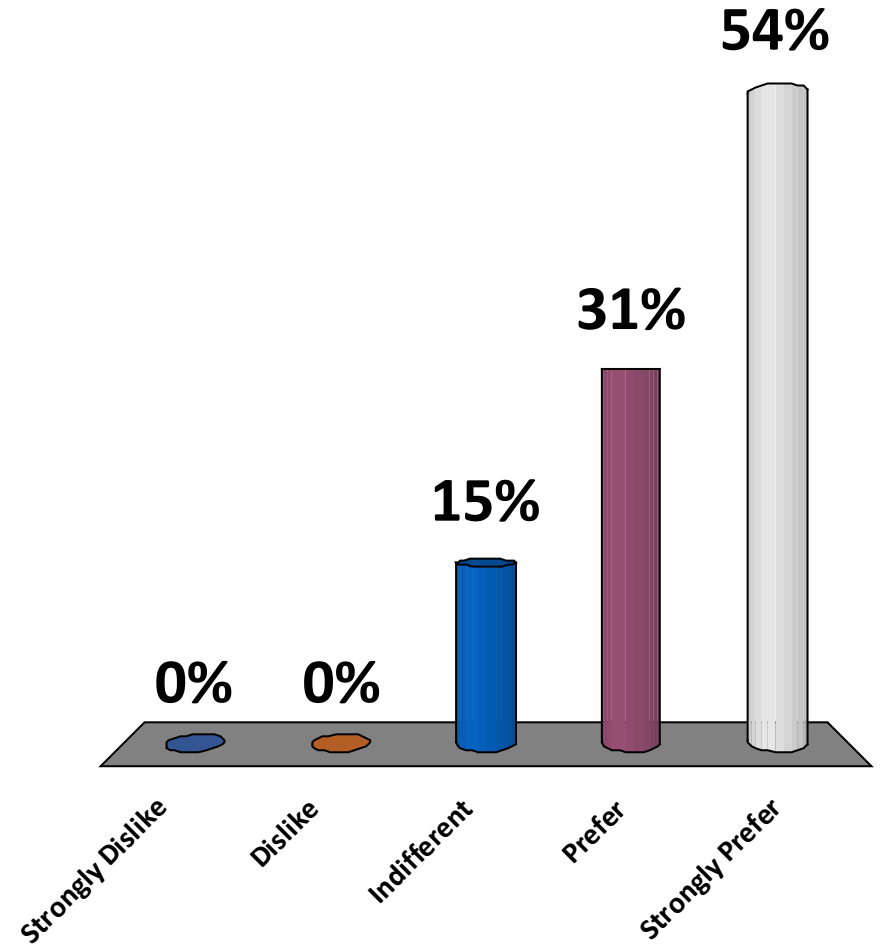


Sidewalk Lighting

Sidewalk Lighting | Sidewalk/Parking Lighting

Mean = 4.38

Item 14.

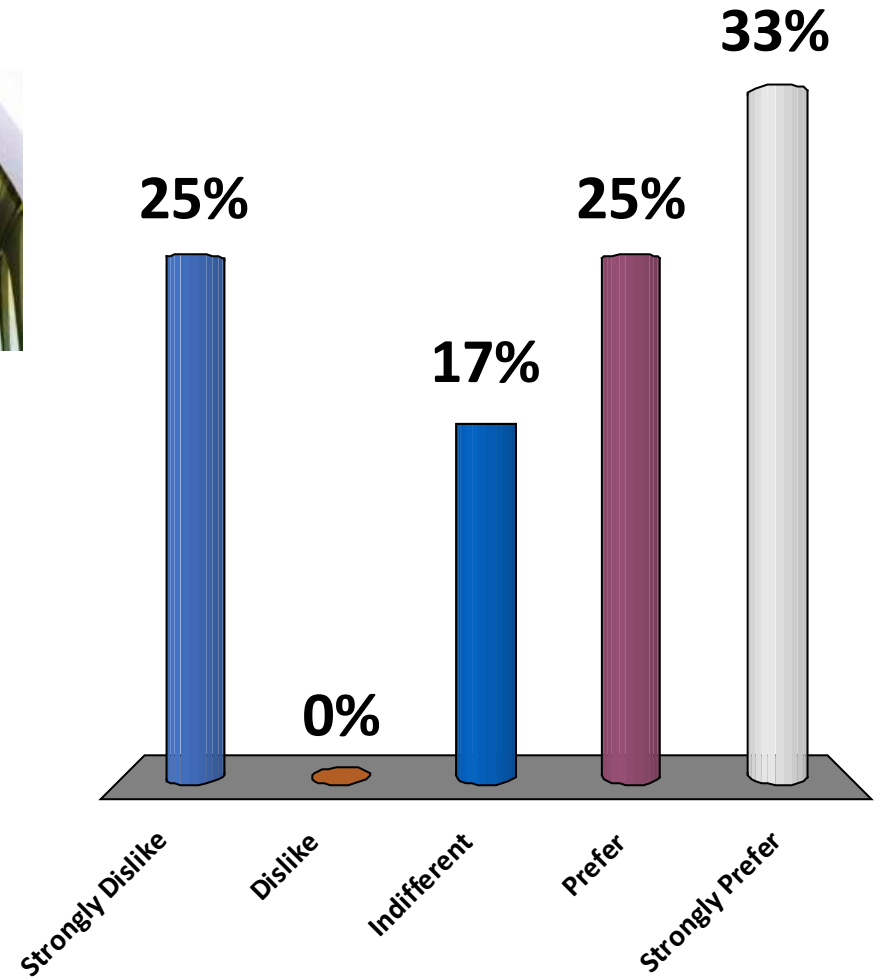


Sidewalk Lighting | Building-Mounted Lighting



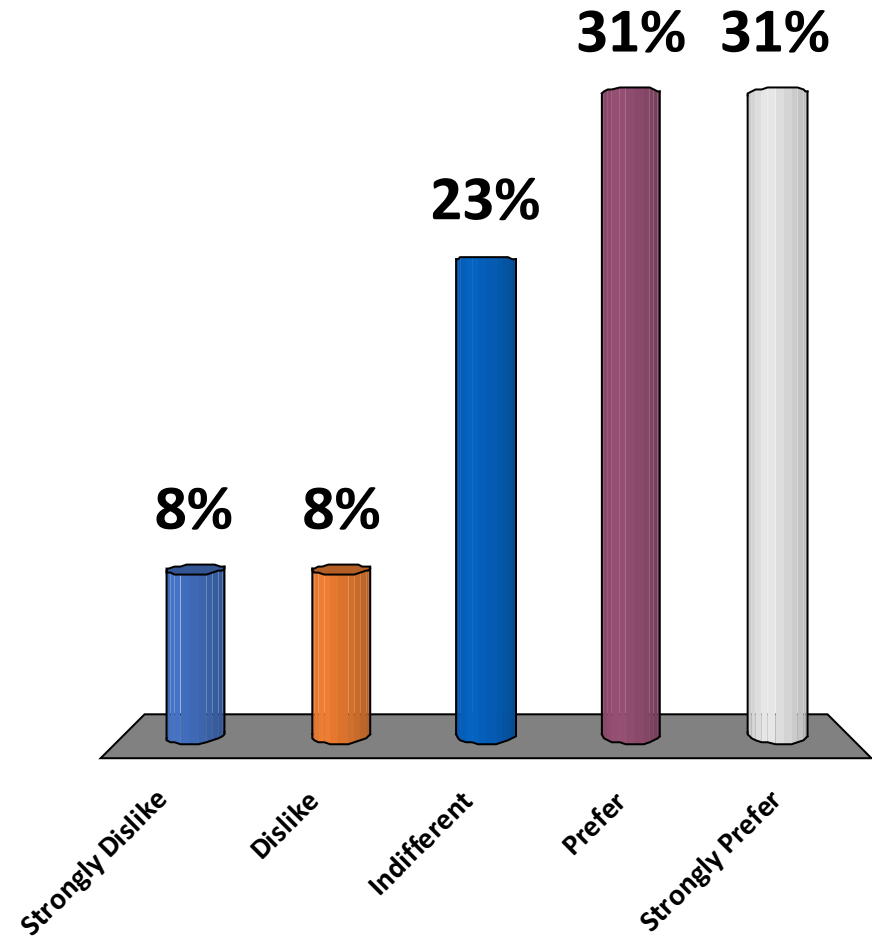
Mean = 3.42

Item 14.



Sidewalk Lighting | Bollard Lighting/Divider

Mean = 3.69 Item 14.

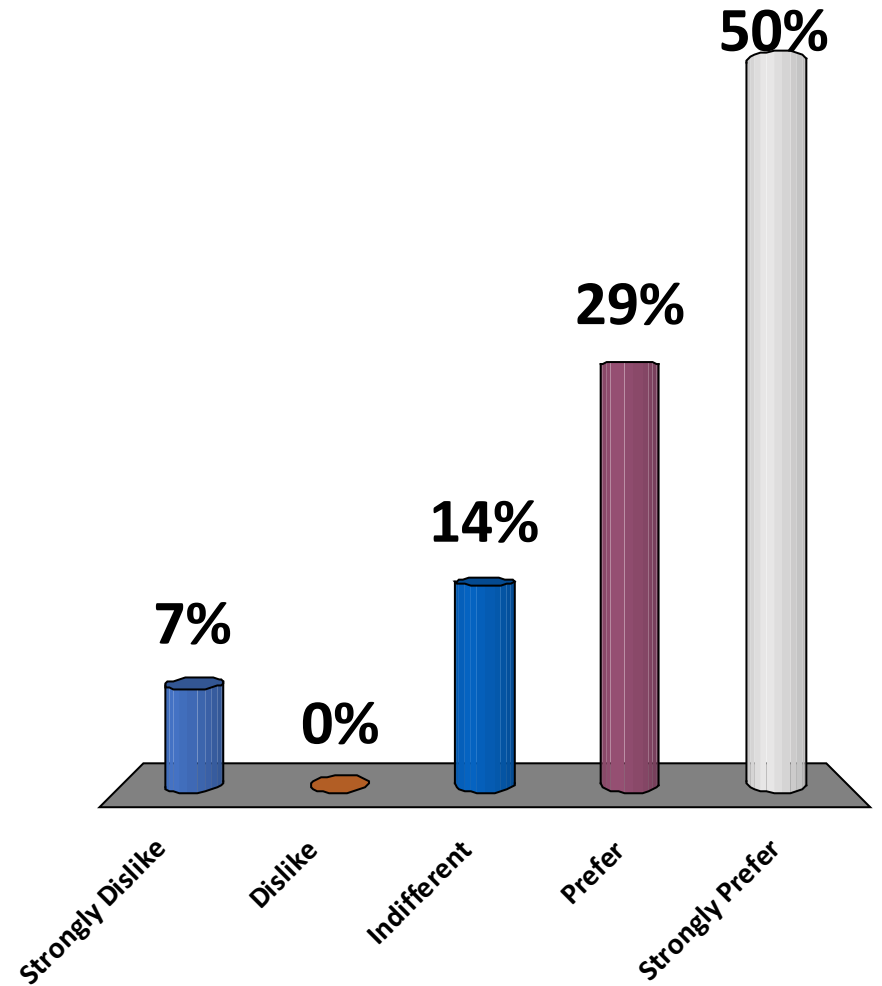


Bike Storage

Bike Storage | Bike Rack



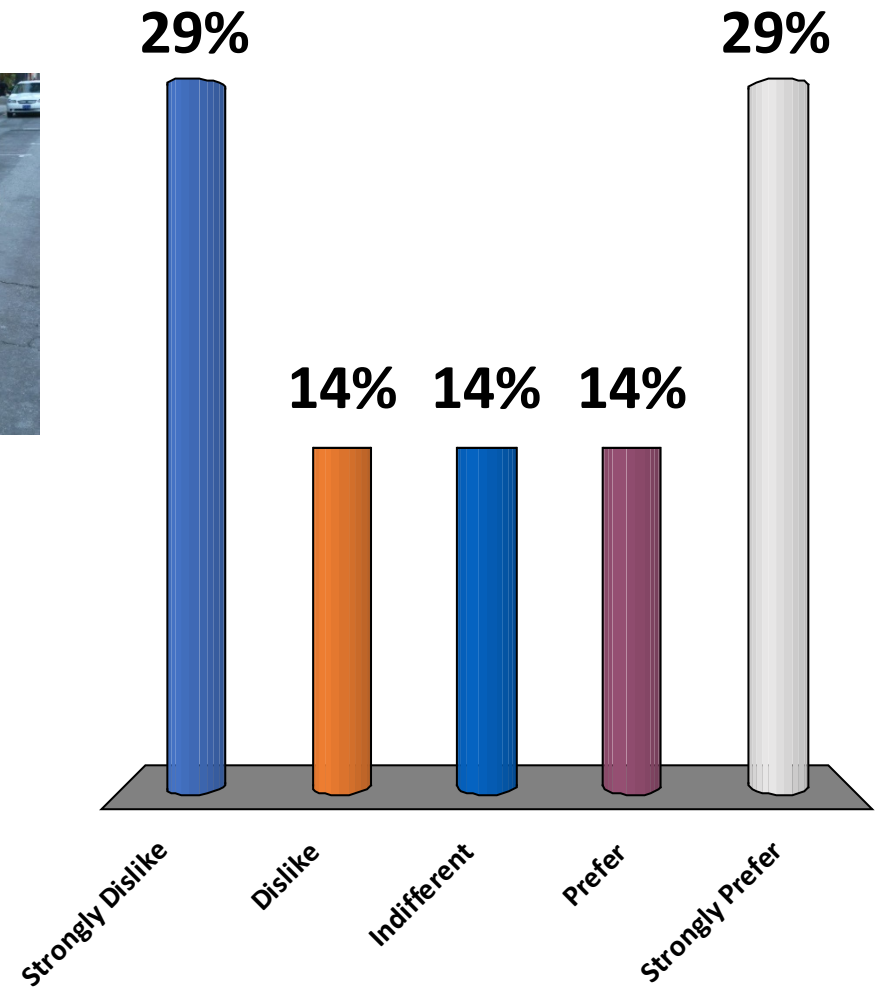
Mean = 4.14 Item 14.



Bike Storage | Bike Corral



Mean = 3.00 Item 14.



Landscaping

Landscaping | Planters

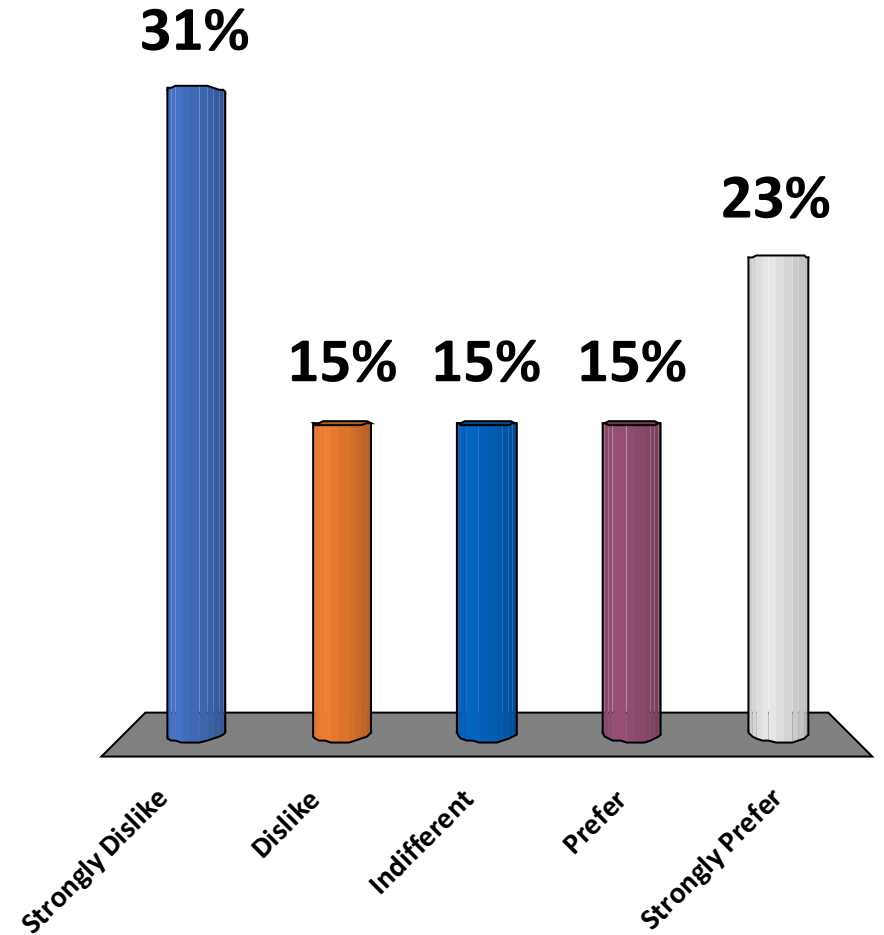


1 2 3 4 5
 STRONGLY DISLIKE ← INDIFFERENT → STRONGLY PREFER



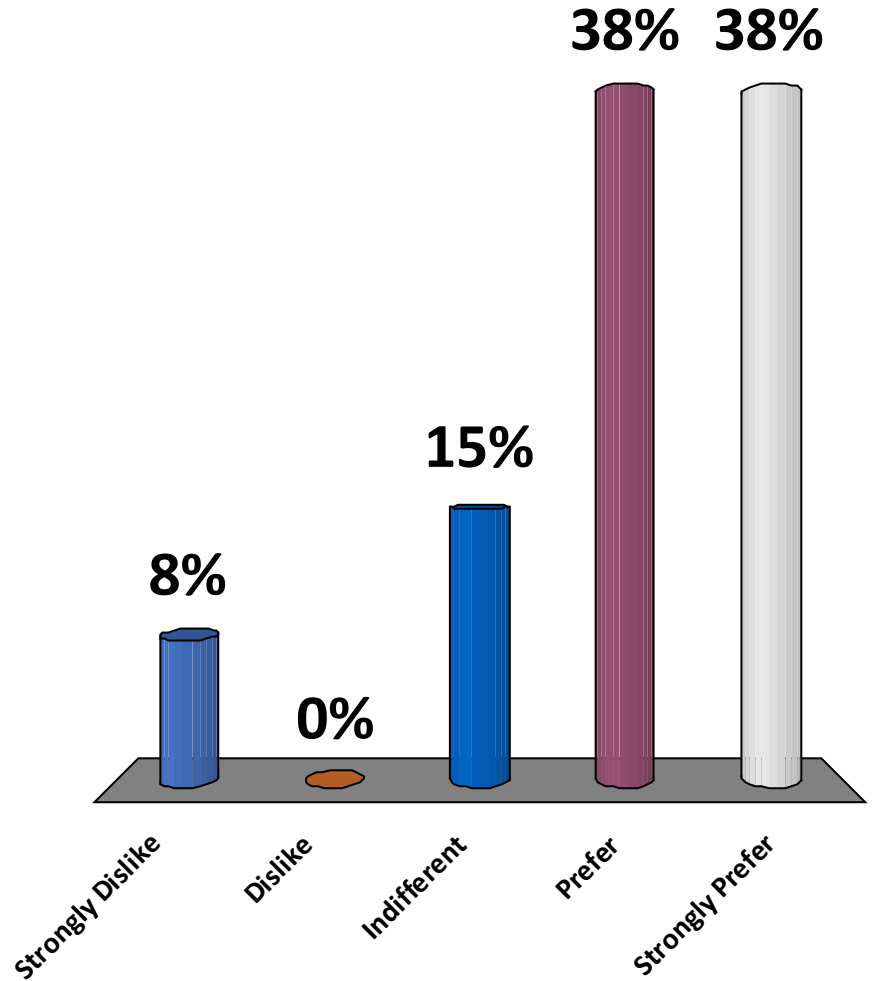
Mean = 2.85

Item 14.



Landscaping | Potted Plants

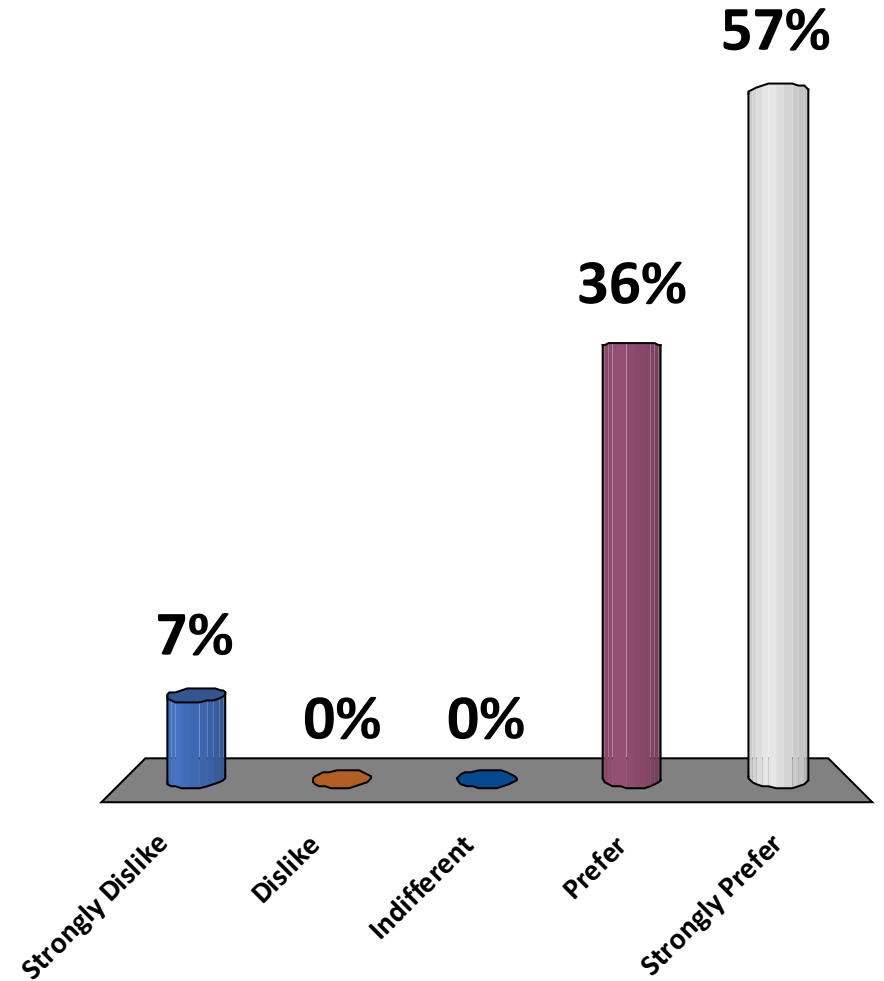
Mean = 4.00 Item 14.



Landscaping | Trees in Tree Wells (Open)

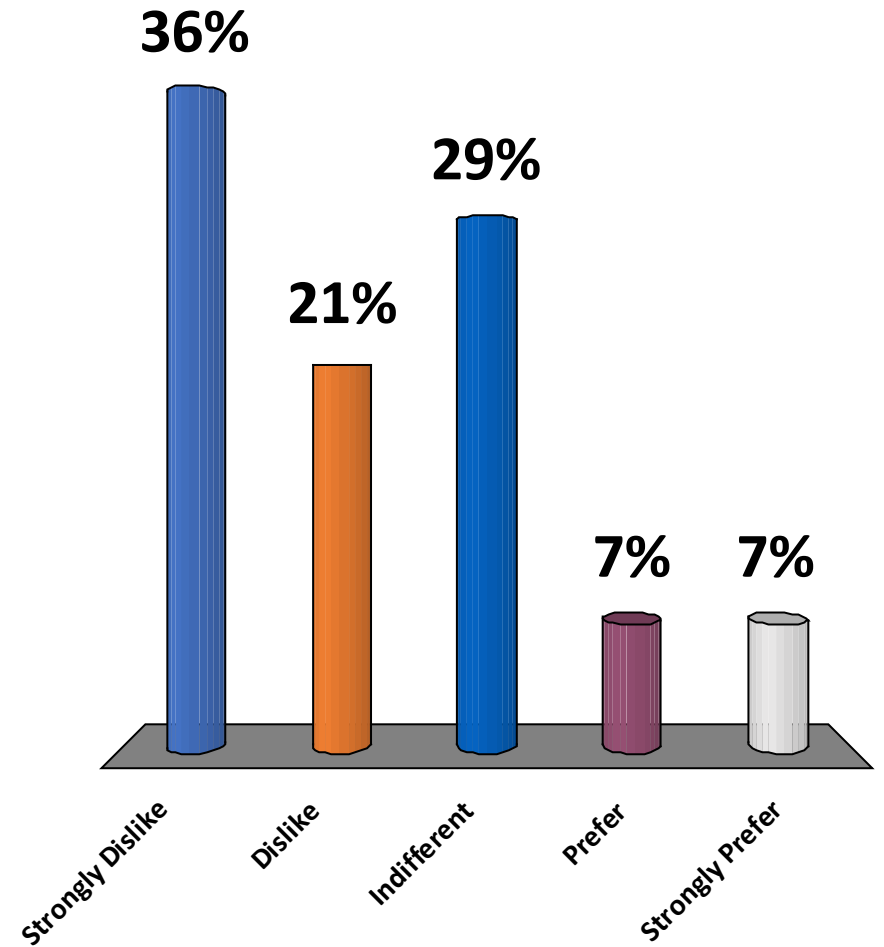


Mean = 4.36 Item 14.



Landscaping | Trees in Tree Wells (Solid)

Mean = 2.29 Item 14.

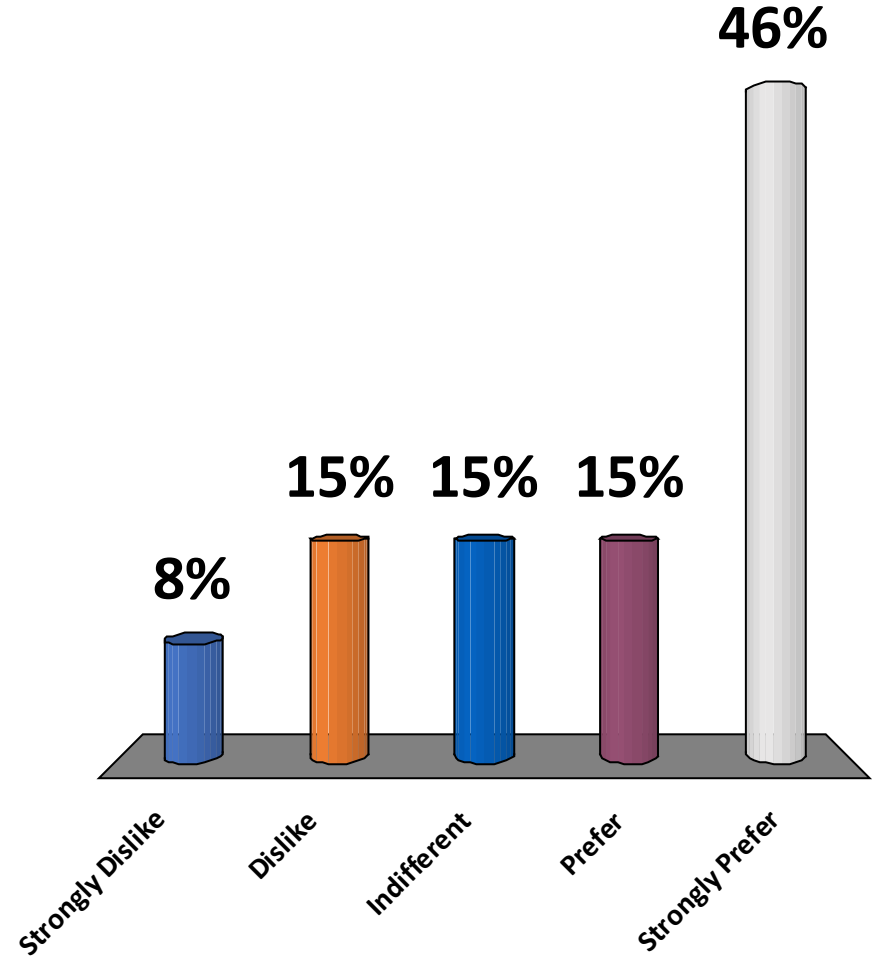


Water Features

Water Feature | Fountains

Mean = 3.77

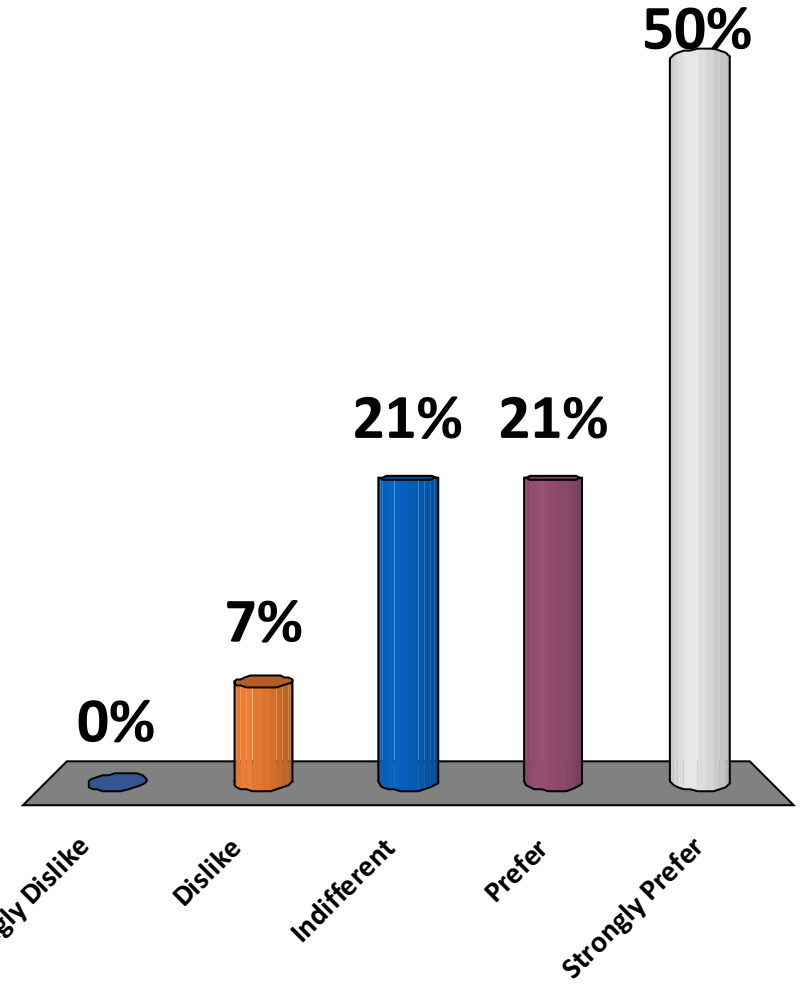
Item 14.



Water Feature | Interactive Fountains

Mean = 4.14

Item 14.



Charrette Overview



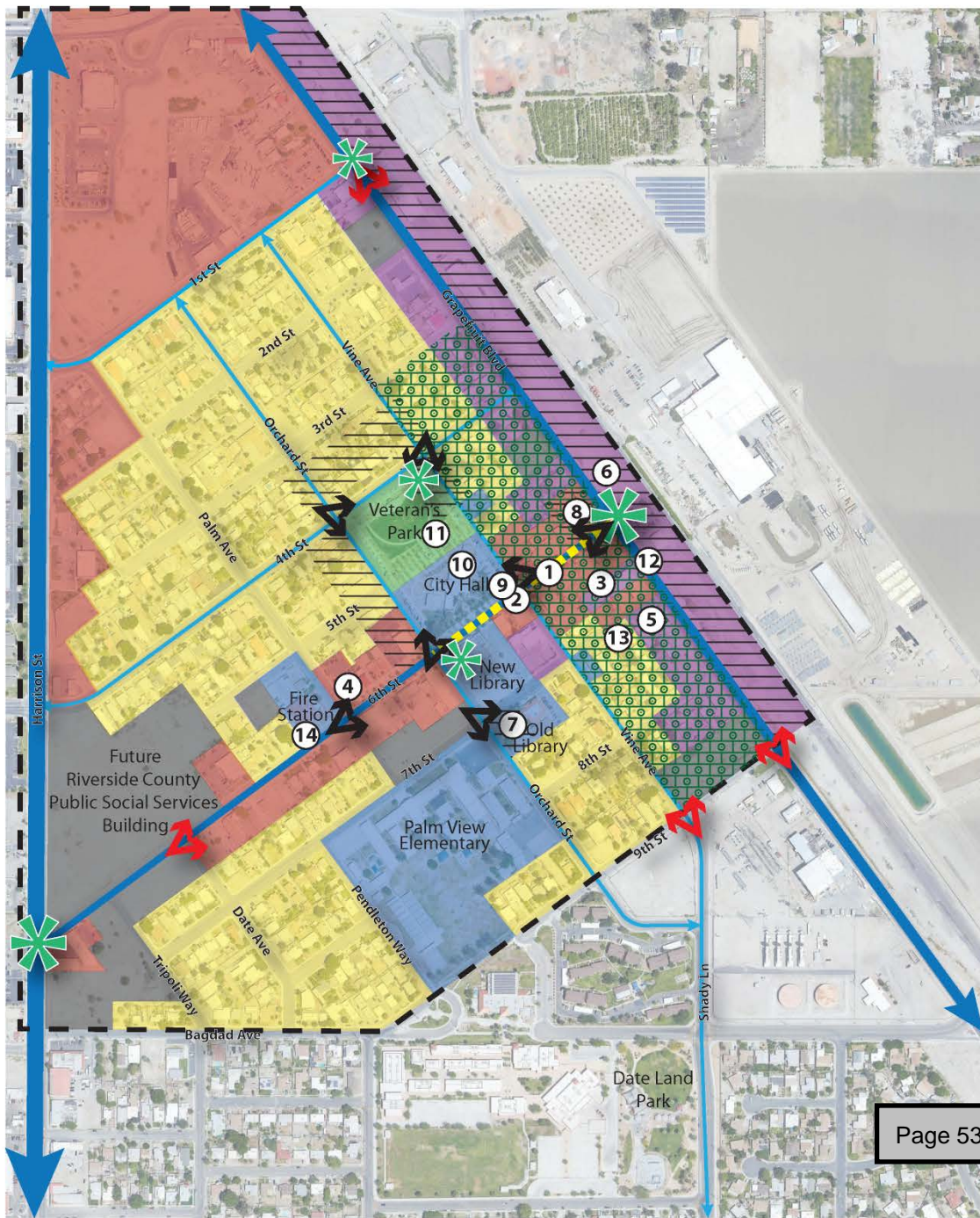
Activities

- Field Study of the Pueblo Viejo District
- Stakeholder Interviews with 10 community groups
- Technical Charrette with members of City staff
- Meet with representatives of City Council



SITE ANALYSIS - EXISTING CONDITIONS

Item 14.



LEGEND

- - - Primary Project Area Boundary
- Blue Arrow Primary Roadways
- Light Blue Arrow Secondary Roadways
- Red Commercial/Retail/Office
- Yellow Residential
- Blue Schools/Civic/Religious Uses
- Purple Industrial Uses
- Grey Vacant Parcels
- Green Parks
- Diagonal Lines Opportunity Areas
- Yellow Dashed Line Solid Frontage / Complete Streetscape
- Green Grid Proposed Retail Cannabis District
- Green Asterisk Major Nodes and Gateways
- White Star Mural and/or Public Art
- Black Arrow Viewshed To Preserve
- Red Arrow Viewshed To Improve









Item 14.



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Google

33°40'30"N 116°10'56"W 198 ft

Next Steps

- Creation of Design Guidelines for the Pueblo Viejo District
- Zoning Code Update
- Project Identification and Implementation Program
 - Conceptual Design
 - Preliminary Feasibility Analyses
 - Project Cost Opinions



Thank you!





STAFF REPORT
10/23/2019

TO: Honorable Mayor and City Council Members

FROM: Luis Lopez, Development Services Director

SUBJECT: Zona Central – Downtown Expansion

SPECIFICS:

- a) Resolution No. 2019-57 to adopt a Negative Declaration pursuant to California Environmental Quality Act Guidelines for the Zona Central - Change of Zone (CZ 18-10) project on approximately 206 acres of land located on the west side of Tyler Street between Avenue 50 and ¼ mile north of Avenue 52.
- b) Ordinance No. 1143 approving the City-Initiated Change of Zone (CZ 18-10) from M-S (Manufacturing Service) and R-S (Single Family Residential) to MS-IP (Manufacturing Service – Industrial Park); C-N (Neighborhood Commercial); CN-PD (Neighborhood Commercial Planned Development); R-M (Residential, Multi-Family); O-S (Open Space); and OS-PF (Open Space Public Facilities) on approximately 206 acres of partially developed land on the west side of Tyler Street between Avenue 50, and ¼ mile north of Avenue 52. *1st Reading.*

STAFF RECOMMENDATION:

Staff recommends that that the City Council approve the Zona Central – Downtown Expansion project by taking the following actions:

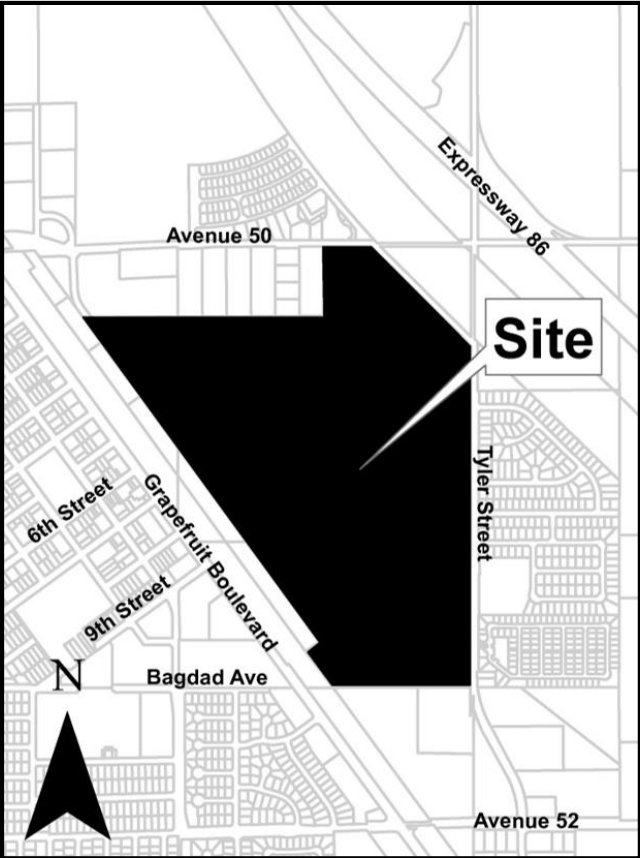
- 1) Adopt Resolution No. 2019-57 adopting a Negative Declaration, pursuant to California Environmental Quality Act Guidelines, for the Zona Central – Downtown Expansion Project consisting of Change of Zone (CZ 18-10) pursuant with the findings of the attached resolution.
- 2) Introduce for 1st Reading, by title only, Ordinance No. 1143 approving the City-Initiated Change of Zone (CZ 18-10) from M-S (Manufacturing Service) and R-S (Single Family Residential) to MS-IP (Manufacturing Service – Industrial Park); C-N (Neighborhood Commercial); CN-PD (Neighborhood Commercial Planned Development); R-M (Residential, Multi-Family); O-S (Open Space); and OS-PF (Open Space Public Facilities) on approximately 206 acres of partially developed land on the west side of Tyler Street between Avenue 50, and ¼ mile north of Avenue 52.

EXECUTIVE SUMMARY:

The City of Coachella, in cooperation with Peter Rabbit Farms (Property Owner), has initiated a Change of Zone to bring approximately 206-acres of land located south of Avenue 50 and west of Tyler Street into conformance with the City of Coachella General Plan. The current General Plan Land Use designation for the property is an Urban Employment Center land use designation that is more specifically defined under the Subarea 6 – Downtown Expansion policies, which provide for a variety of employment (office/business parks/light industry) with high-density housing opportunities. When approved, this Zone Change will bring 206 acres of land into compliance with the General Plan and provide significant opportunities for a variety of future business park, offices, light industrial, and college campus environments mixed with high-density residential uses.

BACKGROUND:

Pursuant to the City of Coachella, General Plan 2035, the *Zona Central* project is proposed in order to bring the 206-acres (located on the south side of Avenue 50, west of Tyler Street) into full compliance with the General Plan’s “Land Use and Community Character Element”. Sub-Area 6 is a combination district that includes elements of both employment and residential uses. Prior to the approval of this Change of Zone (CZ 18-10), the project site contains only two zoning districts: M-S (Manufacturing Service) and R-S (Single Family Residential). The current zoning fails to provide the variety of employment flexibility and the variety of housing types envisioned by the General Plan. To bring the site into compliance with the General Plan, and to comply with the City’s Housing Element rezoning requirements, the City of Coachella Development Services with the consent of the Property Owner proposes to create the following Zoning Districts:



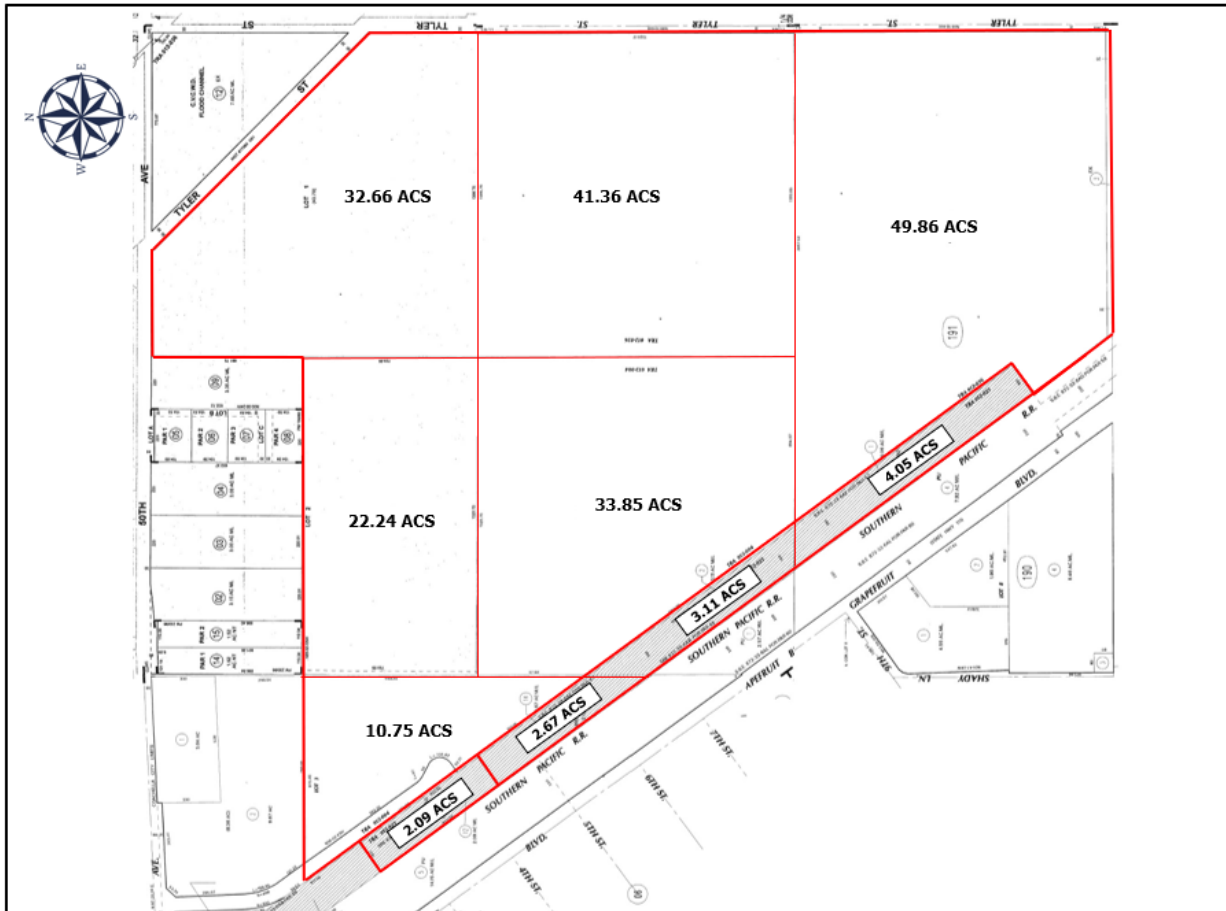
- MS-IP; Manufacturing Service – Industrial Park
- C-N; Neighborhood Commercial
- CN-PD; Neighborhood Commercial Planned Development (Mixed Use, Urban Employment Center)
- R-M; Residential, Multi-Family (General Neighborhood)
- R-M; Residential, Multi-Family (Urban Neighborhood)
- O-S; Open Space
- OS-PF; Open Space Public Facilities

Total Project Area	206.2 acres*
MS-IP Zone	35.6 acres*
C-N Zone	13.5 acres*
CN-PD (Mixed Use) Zone	42.6 acres*
R-M (General) Zone	21.2 acres*
R-M (Urban) Zone	51.4 acres*
O-S Zone	8.0 acres*
OS-PF Zone	10.3 acres*
Interior Streets	23.6 acres*

**Note!* Project Area Estimates taken from Riverside County Tax Assessor Maps. Exact acreage may vary.

A total of 10 separate properties are included within the proposed rezoning. **Exhibit B** provides a composite view of the Riverside County Tax Assessor’s Maps for this area. The properties being rezoned for consistency with the General Plan are outlined in red.

EXHIBIT B



RIVERSIDE COUNTY TAX ASSESSORS MAP - COMPOSITE

DISCUSSION/ANALYSIS:

The City is proposing Change of Zone 18-10 (CZ 18-10) to bring the properties within Subarea 6 into compliance with the General Plan. The approval of CZ 18-10 will fulfill many of the stated goals for employment and housing within Subarea 6 which encourages the development of a wide variety of business, employment, and housing types within a walkable, urban environment.

Exhibit C shown below is a Google Earth Image which shows the general vicinity of the Zona Central project in proximity to Pueblo Viejo, Highway 111 and the Union Pacific Railroad, the CVWD Coachella Campus to the south, and the existing homes on the east side of Tyler Street.

EXHIBIT C



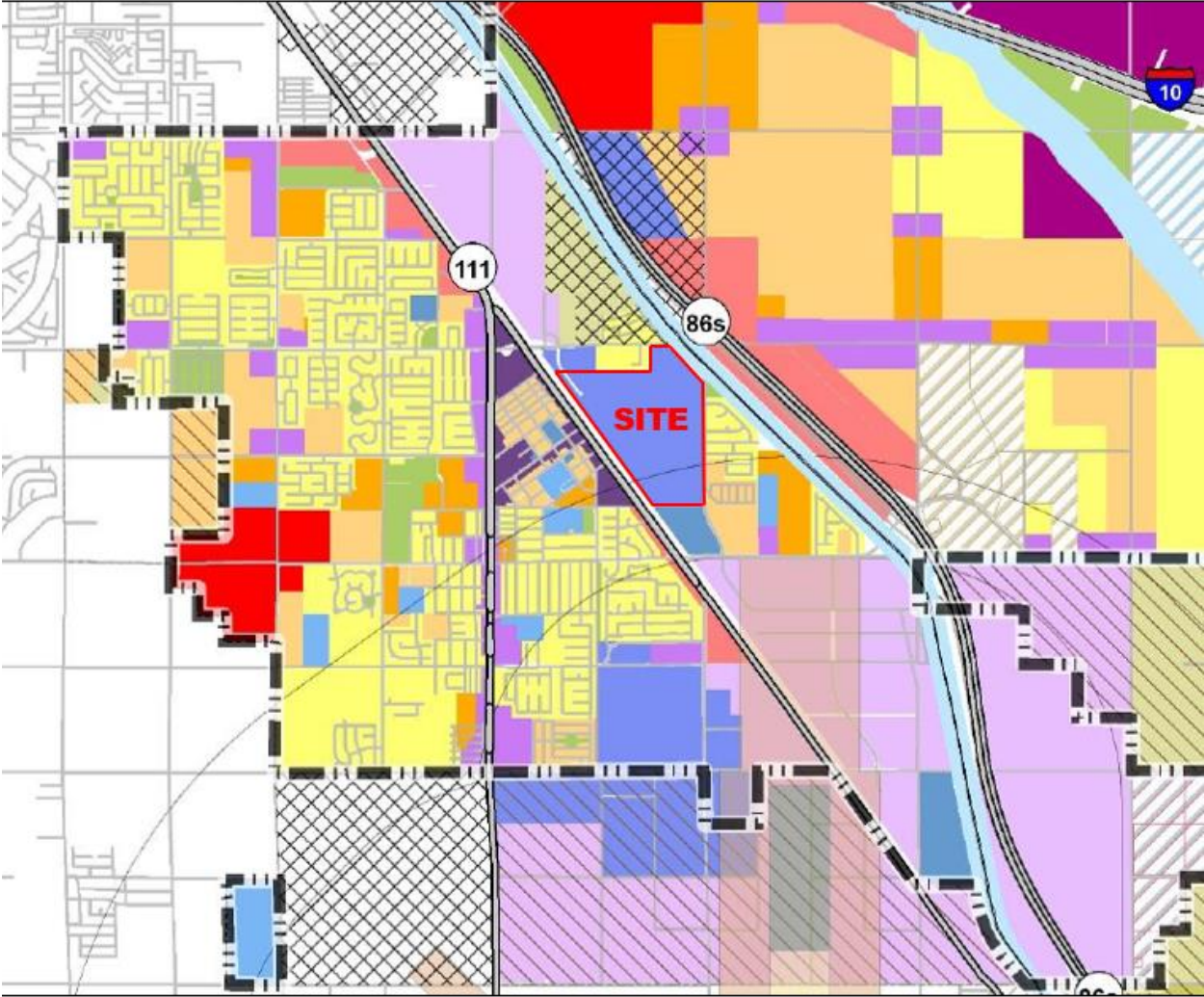
Consistency with the Coachella General Plan:

The proposed project is consistent with the General Plan and, upon approval by the City Council, the City of Coachella Official Zoning Map. According to the General Plan 2035 Land Use Element, the site is within Sub-Area 6, Downtown Expansion which encourages a mix of

business and employment types, with a variety of housing and residential uses, within a walkable urban environment.

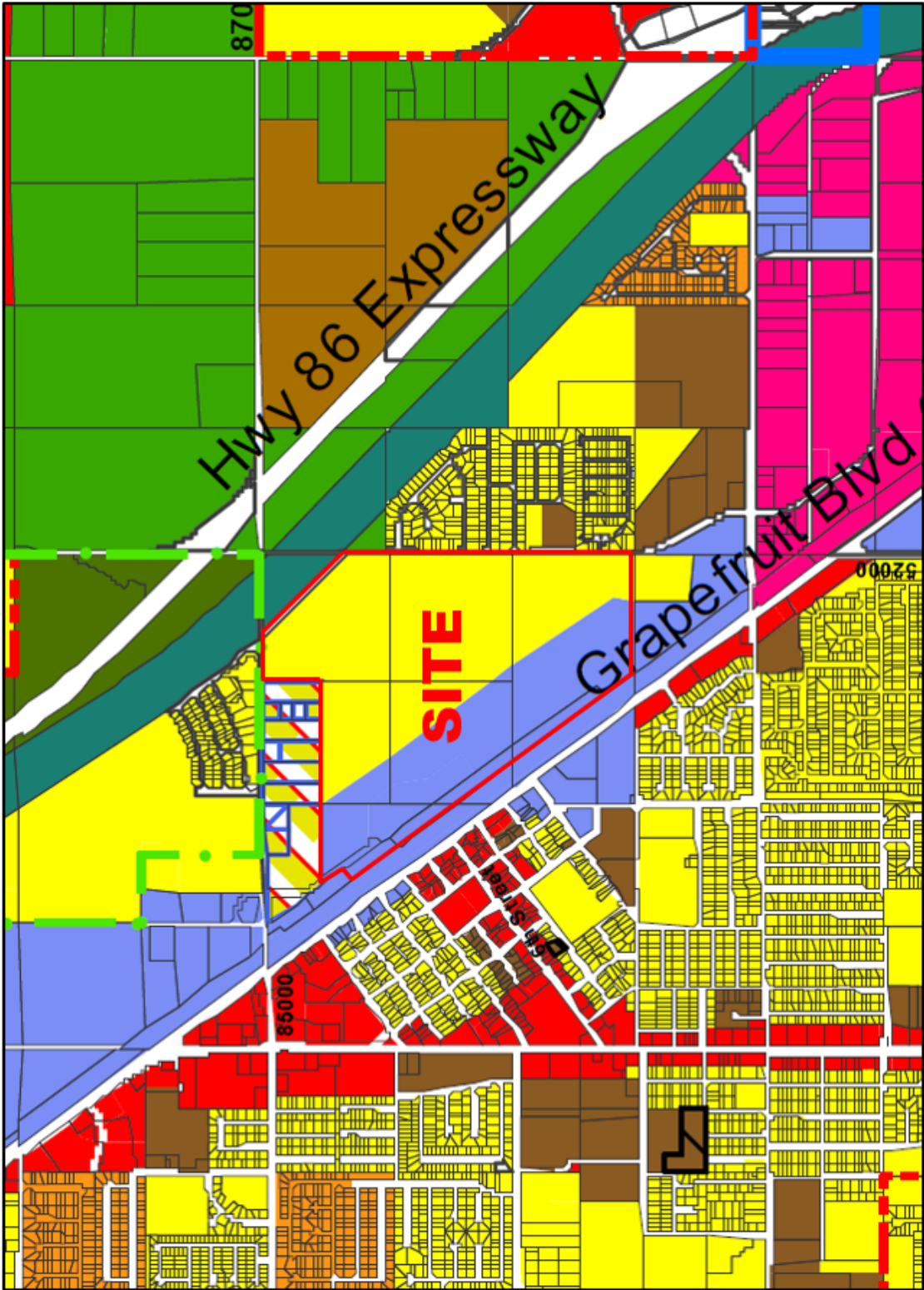
Exhibit F is provided to show the current General Plan designation, and **Exhibit G** is provided to show the current Zoning Districts (City of Coachella 2013 Zoning Map) which are inconsistent with the goals and policies of General Plan 2035 and Subarea 6.

EXHIBIT F



GENERAL PLAN 2035 – LAND USE ELEMENT

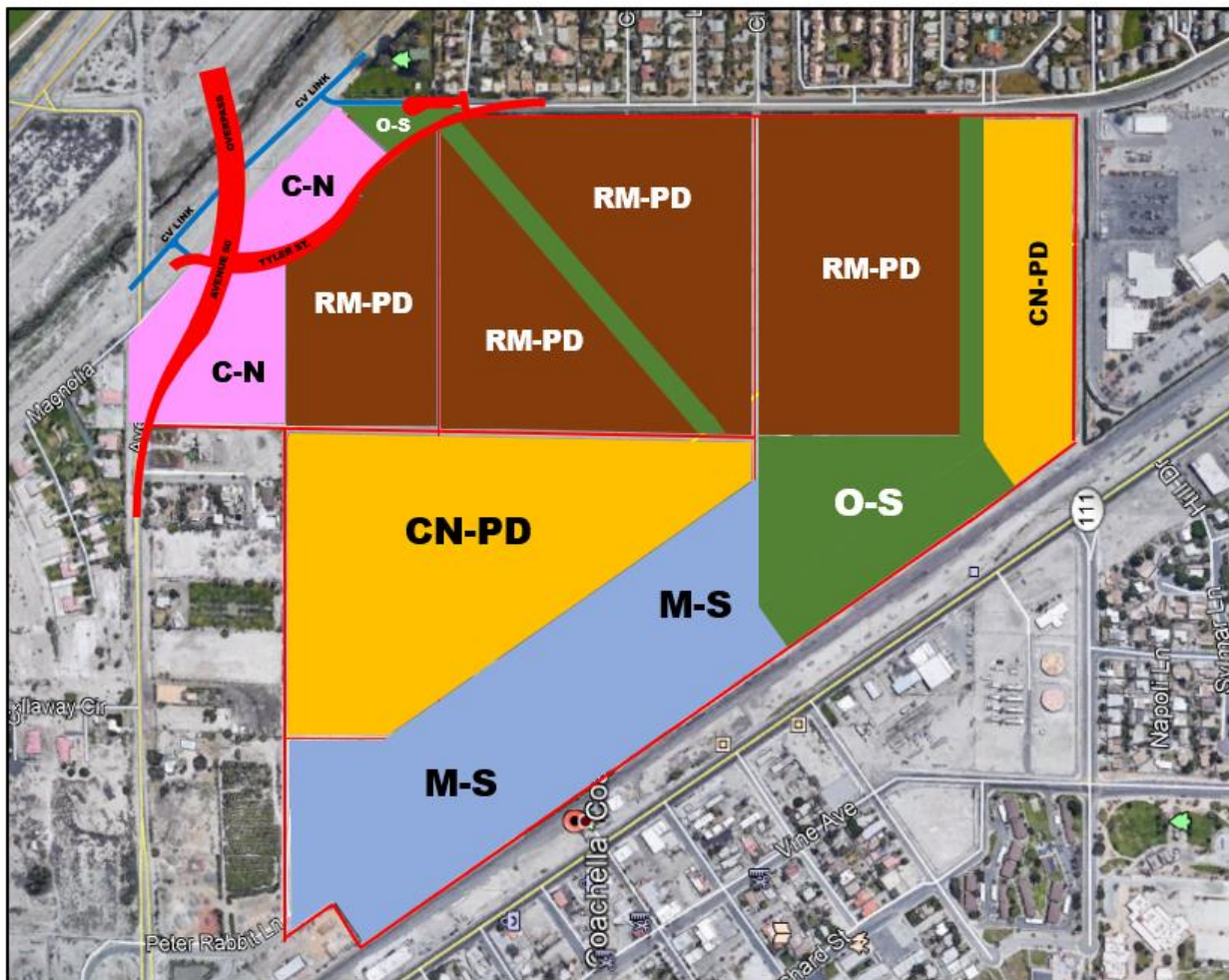
EXHIBIT G



CITY OF COACHELLA – 2013 ZONING MAP

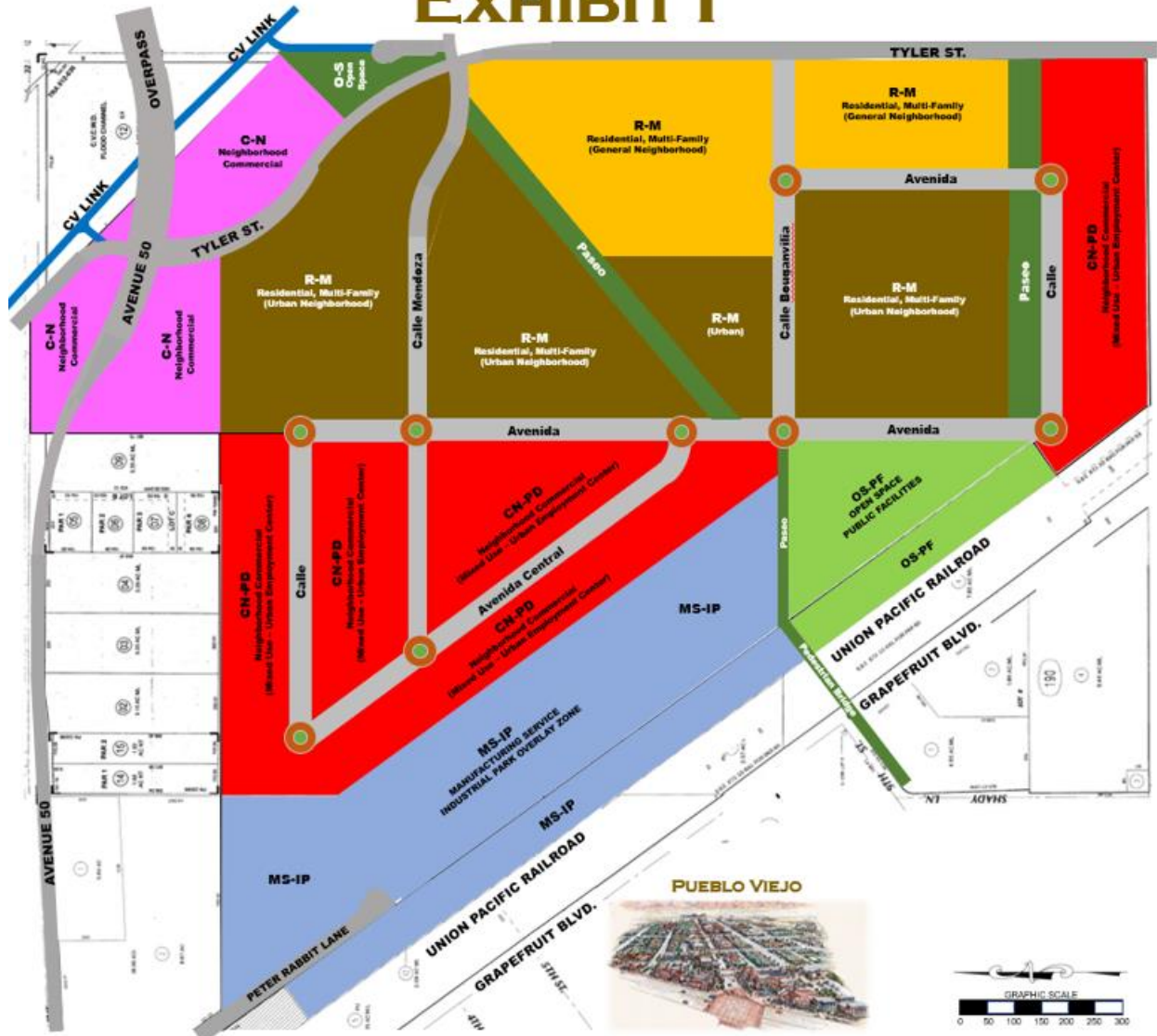
The current General Plan Land Use exhibit shows the entirety of the site to be within the “Urban Employment Center” designation. However, the SubArea 6 – Downtown Expansion policies provide further guidance for the appropriate zoning districts that should be used. Pursuant to these policies, this area will serve as the expansion of Downtown Coachella and it is envisioned to have a wide diversity of employment and civic uses. The area is envisioned as having small, walkable blocks with office, Research & Development, and supporting retail and services. Multi-family residential uses may be located on the edges of this area and adjacent to existing residential development, such as the area east of Tyler Street. This area would also be an ideal location for a college or university that could benefit from the proximity to Downtown and expected office and R&D uses and an orientation towards walkable, university campus-style development. The foregoing is graphically depicted in **Exhibit H – Proposed Zoning** and **Exhibit I – Conceptual Land Use Plan**, respectively.

EXHIBIT H



**CITY OF COACHELLA – PROPOSED ZONING
CONSISTENT WITH GENERAL PLAN 2035 LAND USE ELEMENT**

EXHIBIT I



CONCEPTUAL LAND USE PLAN

Environmental Setting:

As shown in **Exhibit B**, the project site is located south of Avenue 50, west of Tyler Street, approximately ¼ mile north of Avenue 52. The project site comprises Assessor Parcel Numbers 778-030-07, 12, 14; 778-170-10, 11; 778-180-02, 03, 04; and 778-191-01, 02, 03. The western portion of the project site is currently developed with a number of buildings and other facilities dedicated primarily to agriculture production, processing, and packing. The majority of the site is undeveloped and is generally flat due to many years of agricultural use.

Project Site Characteristics

Exhibit C presents the location of the project site from an aerial perspective. Project site photographs are provided. **Exhibit J, Project Site Photo Key** gives the location and direction of the Project Site Photos. **Exhibits J1 through J7, Project Site Photos**, provide a wide variety of locations and angles for viewing the project site.

As can be seen in **Exhibits J through J7**, the western portion of the site is currently occupied by a number of industrial buildings and associated facilities dedicated to agricultural production. However, the majority of the site is vacant as it has been in use as agricultural property. During discussions leading up the approval of General Plan 2035, this property was chosen specifically for its ability to support large, mixed use, urban employment, and residential development projects.

Project Vicinity Characteristics

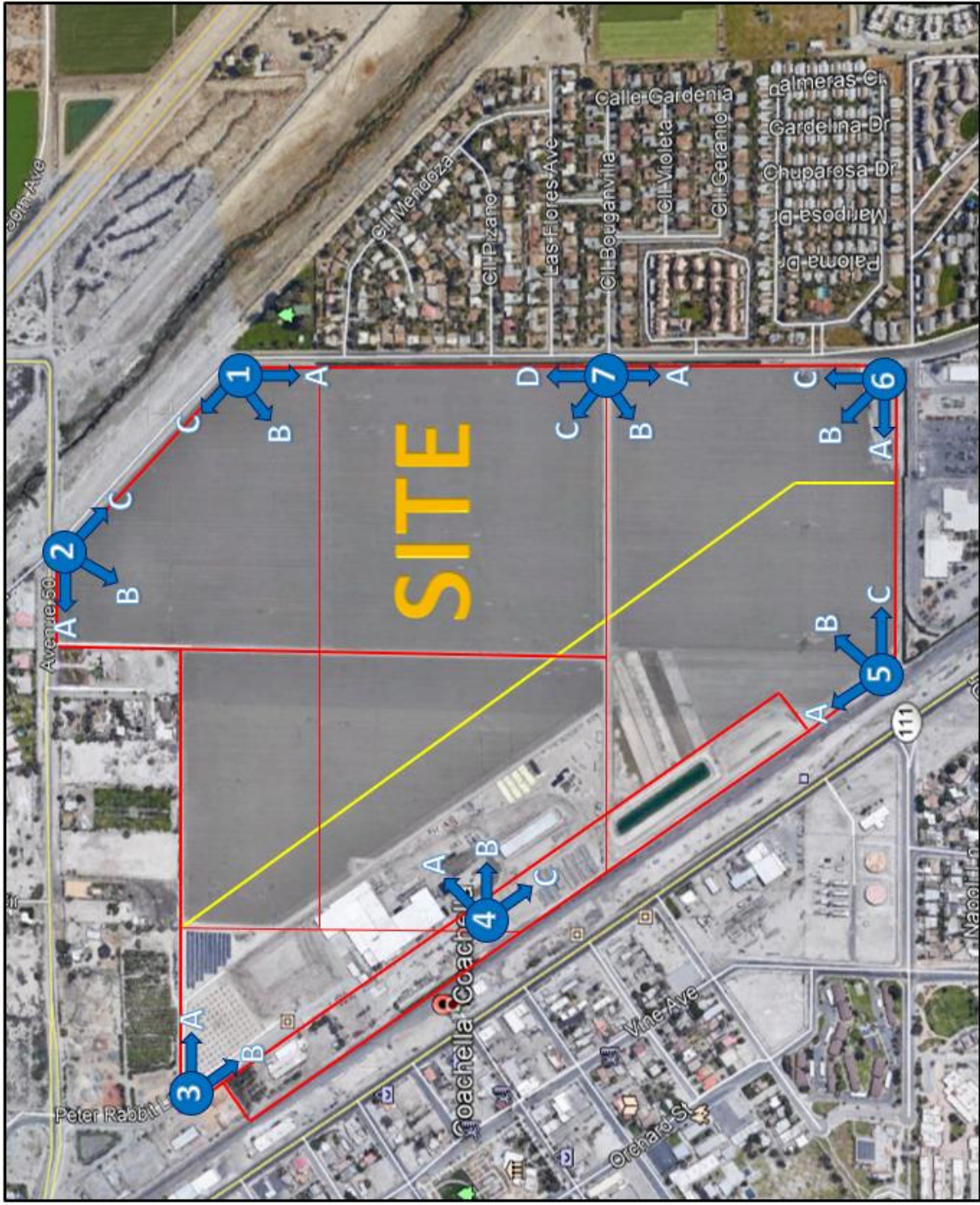
The following matrix characterizes land uses in the immediate project vicinity (See Exhibit C).

Direction from Project Site	Land Use
<i>North</i>	Large lot residential uses; Single family Homes (Cabazon Indian Reservation).
<i>South</i>	CVWD Maintenance Yard and Offices
<i>East</i>	Single Family Residential; Multi-Family Residential; Mobile Home Park
<i>West</i>	Union Pacific Railroad; Industrial Development; Grapefruit Blvd.

The trust lands of the Cabazon Band of Mission Indians, making up territories of the 29 Palms Band of Mission Indians, are located north of the project site along the north side of Avenue 50.

The photographs below show the current conditions of the property, which are largely used for seasonal row crop agriculture, and the agricultural packing plant located nearest the Union Pacific Railroad. The site is traversed by several IID overhead utility line easements and a hedgerow of oleander trees along the east side.

EXHIBIT J



SITE PHOTO KEY



Legend:
Indicates Location and
Direction of Photo

EXHIBIT J1

1A

View from Northeast
Corner of Site
Looking South



1B

View from Northeast Corner
of Site Looking Southwest



1C

View from Northeast Corner
of Site Looking Northwest

PROJECT SITE PHOTOS

EXHIBIT J2

2A

View from Northernmost Corner of Site Looking West



View from Northernmost Corner of Site Looking South

2B



View from Northernmost Corner of Site Looking Southeast

2C

PROJECT SITE PHOTOS

EXHIBIT J3

3A

View from Northwest
Corner of Site
Looking East



3B

View from Northwest
Corner of Site
Looking Southeast



PROJECT SITE PHOTOS

EXHIBIT J4

4A

View from 7th Street
Looking Northeast



View from 7th Street
Looking Easterly

4B



View from 7th Street
Looking Southeast

4C

PROJECT SITE PHOTOS

EXHIBIT J5

5A

View from Southwest
Corner of Site
Looking Northwest



View from Southwest Corner
of Site Looking Northeast

5B



View from Southwest Corner
of Site Looking East

5C

PROJECT SITE PHOTOS

EXHIBIT J6

6A

View from Southeast Corner of Site Looking West



View from Southeast Corner of Site Looking Northwest

6B



View from Southeast Corner of Site Looking North

6C

PROJECT SITE PHOTOS

7A

View from Calle Bouganvilia
Looking South



7B

View from Calle Bouganvilia
Looking Southwest



7C

View from Calle Bouganvilia
Looking Northwest



7D

View from Calle Bouganvilia
Looking North



EXHIBIT J7

PROJECT SITE PHOTOS

ENVIRONMENTAL REVIEW:

Environmental Assessment/Initial Study No. 18-04 was prepared for the subject project pursuant to the California Environmental Quality Act Guidelines and distributed to responsible agencies for review and comment. Based on this Environmental Assessment/Initial Study it has been determined that the project will not have a significant impact on the environment, and it is appropriate to recommend adoption of a Negative Declaration for this project. A 20-day review period for the proposed Mitigated Negative Declaration commenced on September 13, 2019 and will end on October 2, 2019 for interested and concerned individuals and public agencies to submit written comments on the project. The Environmental Assessment/Initial Study is attached to this report.

No significant issues were identified as part of the initial study and staff is recommending that the Planning Commission adopt the Negative Declaration prepared for the proposed project.

Staff received 3 written responses from the following agency regarding the proposed project and date received are listed below:

1. Riverside County Airport Land Use Commission (September 19, 2019)
2. 29 Palms Band of Cahuilla Indians (September 20, 2019)
3. Augustine Band of Cahuilla Indians (October 6, 2019)

The September 19, 2019 letter from Riverside County Airport Land Use Commission is advisory in nature and addresses no specific issue. A small portion of the extreme southern end of *Zona Central* falls within Compatibility Zone E of the Jacqueline Cochran Regional Airport Influence Area. Compatibility Zone E does not restrict residential density or non-residential intensity but does prohibit certain hazards to flight. Coachella General Plan 2035 incorporates the requirements of the ALUCP and CZ 18-10 is consistent with those requirements. Any deviation from current, approved development requirements will require trigger further discretionary review by the City and possible review by Riverside County ALUC.

The September 20, 2019 letter from the 29 Palms Band of Cahuilla Indians requested information regarding cultural resources investigations for the property. In consultation with the Tribe, staff explained that because no grading or construction is proposed at this time, there were no historic/cultural resources investigations conducted on the subject site. The Tribe understands that subsequent environmental assessments, for individual projects, will include these site investigations.

The October 6, 2019 letter from the Augustine Band of Cahuilla Indians does not request any clarification or further consultation on the project. The Tribe states in the letter that they are not aware any tribal sensitive cultural resources on the subject site. Therefore, there is no need to augment the public record in light of this letter.

CONCLUSIONS AND RECOMMENDATIONS:

Based on foregoing analysis of the project proposal, including a thorough review of the City of Coachella General Plan 2035 goals, policies, and recommendations, the proposed *Zona Central – Downtown Expansion* project represents an a significant movement toward creating a sustainable, mixed-use community that will provide a homes, places of employment, gathering spaces, and civic pride to the central core of the City of Coachella. This walkable, family-friendly environment will eventually provide thousands of Coachella residents with a place to live, work, shop, gather, and play within a new, environmentally friendly, and socially conscious atmosphere. *Zona Central* has the potential to meet the sustainability goals of the city by creating a community that achieves all three pillars of sustainable development:

- 1. Social Responsibility
- 2. Environmental Stewardship
- 3. Economic Viability

Accordingly, staff is recommending approval of this project.

ALTERNATIVES:

- 1. Approve the *Zona Central – Change of Zone* project.
- 2. Continue the *Zona Central – Change of Zone* project and provide staff with direction.
- 3. Take no action.

FISCAL IMPACT:

There are no fiscal impacts associated with the approval of the *Zona Central – Change of Zone* project alone, in that there is no construction proposed at this time and the new zoning on the properties will be brought into conformity with the General Plan and Housing Element, in response to State of California mandates, based on the Housing Elements required rezoning efforts for complying with the Regional Housing Needs Allocation (RHNA) capacity.

RECOMMENDED ALTERNATIVE(S):

Staff recommends Alternative #1 above.

Attachments: Resolution No.2019-57
 Ordinance No. 1143
 Negative Declaration
 Correspondence

RESOLUTION NO. 2019-57

A RESOLUTION OF THE CITY COUNCIL OF THE COACHELLA, CALIFORNIA ADOPTING A NEGATIVE DECLARATION FOR THE ZONA CENTRAL – DOWNTOWN EXPANSION PROJECT, PURSUANT TO CEQA GUIDELINES, CONSISTING OF CHANGE OF ZONE (CZ 18-10) FOR 206 ACRES LOCATED ON THE SOUTH SIDE OF AVENUE 50, WEST OF TYLER STREET. CITY-INITIATED.

WHEREAS, the Proposed Project, as set forth in Change of Zone No. 18-10 (CZ 18-10) consists of the above referenced application on approximately 206-acres of land located south of Avenue 50 and west of Tyler Street; and the Project includes the following requests: Change of Zone (CZ 18-10) to bring properties into full compliance with the General Plan’s “Land Use and Community Character Element”; and

WHEREAS, the Sub-Area 6 policies of the Coachella General Plan 2035 “Land Use and Community Character Element” includes a combination district that includes elements of both urban employment uses and very-high density residential uses; and,

WHEREAS, Prior to the approval of this Change of Zone (CZ 18-10), the project site contains only two zoning districts: M-S (Manufacturing Service) and R-S (Single Family Residential). Neither of these two zones provides the employment flexibility nor the variety of housing types envisioned by the General Plan; and,

WHEREAS, to bring the site into compliance with the General Plan, and to comply with the City’s Housing Element re-zoning requirements, the City of Coachella Development Services with the consent of the Property Owner proposes to create the following Zoning Districts:

- MS-IP; Manufacturing Service – Industrial Park
- C-N; Neighborhood Commercial
- CN-PD; Neighborhood Commercial Planned Development (Mixed Use, Urban Employment Center)
- R-M; Residential, Multi-Family (General Neighborhood)
- R-M; Residential, Multi-Family (Urban Neighborhood)
- O-S; Open Space
- OS-PF; Open Space Public Facilities; and,

WHEREAS, the City completed Environmental Assessment/Initial Study No. 18-04 for the Proposed Project pursuant to the California Environmental Quality Act, as amended; and

WHEREAS, based on this Environmental Assessment/Initial Study the City has made a determination that the Project will not have a significant impact on the environment and has prepared a Negative Declaration for this Project; and

WHEREAS, a Notice of Intent to Adopt a Negative Declaration for the Project was posted with the County Clerk and duly noticed and published in the Desert Sun Newspaper, a local newspaper of general circulation, on September 15, 2019; and

WHEREAS, the proposed Negative Declaration was made available for a 20-day public review period commencing on September 13, 2019 and ending on October 2, 2019; and

WHEREAS, interested and concerned individuals and public agencies had the opportunity to review and comment on the proposed Negative Declaration; and

WHEREAS, findings of the Initial Study indicated that the Proposed Project would not create any significant impacts to the environment; and

WHEREAS, the Coachella Planning Commission held a duly noticed public hearing on October 2, 2019 and made a finding of adequacy with the environmental documents; and,

WHEREAS, the Proposed Project would not be detrimental to the general health, safety and welfare of the community.

NOW, THEREFORE, be it resolved that the City Council has considered the Negative Declaration prepared for Change of Zone 18-10 (CZ 18-10) under Environmental Assessment No. 18-04 (EA 18-04), attached hereto, and has determined that the project would have no significant deleterious effect on the environment and orders that a Negative Declaration be adopted and filed pursuant to the California Environmental Quality Act, as amended, for CZ 18-10 for approximately 206-acres of land located south of Avenue 50 and west of Tyler Street further identified as Assessor Parcel Numbers 778-030-07, 12, 14; 778-170-10, 11; 778-180-02, 03, 04; and 778-191-01, 02, 03.

PASSED, APPROVED, AND ADOPTED this 23rd day of October 2019 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Steven A. Hernandez
Mayor

ATTEST:

Angela M. Zepeda
City Clerk

APPROVED AS TO FORM:

Carlos Campos
City Attorney

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF COACHELLA)

I HEREBY CERTIFY that the foregoing Resolution No. 2019-57 was duly adopted by the City Council of the City of Coachella at a regular meeting thereof, held on this 23rd day of October 2019 by the following vote of Council:

AYES:

NOES:

ABSENT:

ABSTAIN:

Andrea J. Carranza
Deputy City Clerk

ORDINANCE NO. 1143

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF COACHELLA APPROVING THE CITY-INITIATED CHANGE OF ZONE (CZ 18-10) FOR THE ZONA CENTRAL PROJECT FROM M-S (MANUFACTURING SERVICE) AND R-S (SINGLE FAMILY RESIDENTIAL) TO A MIXTURE OF MS-IP (MANUFACTURING SERVICE – INDUSTRIAL PARK), C-N (NEIGHBORHOOD COMMERCIAL), CN-PD (NEIGHBORHOOD COMMERCIAL - PLANNED DEVELOPMENT OVERLAY), R-M (MULTI-FAMILY RESIDENTIAL – GENERAL), R-M (MULTI-FAMILY RESIDENTIAL - URBAN), O-S (OPEN SPACE), AND OS-PF (OPEN SPACE - PUBLIC FACILITIES). PETER RABBIT FARMS, OWNER. (1st Reading)

WHEREAS, The City of Coachella, Development Services Department initiated and filed an application for Change of Zone 18-10 on approximately 206 acres of land located south of Avenue 50 and west of Tyler Street; Assessor Parcel Numbers 778-030-07, -12, -14; 778-170-10, -11; 778-180-02, -03, -04; and 778-191-01, -02, -03 (“Project”); and,

WHEREAS, the Planning Commission conducted a duly noticed public hearing on Change of Zone 18-10 on October 2nd, 2019 in the Coachella Permit Center, 53-990 Enterprise Way, Coachella, California; and,

WHEREAS, the landowner and members of the public were present and were afforded an opportunity to testify regarding the Project; and,

WHEREAS, the Proposed Project includes the following requests: Change of Zone (CZ 18-10) to bring properties into full compliance with the General Plan’s “Land Use and Community Character Element”; and,

WHEREAS, the General Plan’s Sub-Area 6 policies call for a combination district that includes elements of both employment and residential uses; and

WHEREAS, Prior to the approval of this Change of Zone (CZ 18-10), the project site contains only two zoning districts: M-S (Manufacturing Service) and R-S (Single Family Residential). Neither of these two zones provides the employment flexibility nor the variety of housing types envisioned by the General Plan; and

WHEREAS, to bring the site into compliance with the General Plan, and to comply with the City’s Housing Element re-zoning requirements, the City of Coachella Development Services with the consent of the Property Owner proposes to create the following Zoning Districts:

- MS-IP; Manufacturing Service – Industrial Park
- C-N; Neighborhood Commercial
- CN-PD; Neighborhood Commercial Planned Development (Mixed Use, Urban Employment Center)

- R-M; Residential, Multi-Family (General Neighborhood)
- R-M; Residential, Multi-Family (Urban Neighborhood)
- O-S; Open Space
- OS-PF; Open Space Public Facilities; and

WHEREAS, the Project is permitted pursuant to Chapter 17.80 of the Coachella Municipal Code, and the attendant application for a Change of Zone to allow the Project; and,

WHEREAS, the proposed use is necessary or desirable for the development of the community, is consistent with the objectives of the City’s General Plan, and is not detrimental to the existing uses or the uses specifically permitted in the zone in which the proposed use is to be located; and,

WHEREAS, the proposed site is adequate in size and shape to accommodate the proposed development; and,

WHEREAS, the City completed Environmental Assessment/Initial Study No. 18-04 for the Proposed Project pursuant to the California Environmental Quality Act, as amended; and

WHEREAS, findings of the Initial Study indicated that the Proposed Project will not create any significant impacts to the environment; and

WHEREAS, the Proposed Project would not be detrimental to the general health, safety and welfare of the community.

WHEREAS, the City Council conducted a duly noticed public hearing on Change of Zone No. 18-01 on October 23, 2019 in the Council Chambers, 1515 Sixth Street, Coachella, California to consider staff recommendations and prior written and oral testimony regarding the project and wherein the public was given an opportunity to testify; and,

WHEREAS, a Negative Declaration was previously prepared and adopted for the Zona Central Project pursuant to the California Environmental Quality Act, as amended.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA, DOES HEREBY ORDAIN AS FOLLOWS:

Section 1. That the City of Coachella Official Zoning Map be amended as shown on the attached Change of Zone 18-10 Map marked “Exhibit A” from M-S (Manufacturing Service) and R-S (Single Family Residential) To Mixture MS-IP (Manufacturing Service – Industrial Park), C-N (Neighborhood Commercial), CN-PD (Neighborhood Commercial - Planned Development Overlay), R-M (Multi-Family Residential – General), R-M (Multi-Family Residential - Urban), O-S (Open Space), And OS-PF (Open Space - Public Facilities) on approximately 206-acres of land located south of

Avenue 50 and west of Tyler Street, with the findings listed below.

Findings for Change of Zone 18-10:

1. The Project is consistent with the goals, objectives, policies, and implementation measures of the Coachella General Plan 2035. The site has an Urban Employment Center land use designation that is more specifically defined under the Subarea 6 – Downtown Expansion policies, that allows for the proposed development. The proposed change of zone is in keeping with the policies of the Urban Employment Center land use classification and the Project is internally consistent with other General Plan policies for this type of development.
2. The Project is in compliance with the applicable land use regulations and development standards of the City’s Zoning Code. The Project complies with applicable sections of the General Plan and Section 17.80 if the City of Coachella Municipal Code. notwithstanding the lack of on-site parking serving the building.
3. Every use, development of land and application of architectural guidelines and development standards shall be considered on the basis of the suitability of the site for a particular use or development intended, and the total development, including the prescribed development standards, shall be so arranged as to avoid traffic congestion, ensure the protection of public health, safety and general welfare, prevent adverse effects on neighboring property and shall be in accord with all elements of the general plan. The proposed change of zone is compatible with existing surrounding land uses that include commercial land uses.
4. The Project will be compatible with neighboring properties with respect to land development patterns and application of architectural treatments. Surrounding properties to the project site include industrial, commercial, and residential land uses. As such, the Project will be in keeping with the scale, massing, and aesthetic appeal of the existing area and future development.
5. The Project will not create any significant impacts to the environment based upon a complete and accurate reporting of the environmental impacts associated with the Project contained in the Negative Declaration, Initial Study, and administrative record. The Negative Declaration and the Initial Study have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Coachella’s Local CEQA Guidelines.

Section 2. SEVERABILITY. The City Council declares that, should any provision, section, paragraph, sentence or word of this ordinance be rendered or declared invalid by any final court action in a court of competent jurisdiction or by reason of any preemptive legislation, the remaining provisions, sections, paragraphs, sentences or words of this ordinance as hereby adopted shall remain in full force and effect.

Section 3. EFFECTIVE DATE. This ordinance shall take effect thirty (30) days after its

second reading by the City Council.

Section 4. CERTIFICATION. The City Clerk shall certify to the adoption of this Ordinance and shall cause it to be published and circulated in the City of Coachella.

ORDINANCE PASSED AND APPROVED on this 23rd day of October 2019.

Steven A. Hernandez, Mayor

ATTEST:

Angela M. Zepeda, City Clerk

APPROVED AS TO FORM:

Carlos Campos, City Attorney

I, Angela M. Zepeda, City Clerk, City of Coachella, California, certify that the foregoing Ordinance No. 1143 was adopted by the City Council at a regular meeting of the City Council held on the 23rd day of October 2019, and was adopted by the following vote:

AYES:

NOES:

ABSENT:

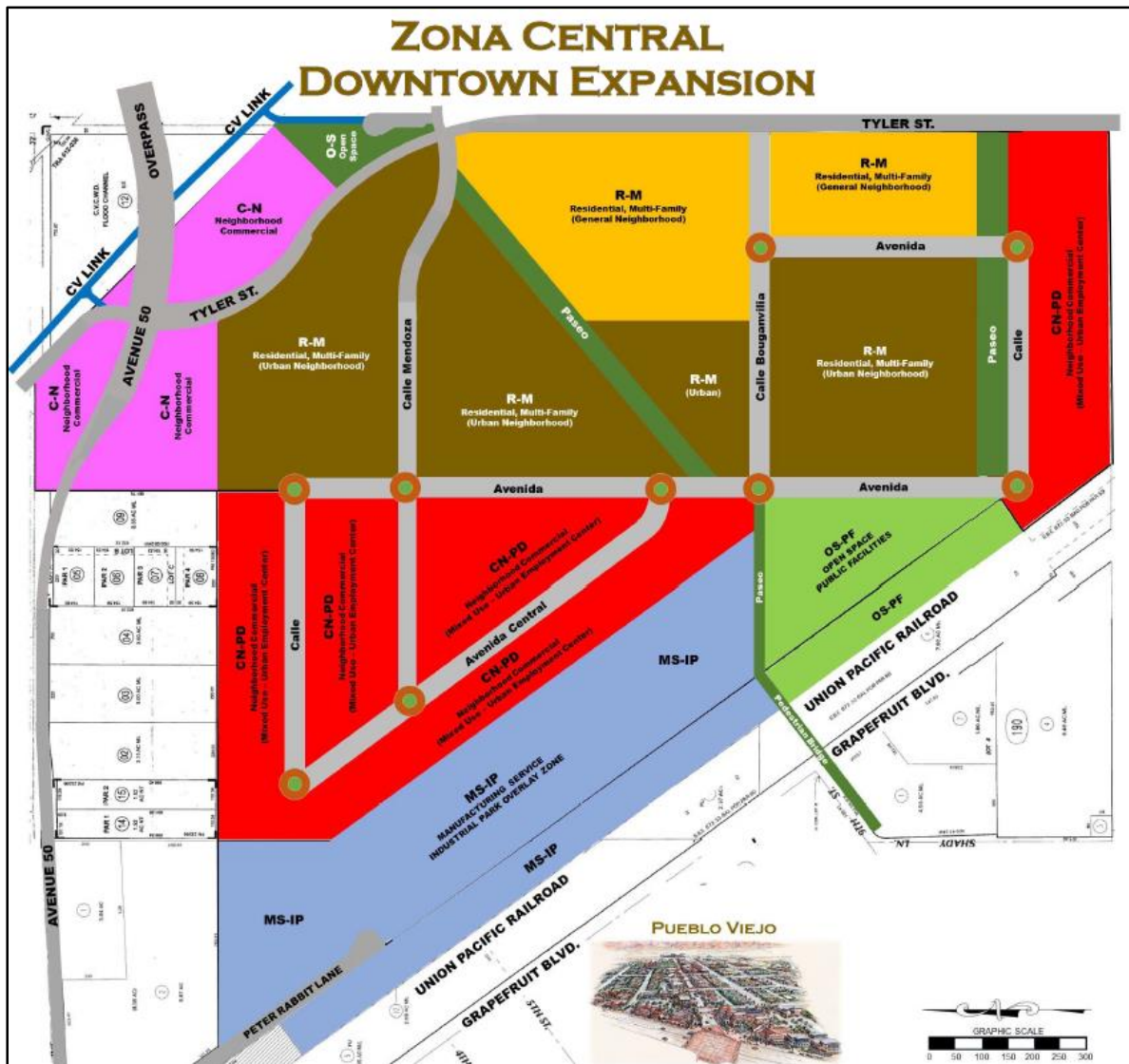
ABSTAIN:

Angela M. Zepeda, City Clerk

“Exhibit A”

Existing Zoning: M-S (Manufacturing Service) and R-S (Single Family Residential)

Proposed Zoning: Mixture MS-IP (Manufacturing Service – Industrial Park), C-N (Neighborhood Commercial), CN-PD (Neighborhood Commercial - Planned Development Overlay), R-M (Multi-Family Residential – General), R-M (Multi-Family Residential - Urban), O-S (Open Space), And OS-PF (Open Space - Public Facilities).



Final Initial Study and Negative Declaration for

ZONA CENTRAL; DOWNTOWN EXPANSION

CZ 18-10; EA 18-04

CITY OF COACHELLA, CA

Prepared for:

Lead Agency:



City of Coachella

Development Services Department
Luis Lopez, J.D., Director
53-990 Enterprise Way
Coachella, CA 92236

Property Owner:



Peter Rabbit Farms
Peter Rabbit Farms
John Powell, Jr., President
85-810 Peter Rabbit Lane
Coachella, CA 92236

Prepared by:



Arivitas Partners, LLC
Kevin L. Maevers, AICP
79-405 Highway 111, Unit 9, #462
La Quinta, CA 92253

October 2019



F.1 Introduction

This Final Initial Study and Negative Declaration (IS/ND) was prepared for the Zona Central – Downtown Expansion project. Prior to approving the project, the City Council as the lead agency must consider the proposed Negative Declaration (ND) together with any comments received during the public review process. The Council may adopt the proposed ND only if it finds on the basis of the whole record before it (including the Initial Study and any comments received), that there is no substantial evidence that the project will have a significant effect on the environment and that the ND reflects the lead agency’s independent judgment and analysis.

The City prepared this Final IS/ND in accordance with the California Environmental Quality Act (CEQA; California Public Resources Code Section 21000, et seq.) and the “Guidelines for the Implementation of the California Environmental Quality Act” (California Code of Regulations, Title 14, Section 15000, et seq., State CEQA Guidelines).

Organization of the Final IS/ND

The Final document includes the following information:

- F.1 Introduction to the Final IS/MND
- F.2 Comments received on the Draft IS/Notice of Intent and responses
- F.3 Errata

The Final document is available for review at the following location:

City of Coachella Permit Center
53-990 Enterprise Way
Coachella, CA 92236
Hours: 7:00 am to 6:00 pm, Monday through Thursday



F.2 Comments and Responses

Introduction

This section includes the comment letters received on the Draft Initial Study/Notice of Intent to Adopt a Negative Declaration. Each comment letter is labeled with a unique number and comments within each letter are numbered consecutively. For example, the letter from the Imperial Irrigation District (IID) is labeled as letter number 2 and the first comment in this letter is labeled 2-1.

The City of Coachella received a total of four (4) comment letters from state, regional and local agencies; as well as from special interest groups. The following list provides the name of the commenter along with his/her affiliation, and the date the letter was sent.

Comment Letters

Letter No. Author/Affiliation Date

1. Riverside County Airport Land Use Commission; September 19, 2019
2. Imperial Irrigation District (IID); September 25, 2019
3. Twenty-Nine Palms Band of Mission Indians; September 20, 2019
4. Agua Caliente Band of Cahuilla Indians; October 2, 2019
5. Riverside County Fire Department; September 24, 2019
6. Augustine Band of Cahuilla Indians; October 8, 2019



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



September 30, 2019

Mr. Luis Lopez, Development Services Director
City of Coachella Planning Department
53-990 Enterprise Way
Coachella CA 92236

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Simon Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW REQUIRED

Jurisdiction Project Case: CZ18-10

Dear Mr. Lopez:

Thank you for providing the Riverside County Airport Land Use Commission (ALUC) with a copy of the transmittal for the City of Coachella case; a proposal to rezone and bring into compliance with the City's General Plan approximately 206 acres called Zona Central (Downtown Expansion)

ALUC staff has determined that the project is located within Compatibility Zone E of Jacqueline Cochran Regional Airport Influence Area which does not restrict residential density or non-residential intensity, and prohibits hazards to flight.

} 1-1

California Public Utilities Code section 21676 requires the local agency to refer any amendment of a general plan or specific plan, or the adoption or approval of a zoning ordinance or building regulation within an Airport Land Use Compatibility Plan (ALUCP) to the ALUC. Additionally, California Public Utilities Code Section 21676.5 allows the ALUC to review all projects within the Airport Influence Area when the local jurisdiction's General Plan is not consistent with the applicable ALUCP. Since the General Plan is not consistent with the ALUCP and/or because the project contemplates amendment of a general plan or specific plan, or the adoption or approval of a zoning ordinance or building regulation, the ALUC requests that you submit the above-identified project(s) for its review. ALUC staff is also available to assist in bringing your jurisdiction's General Plan into consistency with the applicable ALUCP, if the local jurisdiction so desires.

} 1-2

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,

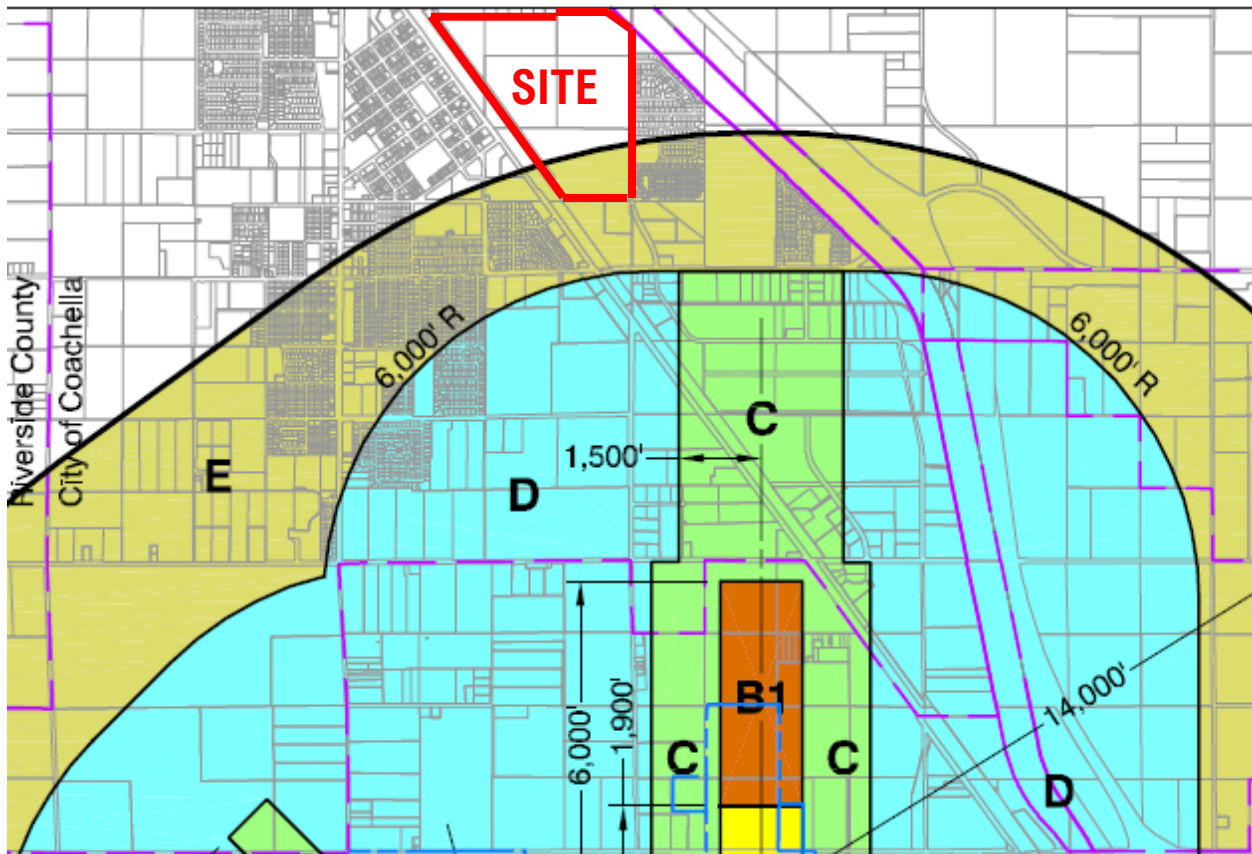
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Paul Rull, ALUC Principal Planner

Letter 1 Riverside County Airport Land Use Commission, Sept. 19, 2019

Response 1-1 The City of Coachella appreciates the concerns expressed by the Airport Land Use Commission. Prior to initiating this Change of Zone, City personnel reviewed the appropriate sections of the Riverside County Airport Land Use Compatibility Plan including that portion of Chapter 3 specific to Jaqueline Cochran Regional Airport. An additional review of City of Coachella General Plan 2035 and the supporting Environmental Impact report demonstrates that the Land Use Element of the City General Plan is in compliance with the Riverside County Land Use Compatibility Plan. However, it should be noted that less than 25% of the property contained within the Zona Central Change of Zone is within Zone E of the Airport Influence area (See Figure 1 below).

FIGURE 1: Portion of Jaqueline Cochran Regional Airport Airport Land Use Compatibility Map





Response 1-2 As the City General Plan 2035 is consistent with the ALUCP, and as this Change of Zone is intended only to bring the properties into compliance with the previously approved General Plan 2035 and supporting EIR, and as there is no proposed development plan attached to this request, further review by Airport Land Use Commission is not necessary at this time. Furthermore, as stated above, less than 25% of Zona Central falls within Compatibility Zone E which (pursuant to Table 2A of the Countywide Policies) does not restrict residential density or nonresidential intensity and prohibits hazards to flight. Current City Development policy prohibits construction of buildings over 100' tall or the construction of any sports stadiums, concert halls, amphitheaters, etc., without further development review (such as a Conditional Use Permit).



IID

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September 25, 2019

Mr. Luis Lopez
Director
Development Services Department
City of Coachella
1515 6th Street
Coachella, CA 92236

SUBJECT: Zona Central Project in Coachella, CA (CZ 18-10 & EA 18-04)

Dear Mr. Lopez:

On September 13, 2019, the Imperial Irrigation District received from the City of Coachella Development Services Department, a request for agency comments on the conceptual land use plan for the Zona Central project (Change of Zone No. 18-10, Environmental Assessment No. 18-04). The City of Coachella proposes to amend the official zoning map by changing the zoning on approximately 206.2 acres of vacant agricultural land and partially-developed land in the Manufacturing Service zone to provide consistency in the policies of the City of Coachella General Plan 2035 and specifically in its Land Use & Community Character Element and its Housing Element. The land is located south of Avenue 50, west of Tyler Street, approximately ¼ mile north of Avenue 52 in Coachella, CA.

The Imperial Irrigation District has reviewed the project information and has the following comments:

- 1. IID will not begin any engineering or estimate costs to provide electrical service for any project within the Zona Central area until the owner/developer submits a customer project application, detailed loading information, panel sizes, project schedule and estimated in-service date. The application is available for download at the district website <http://www.iid.com/home/showdocument?id=12923>. } 2-1
- 2. However, based on the information provided, IID has performed a preliminary assessment of the development area and has determined that there will be a substantial impact to the IID electrical system as the area develops; the cumulative impact of office, R&D, retail and services uses and possibly a college or university, in addition to multi-family residential uses potentially located on the edges of the Zona Central area, significantly increase the electrical demand on district's system. } 2-2
- 3. The environmental analysis in the Initial Environmental Study does not address, even conceptually, the level of adequacy of the existing electrical utility infrastructure to service the development area; neither in the *Project Description* section (Chapter 2, section 2.3) nor in the *Utilities and Services Systems* section (Chapter 3, section XVII) of the IES. Electrical service is a factor of utmost importance in the implementation and success of any new project. The deficient assessment of a project's potential impact on this resource } 2-3

IMPERIAL IRRIGATION DISTRICT • PO. BOX 937 • IMPERIAL, CA 92251



Luis Lopez
September 25, 2019
Page 2

could adversely affect the capability of the district to provide electrical service in an efficient and timely manner.

- 4. Chapter 3, section XVII, titled *Utilities and Services Systems*, of the IES should be amended to reflect a "Potentially Significant Impact" with the following mitigation measures:
 - a) Based on preliminary information received, it is anticipated that the additional power load for the development of the Zona Central area will require the acquisition, design and construction of a substation, transmission and distribution line extensions, distribution getaways and distribution feeders all of which shall be at the expense of the developer. A minimally-dimensioned substation site of 315 ft. by 315 ft. will be required at a location approved by IID. Additional requirements include grading, fencing, applicable permits, zoning change, landscaping (if required by the City), and all rights-of-way and easements for the substation site and access rights for both ingress and egress, as well as for power line facilities (transmission and distribution), all of which shall be at the expense of the developer.
 - b) Underground distribution line extensions will be required to serve the Zona Central area as well, including such items as all rights-of-way and easements for the line extensions and any line relocations or reconfigurations needed, all of which shall be at the expense of the developer.

- 5. Once the district is provided with the required information as stated in comment no. 1, IID can carry out a thorough assessment to determine the specific requirements to supply electrical service to a project. Likewise, IID will determine the availability of temporary construction power from existing power lines based on construction schedules and or phasing.

- 6. For additional information regarding electrical service for a project, the applicant should be advised to contact the IID Energy - La Quinta Division Customer Operations, 81-600 Avenue 58 La Quinta, CA 92253, at (760) 398-5841 and speak with the project development planner assigned to the area.

- 7. It is important to note that IID's policy is to extend its electrical facilities only to those developments that have obtained the approval of a city or county planning commission and such other governmental authority or decision-making body having jurisdiction over said developments.

- 8. The applicant will be required to provide rights-of-way and easements for any power line extensions needed to serve the project.

- 9. Line extensions are made in accordance with IID Regulations:
 - No. 2 (<http://www.iid.com/home/showdocument?id=2540>),
 - No. 13 (<http://www.iid.com/home/showdocument?id=2553>),
 - No. 15 (<http://www.iid.com/home/showdocument?id=2555>) and
 - No. 20 (<http://www.iid.com/home/showdocument?id=2560>).

- 10. Any construction or operation on IID property or within its existing and proposed right of way or easements including but not limited to: surface improvements such as proposed

} 2-4

} 2-5



Luis Lopez
September 25, 2019
Page 3

new streets, driveways, parking lots, landscape; and all water, sewer, storm water, or any other above ground or underground utilities; will require an encroachment permit, or encroachment agreement (depending on the circumstances). A copy of the IID encroachment permit application and instructions for its completion are available at <http://www.iid.com/departments/real-estate>. The IID Real Estate Section should be contacted at (760) 339-9239 for additional information regarding encroachment permits or agreements.

- 11. Relocation of existing IID facilities to accommodate the project and/or to accommodate street widening improvements imposed by the City will be deemed project-driven and all costs, as well as securing of rights of way and easements for relocated facilities, shall be borne by the applicant.
- 12. Public utility easements over all private and public roads and an additional ten (10) feet in width on both side of the private and public roads shall be dedicated to IID for the construction, operation, and maintenance of electrical infrastructure.
- 13. Any new, relocated, modified or reconstructed IID facilities required for and by the project (which can include but is not limited to electrical utility substations, electrical transmission and distribution lines, etc.) need to be included as part of the project's CEQA and/or NEPA documentation, environmental impact analysis and mitigation. Failure to do so will result in postponement of any construction and/or modification of IID facilities until such time as the environmental documentation is amended and environmental impacts are fully mitigated. **Any mitigation necessary as a result of the construction, relocation and/or upgrade of IID facilities is the responsibility of the project proponent.**
- 14. Dividing a project into two or more pieces and evaluating each piece in a separate environmental document (Piecemealing or Segmenting), rather than evaluating the whole of the project in one environmental document, is explicitly forbidden by CEQA, because dividing a project into a number of pieces would allow a Lead Agency to minimize the apparent environmental impacts of a project by evaluating individual pieces separately, each of which may have a less-than-significant impact on the environment, but which together may result in a significant impact. Segmenting a project may also hinder developing comprehensive mitigation strategies. In general, if an activity or facility is necessary for the operation of a project, or necessary to achieve the project objectives, or a reasonably foreseeable consequence of approving the project, then it should be considered an integral project component that should be analyzed within the environmental analysis. The project description should include all project components, including those that will have to be approved by responsible agencies. The State CEQA Guidelines define a project under CEQA as "the whole of the action" that may result either directly or indirectly in physical changes to the environment. This broad definition is intended to provide the maximum protection of the environment. CEQA case law has established general principles on project segmentation for different project types. For a project requiring construction of offsite infrastructure, the offsite infrastructure must be included in the project description. *San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus* (1994) 27 Cal.App. 4th 713.

} 2-5



Luis Lopez
September 25, 2019
Page 4

15. Applicant should be advised that landscaping can be dangerous if items are planted too close to IID's electrical equipment. In the event of an outage, or equipment failure, it is vital that IID personnel have immediate and safe access to its equipment to make the needed repairs. For public safety, and that of the electrical workers, it is important to adhere to standards that limit landscaping around electrical facilities. IID landscaping guidelines are available at <https://www.iid.com/energy/vegetation-management>.

} 2-5

Should you have any questions, please do not hesitate to contact me at (760) 482-3609 or at dvargas@iid.com. Thank you for the opportunity to comment on this matter.

Respectfully,

Donald Vargas
Compliance Administrator II

- Enrique B. Martinez – General Manager
- Mike Pecheco – Manager, Water Dept.
- Marilyn Del Bosque Gilbert – Manager, Energy Dept.
- Jamie Asbury – Deputy Manager, Energy Dept., Operations
- Charles Bery – Mgr., Energy Dept., Distr. Services & Maintenance Operations
- Enrique De Leon – Asst. Mgr., Energy Dept., Distr., Planning, Eng. & Customer Service
- Vanda Taylor – Asst. General Counsel
- Robert Laurie – Outside Counsel
- Michael F. Kemp – Superintendent, Regulatory & Environmental Compliance
- Laura Cervantes – Supervisor, Real Estate



Letter 2 Imperial Irrigation District, September 25, 2019

- Response 2-1 The City of Coachella remains sympathetic to the concerns expressed herein. However, many of the concerns expressed are not appropriate for a CEQA review. As IID personnel are aware, the City of Coachella, and a wide section of the property owners in the vicinity of this project, as well as on projects to the north and south, are actively working toward a solution, including the development of 120MV substation(s) to resolve any potential electrical service issues caused by future demand.
- Response 2-2 While the City of Coachella agrees that there is the potential for increased systemwide demands as development occurs in this area, this current project (CZ 18-10) is intended solely to bring the property into compliance with the previously approved General Plan 2035. No development permits are being requested at this time. Additionally, due to the continuing innovation in both building efficiency and electrical power generation, it is impractical to perform an energy needs assessment at this time as any such assessment will not take into account such efficiencies and innovations and will thereby result in a negatively skewed result on systemwide demands.
- Response 2-3 As the City has previously expressed to IID on other projects, the City of Coachella has the following concerns with IID's request at this time.
- A. The CEQA environmental review process is not intended to address the adequacy of existing utility infrastructure. A review under CEQA addresses the "environmental effects" of a project. Should the project result in the need for a major overhaul of any utility system, or require new infrastructure to be installed, those impacts would be covered under other environmental factors (cultural resources, air quality, noise, etc.). What is important is the detailed project description that identifies the utility infrastructure needs of the project.
 - B. Adding a new layer of analysis for our project developers, looking into the project's "environmental effects" upon the electrical utility system, is unproductive and would require significant expert opinion and new costs to the CEQA process. For example, to fully understand and analyze the need for expanded or enlarged infrastructure needs for a project, your own engineers may be required to give the CEQA consultant detailed information about the circuits in the region and the overall capacity of the system. This information, once disclosed to the public, may cause unintended effects on land speculation and development investor activity, which is not in the City's best interest. Experience has demonstrated that IID typically does not disclose this information to the public.
- Response 2-4 As previously stated in Response 2-2, the project as it is currently proposed (CZ 18-10) intends only to bring the subject properties into compliance with the City of Coachella General Plan 2035. There is no development proposal under consideration at this time. It is anticipated that any future development application in this area will adhere to all requirements of the City of Coachella and IID, including the utilization of building and energy efficiency standards in effect at the time of development. As such, IID's



recommendation at this time is premature and may reflect an overly conservative point of view toward future development needs.

Response 2-5 All other comments (5 – 15) are advisory in nature and may, or may not, be suitable for future development projects as those projects come online.



TWENTY-NINE PALMS BAND OF MISSION INDIANS

46-200 Harrison Place . Coachella, California . 92236 . Ph. 760.863.2444 . Fax: 760.863.2449

September 20, 2019

Luis Lopez, Development Services Director
City of Coachella – Development Services Department
1515 Sixth Street
Coachella, CA 92236

RE: Request to Consult on the Zona Central Re-Zoning Project in Coachella, California

Dear Mr. Lopez:

This letter in regards to a Notice of Intent to Adopt a Negative Declaration for a Change of Zone No. 18-10. This Change of Zone entails an amendment to the Official Zoning Map by changing the zoning on approximately 206.2 acres of vacant agricultural land in the R-S (Residential Single-Family) and partially-developed land in the M-S (Manufacturing Service) zone to provide consistency with the policies of the City of Coachella General Plan 2035.

After review of the project area the Tribal Historic Preservation Office (THPO), established to protect and preserve cultural resources that are within the ancestral territory of the Chemehuevi, has some concerns for the project. The project is within the Chemehuevi Traditional Use Area (TUA) and in the vicinity of known cultural resources. For this reason, the While portions of the project may have been disturbed, the THPO requests the completed cultural report for this project. After review of this report, the THPO may provide additional recommendations.

} 3-1

If you have any questions, please do not hesitate to contact the Tribal Historic Preservation Office at (760) 775-3259 or by email: TNPConsultation@29palmsbomi-nsn.gov.

Sincerely,

Anthony Madrigal, Jr.
Director of the Tribal Historic Preservation Office



Letter 3 Twenty-Nine Palms Band of Mission Indians, September 20, 2019

Response 3-1 The City of Coachella remains sympathetic and shares many of the concerns expressed herein with respect to the sensitive nature of tribal cultural resources. The City of Coachella remains committed to a positive and proactive relationship with the Twenty-Nine Palms Band of Mission Indians. As such, we appreciate the response provided and understand the concerns expressed herein relative to the project being within the ancestral territory of the Chemehuevi.

Because of the nature of this project (Change of Zone for General Plan compliance) and because there are no earth-moving or other development activities proposed at this time, a cultural resources survey was not prepared as there is no intent to further disturb the earth beyond those activities that are currently taking place on the site. Future development projects, whether residential, commercial, or industrial, will be required to fully comply with local, regional, and state environmental laws and regulations (including CEQA Compliance). In order to ensure that the Twenty-Nine Palms Band of Mission Indians are well-informed during the development process for the future project, the City agrees to send a copy of any future cultural reports related to the development of the site to Anthony Madrigal, Jr., the Tribal Historic Preservation Officer.



AGUA CALIENTE BAND OF CAHUILLA INDIANS

TRIBAL HISTORIC PRESERVATION



03-017-2019-006

October 02, 2019

[VIA EMAIL TO: llopez@coachella.org]

City of Coachella
Mr. Luis Lopez
1515 Sixth Street
Coachella, CA 92236

Re: Zona Central

Dear Mr. Luis Lopez,

The Agua Caliente Band of Cahuilla Indians (ACBCI) appreciates your efforts to include the Tribal Historic Preservation Office (THPO) in the Zona Central project. The project area is not located within the boundaries of the ACBCI Reservation. However, it is within the Tribe's Traditional Use Area. Since this action does not have the potential to impact cultural resources, we have no concerns at this time. This letter shall conclude our consultation efforts.

} 4-1

Again, the Agua Caliente appreciates your interest in our cultural heritage. If you have questions or require additional information, please call me at (760)699-6956. You may also email me at ACBCI-THPO@aguacaliente.net.

Cordially,

Lacy Padilla
Archaeologist
Tribal Historic Preservation Office
AGUA CALIENTE BAND
OF CAHUILLA INDIANS

5401 DINAH SHORE DRIVE, PALM SPRINGS, CA 92264
T 760/699/6800 F 760/699/6924 WWW.AGUACALIENTE-NSN.GOV

**Letter 4 Agua Caliente Band of Cahuilla Indians, October 2, 2019**

Response 4-1 The City of Coachella appreciates the timely response and remains committed to a positive and proactive relationship with the Agua Caliente Band of Cahuilla Indians.

The letter confirms that the project area is not located within the boundaries of the ACBCI Reservation. However, it is within the Tribe's Traditional Use Area. Since this action does not have the potential to impact cultural resources, the Agua Caliente Band has no concerns at this time and the letter serves to conclude the consultation efforts. To ensure that the Agua Caliente Band of Cahuilla Indians are well-informed during the development process for the future project(s), the City agrees to send a copy of any future cultural reports related to the development of the site to the Tribal Historic Preservation Office.



CAL FIRE – RIVERSIDE UNIT RIVERSIDE COUNTY FIRE DEPARTMENT

Shawn C. Newman - Fire Chief
Office of the County Fire Marshal (East)
77-933 Las Montañas Rd., Ste. #201, Palm Desert, CA 92211-4131
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- JEFF HEWITT
DISTRICT 5

Date: September 24, 2019
City Case Number: Change of Zone #18-10, Environmental Assessment #18-04
Project Name: Zona Central
Requested By: Luis Lopez, Development Services Director
Reviewed By: Chris Cox, Assistant Fire Marshal
Fire Department Permit Number: FPCZC1900001
 East Office of the Fire Marshal Responsibility

It is the responsibility of the recipient of these Fire Department comments to forward them to all interested parties. The permit number (**as it is noted above**) is required on all correspondence.

Additional information is available at our website: www.rvcfire.org

Questions should be directed to the Riverside County Fire Department, Office of the Fire Marshal at 77933 Las Montañas Suite 201, Palm Desert, CA 92211 (760) 863-8886.

With respect to the conditions of approval for the referenced project, the Fire Department requires the following fire protection measures be provided in accordance with Riverside County Ordinances and/or recognized fire protection standards:

1. Strategic Planning Review: This planning case will also be reviewed by Riverside County Fire Department Strategic Planning for the cumulative impact on the Fire Department's ability to provide an acceptable level of service. Additional requirements may be conditioned by Strategic Planning to mitigate these impacts. Questions for Strategic Planning can be addressed to RRUStrategicPlanningBureau@fire.ca.gov

} 5-1

These conditions are preliminary and further review will occur upon receipt of construction plans. Additional requirements may be required based upon the adopted codes at the time of submittal.

Should you have any questions, or if some items are unclear, please phone our office at 760-863-8886 and speak with Assistant Fire Marshal Chris Cox to assist you with these conditions.



Letter 5 Riverside County Fire Department, September 24, 2019

Response 5-1 The City of Coachella appreciates the response from Riverside County Fire Department. The City is committed to working with Riverside County Fire to create a safe and healthy community for all residents. As this current project does not involve any proposal for new development, the information provided is advisory in nature. When a proposal for a development project is submitted to the City for review, the City will route the proposed project to Riverside County Fire Department's Strategic Planning Office for further review and comment.



AUGUSTINE BAND OF CAHUILLA INDIANS

PO Box 846 84-481 Avenue 54 Coachella CA 92236

Telephone: (760) 398-4722

Fax (760) 369-7161

Tribal Chairperson: Amanda Vance

Tribal Vice-Chairperson: William Vance

Tribal Secretary: Victoria Martin

October 8, 2019

Luis Lopez
City of Coachella
Development Services Department
1515 Sixth Street
Coachella, CA 92236

**RE: Project Title/Location: Zona Central- Downtown Expansion
South of Avenue 50, West of Tyler Street, Approximately ¼ north of Avenue 52
In the City of Coachella, CA**

Dear Mr. Lopez-

Thank you for the opportunity to offer input concerning the development of the above-identified project. We appreciate your sensitivity to the cultural resources that may be impacted by your project, and the importance of these cultural resources to the Native American peoples that have occupied the land surrounding the area of your project for thousands of years. Unfortunately, increased development and lack of sensitivity to cultural resources has resulted in many significant cultural resources being destroyed or substantially altered and impacted. Your invitation to consult on this project is greatly appreciated.

6-1

At this time, we are unaware of specific cultural resources that may be affected by the proposed project, however in the event you should discover any cultural resources during the development of this project please contact our office immediately for further evaluation.

Very truly yours,

Augustine Band of Cahuilla Indians

Victoria Martin
Tribal Secretary

**Letter 6 Augustine Band of Cahuilla Indians, October 8, 2019**

Response 6-1 The City of Coachella appreciates the timely response and remains committed to a positive and proactive relationship with the Augustine Band of Cahuilla Indians.

The letter confirms that the Augustine Band is unaware of any specific cultural resources located within the boundaries of proposed project. However, to ensure that the Augustine Band of Cahuilla Indians are well-informed during the development process for the future project(s), the City agrees to send a copy of any future cultural reports related to the development of the site to the Tribal Historic Preservation Office.



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F.3 Errata

Minor revisions have been made to the Draft Initial Study/Notice of Intent to Adopt a Negative Declaration for grammar, punctuation, spelling readability, and consistency.

These minor revisions do not constitute new significant information that would require recirculation of the Draft Initial Study/Notice of Intent. Revisions are included in the Revised Final IS/ND. All revisions to the Draft IS/ND are done with new text being underlined and ~~deleted text stricken through~~.

In accordance with Section 15073.5(a) of the CEQA Guidelines, a lead agency is required to recirculate a Draft IS/ND when the document must be substantially revised after public notice of its availability has previously been given.

A substantial revision is either: a new, avoidable significant effect is identified and mitigation measures or project revisions must be added in order to reduce the effect to insignificance; or the lead agency determines that the proposed mitigation measures or project revisions will not reduce potential effects to less than significance and new measures or revisions must be required.

Recirculation is not required if (1) mitigation measures are replaced with equal or more effective measures; (2) new project revisions are added in response to written or verbal comments on the project's effects identified in the proposed negative declaration which are not new avoidable significant effects; (3) measures or conditions of project approval are added after circulation of the Draft IS/ND which are not required by CEQA, which do not create new significant environmental effects and are not necessary to mitigate an avoidable significant effect; or (4) new information is added to the Draft IS/ND which merely clarifies, amplifies, or makes insignificant modifications to the Draft IS/ND.

Initial Environmental Study and Negative Declaration for

ZONA CENTRAL; DOWNTOWN EXPANSION

CZ 18-10; EA 18-04

CITY OF COACHELLA, CA

Prepared for:

Lead Agency:



City of Coachella

Development Services Department

Luis Lopez, J.D., Director

53-990 Enterprise Way

Coachella, CA 92236

Property Owner/Applicant:



Peter Rabbit Farms

John Powell, Jr., President

85-810 Peter Rabbit Lane

Coachella, CA 92236

Prepared by:



Arivitas Partners, LLC

Kevin L. Maevers, AICP

79-405 Highway 111, Unit 9, #462

La Quinta, CA 92253

September 2019



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Additional Exhibits

1. Conceptual Land Use Plan - 200' Scale (24" x 36")
2. Conceptual Land Use Plan - Reduced Scale (11"x17")
3. Conceptual Land Use Plan - Reduced Scale (8½" x11")

Acronyms

AOC	Area of Concern
AQMP	Air Quality Management Plan
bsl	below sea level
CAAQS	California Ambient Air Quality Standards
CACP	Clean Air and Climate Protection
CalEEMod	California Emissions Estimator Model
CalEPA	California Environmental Protection Agency
CalOSHA	California Occupational Health and Safety Administration
CAP	Climate Action Plan
CBC	California Building Code
CDL	Clandestine Drug Labs
CEQA	California Environmental Quality Act
CH ₄	Methane
CNEL	Community Noise Equivalent Level
CO	Carbon monoxide
CO ₂	Carbon dioxide
CSD	Coachella Sanitary District
CUP	Conditional Use Permit
CVMSHCP	Coachella Valley Multiple Species Habitat Conservation Plan
CVWD	Coachella Valley Water District
CWA	Coachella Water Authority
dBA	A-weighted decibel
DTSC	Department of Toxic Substances Control
EDR	Environmental Data Resources, Inc.
EIR	Environmental Impact Report
EPA	Environmental Protection Agency
ESA	Environmental Site Assessment
FEMA	Federal Emergency Management Agency
FIRM	Flood Insurance Rate Maps
GHG	Greenhouse Gasses
gpd	gallons per day
gpm	gallons per minute
LOS	Level of Service
LST	Localized Significance Threshold
LUST	Leaking Underground Storage Tank
MBTA	Migratory Bird Treaty Act
Mgd	Million gallons per day
MMT	Million metric tons
MND	Mitigated Negative Declaration
mph	miles per hour
MRZ	Mineral Resources Zone
MS4	Municipal Separate Storm Sewer Systems
N ₂ O	Nitrous oxides
NAAQS	National ambient air quality standards
ND	Negative Declaration



NOx	Nitrogen oxides
NOI	Notice of Intent
NPDES	National Pollutant Discharge Elimination System
O ₃	Ozone
O&M	Operations and Maintenance
Pb	Lead
PM _{2.5}	Particulate matter equal to or less than 2.5 microns in diameter
PM ₁₀	Particulate matter equal to or less than 10 microns in diameter
RCFD	Riverside County Fire Department
RCRA	Resource Conservation and Recovery Act
REC	Recognized Environmental Conditions
ROG	Reactive Organic Gasses
RWQCB	Regional Water Quality Control Board
SCAG	Southern California Association of Governments
SCAQMD	Southern California Air Quality Management District
SFL	Sacred Lands File
SIP	State Implementation Plan
Sox	Sulfur oxides
SRA	Source Receptor Area
SSAB	Salton Sea Air Basin
SWPPP	Stormwater Pollution Prevention Plan
TACs	Toxic Air Contaminants
µg/m ³	Micrograms per cubic meter
USEPA	US Environmental Protection Agency
VOC	Volatile Organic Compounds
VEC	Vapor Encroachment Conditions
WDS	Waste Discharge System
WQMP	Water Quality Management Plan
WWTP	Wastewater Treatment Plant



Chapter 1 Introduction

1.1 Overview

The City of Coachella (Lead Agency), together with Peter Rabbit Farms (Property Owner), has prepared this application for Change of Zone to bring approximately 206-acres of land located south of Avenue 50 and west of Tyler Street into conformance with the City of Coachella General Plan. The current General Plan Land Use designation for the property is Subarea 6 – Downtown Expansion which provides for a variety of employment and housing opportunities. The current zoning designation for most of the property is M-S (Manufacturing Service) or R-S (Residential Single Family). When approved, this Zone Change will bring 206 acres of land into compliance with the General Plan and provide significant opportunities for a variety of employment and housing types. The working name for this project is Zona Central - Downtown Expansion (See Chapter 2 – *Project Description* for further detail.)

1.2 Authority

The City of Coachella is the lead agency for the proposed Project. The City Council is the governing body for the approval of the proposed Project and adoption of the Negative Declaration (ND). Because the proposed Project involves a change to the existing site, the City Council's consideration of the Project and its potential environmental effects is a discretionary action that is subject to the California Environmental Quality Act (CEQA). This Initial Study (IS) has been prepared in accordance with the CEQA (Statute) and the State's Guidelines for Implementation of CEQA (Guidelines) (as amended, 2018). This IS, when combined with the Notice of Intent (NOI) to Adopt a ND, serves as the environmental document for the proposed Project pursuant to the provisions of CEQA (Public Resources Code 21000 et seq.) and the CEQA Guidelines (California

1.3 Scope of Environmental Review

The Initial Study evaluates the proposed project's potential environmental effects on the following topics:

- Aesthetics
- Agricultural Resources
- Air Quality
- Biological Resources
- Cultural Resources
- Geology and Soils
- Greenhouse Gas Emissions
- Hazards/Hazardous Materials
- Hydrology/Water Quality
- Mandatory Findings of Significance
- Land Use/Planning
- Mineral Resources
- Noise
- Population/Housing
- Public Services
- Recreation
- Transportation/Traffic
- Tribal Cultural Resources
- Utilities/Service Systems

1.4 Impact Assessment Terminology

The Environmental Checklist identifies impacts using four levels of significance as follows:

- **No Impact.** A finding of no impact is made when it is clear from the analysis that the project would not affect the environment.



- Less than significant. A finding of less than significant is made when it is clear from the analysis that a project would cause no substantial adverse change in the environment and no mitigation is required.
- Less than significant with mitigation incorporated. A finding of less than significant with mitigation incorporated is made when it is clear from the analysis that a project would cause no substantial adverse change in the environment when mitigation measures are successfully implemented by the project proponent.
- Potentially Significant. A finding of potentially significant is made when the analysis concludes that the proposed project could have a substantially adverse impact on the environment related to one or more of the topics listed in the previous section, *Scope of the Initial Study*.

1.5 Organization of the Initial Study

The content and format of the Initial Study meet the requirements of CEQA. The Initial Study contains the following sections:

- Chapter 1 Introduction. This chapter provides a brief summary of the proposed project, identifies the lead agency, summarizes the purpose and scope of the Initial Study, and provides a discussion of the impact terminology used to assess potential environmental impacts of the proposed project.
- Chapter 2 Project Description. This chapter provides a Project overview including a description of the regional location and Project vicinity, including Exhibits; and provides a description of the Project elements, e.g. dimensions of the Project, and identifies other agencies that may have permitting authority over the Project.
- Chapter 3 Environmental Checklist. This chapter provides a copy of the City's Environmental Checklist, revised to include the latest amendments to the CEQA Guidelines and responses to each question posed in the checklist. This chapter also provides a brief description of existing conditions for each topic and an analysis of potential environmental impacts. Mitigation measures are also identified where necessary.
- Chapter 4 References. This chapter lists all reports used, websites accessed, and persons consulted to prepare the Initial Study.

1.6 Documents Incorporated by Reference

As allowed by CEQA Guidelines Section 15150, a ND may incorporate by reference all or portions of another document that is generally available to the public. The document used must be available for public review for interested parties to access during public review of the IS and NOI to Adopt a Mitigated Negative Declaration for this Project. The following documents are incorporated by reference.

- City of Coachella General Plan Update (Adopted April 22, 2015).
- City of Coachella General Plan EIR (June 25, 2016).
- City of Coachella Municipal Code (online content updated on April 11, 2018).

These documents are also available for review at the City's Development Services Department, located at 1515 Sixth Street, Coachella, CA. The Project specific reports, if any, are attached to this IS as appendices. The List of Documents incorporated by reference are located on the City's website at:

<http://www.coachella.org/departments/general-plan-2035>
<https://cityofcoachellageneralplanupdate.weebly.com/final-eir.html>
<https://library.municode.com/ca/coachella>



Chapter 2 Project Description

2.1 Project Location

The City of Coachella is located on the easterly end of the Coachella Valley in Riverside County (Please see **Figure 1, Regional Location**). As shown in **Figure 2, Project Vicinity**, the project site is located south of Avenue 50, west of Tyler street, approximately ¼ mile north of Avenue 52. The project site comprises Assessor Parcel Numbers 778-030-07, 12, 14; 778-170-10, 11; 778-180-02, 03, 04; and 778-191-01, 02, 03. The western portion of the project site is currently developed with a number of buildings and other facilities dedicated primarily to agriculture production, processing, and packing. The majority of the site is undeveloped and is generally flat due to many years of agricultural use.

Project Site Characteristics

Figure 3, Project Site Google Earth Image, presents the location of the project site from an aerial perspective. Project site photographs are provided. **Figure 4, Project Site Photo Key** gives the location and direction of the Project Site Photos. **Figures 5.1 through 5.7, Project Site Photos**, provide a wide variety of locations and angles for viewing the project site.

As can be seen in **Figures 3 through 5.7**, the western portion of the site is currently occupied by a number of industrial buildings and associated facilities dedicated to agricultural production. However, the majority of the site is vacant as it has been in use as agricultural property. During discussions leading up to the approval of General Plan 2035, this property was chosen specifically for its ability to support large, mixed use, urban employment, and residential development projects.

Project Vicinity Characteristics

The following matrix characterizes land uses in the immediate project vicinity (See Figure 3).

Direction from Project Site	Land Use
North	Large lot residential uses; Single family Homes (29 Palms BOMI Reservation).
South	CVWD Maintenance Yard and Offices
East	Single Family Residential; Multi-Family Residential; Mobile Home Park
West	Union Pacific Railroad; Industrial Development; Grapefruit Blvd.

The trust lands of the Cabazon Band of Mission Indians, making up territories of the 29 Palms Band of Mission Indians, are located north of the project site along the north side of Avenue 50.



2.2 Existing General Plan Land Use Designation/Zoning District

The current General Plan Land Use designation for the project site falls within SubArea 6 – Downtown Expansion. Pursuant to the City General Plan this area will serve as the expansion of Downtown Coachella and it is envisioned to have a wide diversity of employment and civic uses. The area is envisioned as having small, walkable blocks with office, R&D and supporting retail and services. Multi-family residential uses may be located on the edges of this area and adjacent to existing residential development, such as the area east of Tyler Street. This area would also be an ideal location for a college or university that could benefit from the proximity to Downtown and expected office and R&D uses and an orientation towards walkable, university campus-style development. The foregoing is graphically depicted in **Figure 6, General Plan 2035 Land Use Element**, and **Figure 7, City of Coachella - 2013 Zoning Map**, respectively.

2.3 Project Description

Pursuant to the City of Coachella, General Plan 2035, *Zona Central*, is proposed to bring the 206-acres within SubArea 6, into full compliance with the General Plan Land Use element. SubArea 6 is a combination district that includes elements of both employment and residential uses. Prior to the approval of this Change of Zone (18-10), the project site contains only two zoning districts: M-S (Manufacturing Service) and R-S (Single Family Residential). Neither of these two zones provides the employment flexibility or the variety of housing types required by the General Plan Land Use Element, or Housing Element. To bring the site into compliance with the General Plan, and to meet the housing needs of the area, the City of Coachella Development Services with the consent of the Property Owner propose to create the following Zoning Districts:

- MS-IP; Manufacturing Service – Industrial Park
- C-N; Neighborhood Commercial
- CN-PD; Neighborhood Commercial Planned Development (Mixed Use, Urban Employment Center)
- R-M; Residential, Multi-Family (General Neighborhood)
- R-M; Residential, Multi-Family (Urban Neighborhood)
- O-S; Open Space
- OS-PF; Open Space Public Facilities

Total Project Area	206.2 acres*
MS-IP Zone	35.6 acres*
C-N Zone	13.5 acres*
CN-PD (Mixed Use) Zone	42.6 acres*
R-M (General) Zone	21.2 acres*
R-M (Urban) Zone	51.4 acres*
O-S Zone	8.0 acres*
OS-PF Zone	10.3 acres*
Interior Streets	23.6 acres*

***Note!** Project Area Estimates taken from Riverside County Tax Assessor Maps. Exact acreage may vary.

Primary site access will be provided via Avenue 50, Tyler Street, and Peter Rabbit Lane. Access to the interior of the project site will be provided by a number of smaller, residential and local commercial streets similar to those designed for Pueblo Viejo. A Conceptual Land Use Plan is provided in Figure 9.



It should be noted that no development is proposed at this time. Future development will be required to comply with the full provisions of the City of Coachella General Plan, City of Coachella Zoning and Development Code, and the California Environmental Quality Act, including any specific Conditions of Approval or Mitigation Measures required for a specific type of development.

2.4 Permits/Approvals required from other public agencies

Agency	Permit/Approval Required
FEDERAL	None
STATE	None at this time
REGIONAL	None at this time
LOCAL	
City of Coachella	Change of Zone (CZ 18-10)

The following utilities/service providers currently serve the project site and vicinity:

1. Potable Water Supply- Coachella Water Authority
2. Sewage Disposal System- Coachella Sanitary District (future)
3. Electricity- Imperial Irrigation District
4. Natural Gas- Southern California Gas Company (future)
5. Telephone- Frontier
6. Cable TV- Time Warner Cable

It is assumed that plans for future development projects for the proposed area will tie into existing water and sewer lines in Avenue 50 and Tyler Street as well as other dry utility/service system networks available in the general project vicinity.



Figure 1 Regional Location



RIVERSIDE COUNTY



Figure 2 Project Vicinity



FIGURE 4



Legend:
Indicates Location and
Direction of Photo

PROJECT SITE PHOTO KEY

ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

Project Applicant



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Property Owner



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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

FIGURE 5.1

1A

View from Northeast Corner of Site Looking South



1B

View from Northeast Corner of Site Looking Southwest



1C

View from Northeast Corner of Site Looking Northwest

ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

FIGURE 5.2

ZONA CENTRAL
DOWNTOWN EXPANSION
CITY OF COACHELLA, CA

2A

View from Northernmost Corner of Site Looking West



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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion



2B

View from Northernmost Corner of Site Looking South

View from Northernmost Corner of Site Looking Southeast

2C

CZ 18-10
EA 18-04

FIGURE 5.3

3A

View from Northwest
Corner of Site
Looking East



3B

View from Northwest
Corner of Site
Looking Southeast



ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10

EA 18-04

FIGURE 5.4

ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

4A

View from 7th Street
Looking Northeast



4B

View from 7th Street
Looking Easterly



4C

View from 7th Street
Looking Southeast

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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

FIGURE 5.5

ZONA CENTRAL
DOWNTOWN EXPANSION
CITY OF COACHELLA, CA

5A

**View from Southwest
Corner of Site
Looking Northwest**



5B

**View from Southwest Corner
of Site Looking Northeast**

**View from Southwest Corner
of Site Looking East**

5C

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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

FIGURE 5.6

ZONA CENTRAL
 DOWNTOWN EXPANSION
 CITY OF COACHELLA, CA

6A

**View from Southeast
 Corner of Site
 Looking West**



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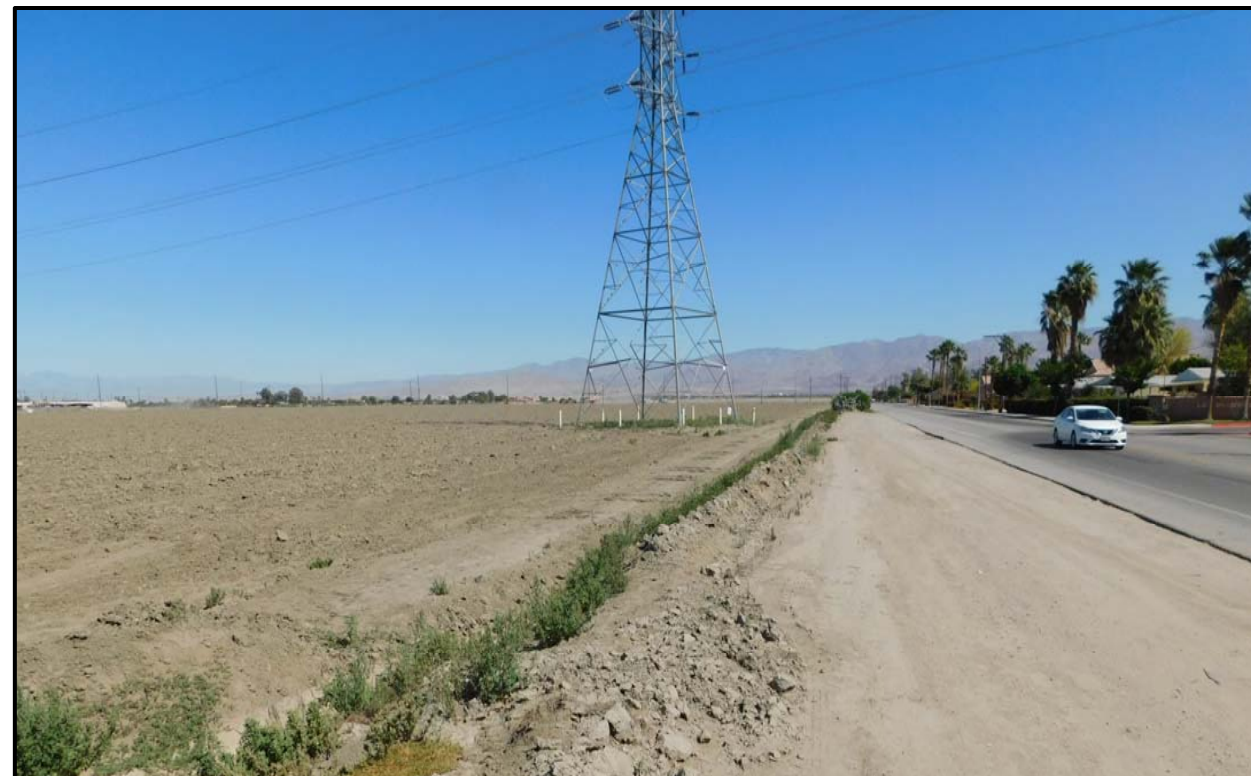
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General Plan 2035 Land Use
 Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04



6B

**View from Southeast Corner
 of Site Looking Northwest**

**View from Southeast Corner
 of Site Looking North**

6C

7A

View from Calle Bouganvillea
Looking South



FIGURE 5.7

7B

View from Calle Bouganvillea
Looking Southwest



ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

**CZ 18-10
EA 18-04**

7C

View from Calle Bouganvillea
Looking Northwest



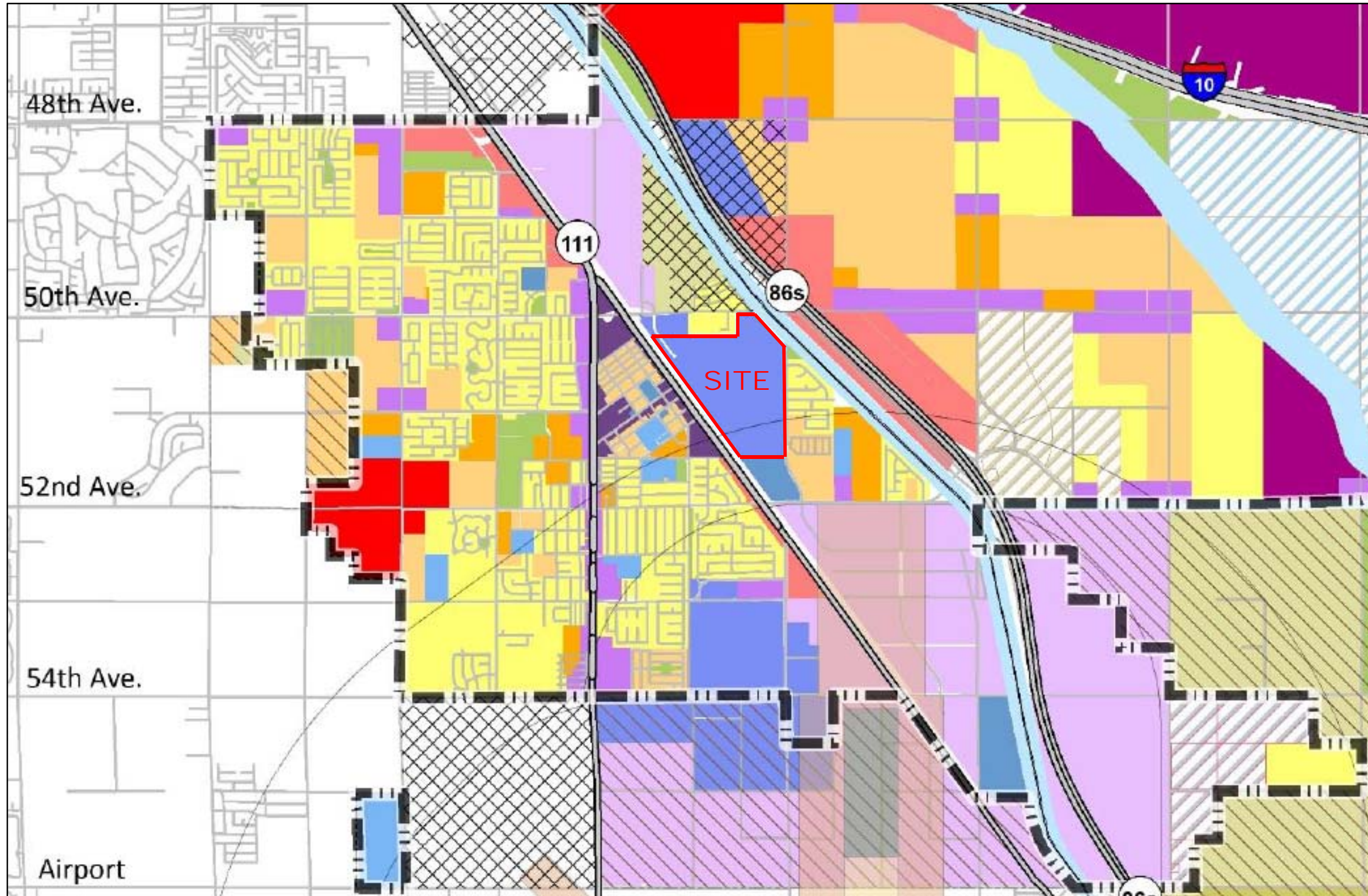
View from Calle Bouganvillea
Looking North



7D

FIGURE 6

ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA



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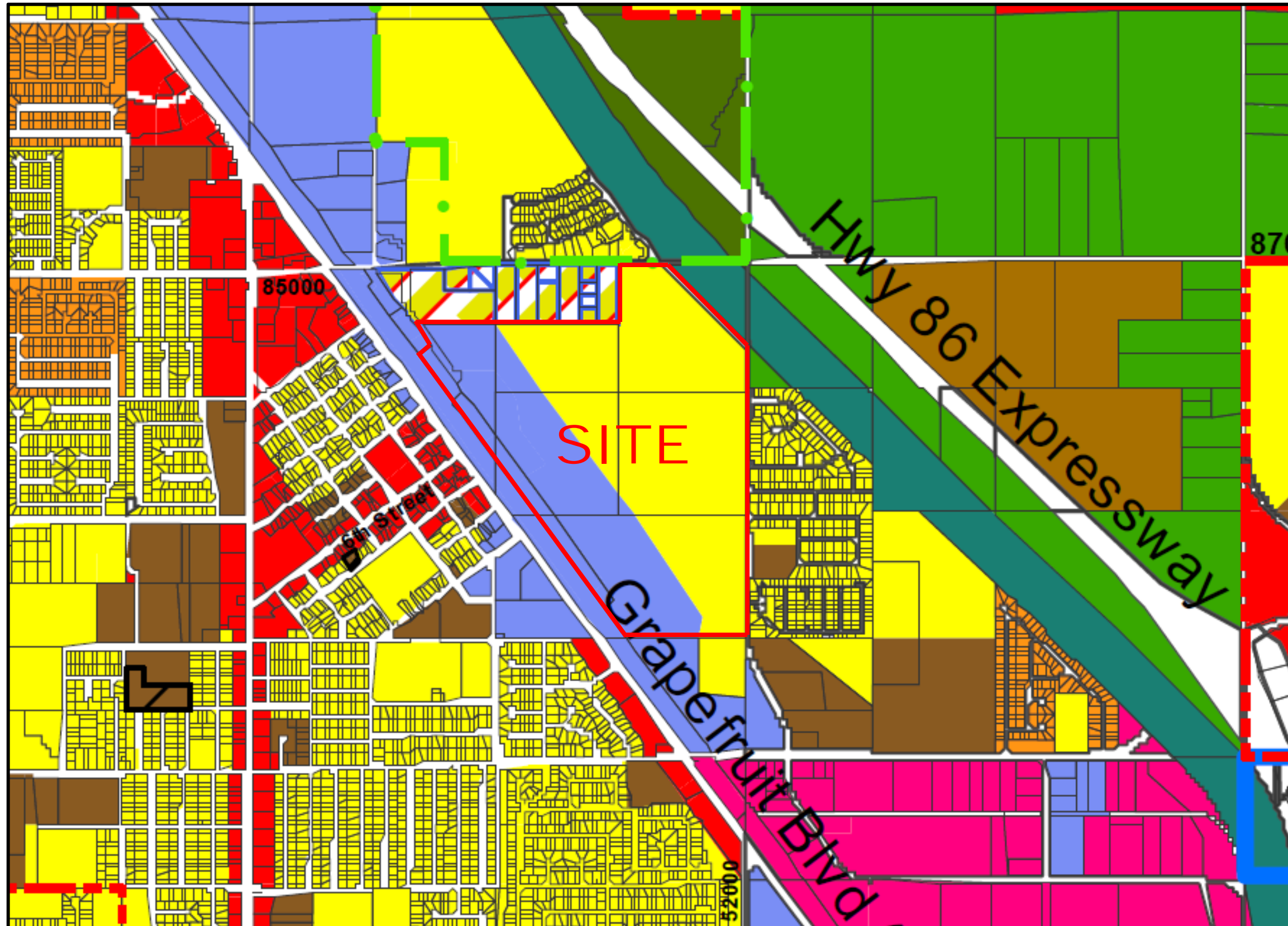
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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

FIGURE 7



ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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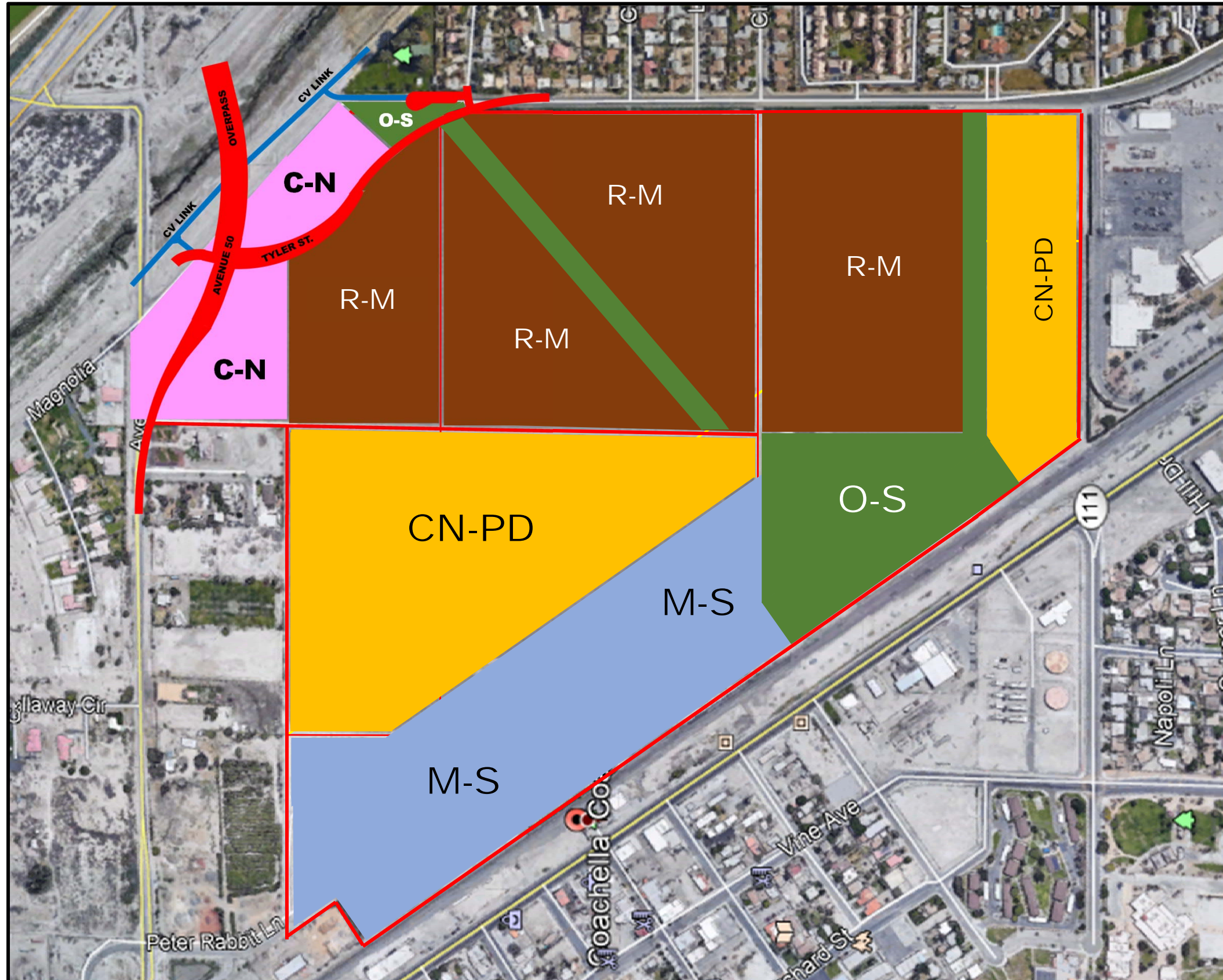
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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

FIGURE 8



ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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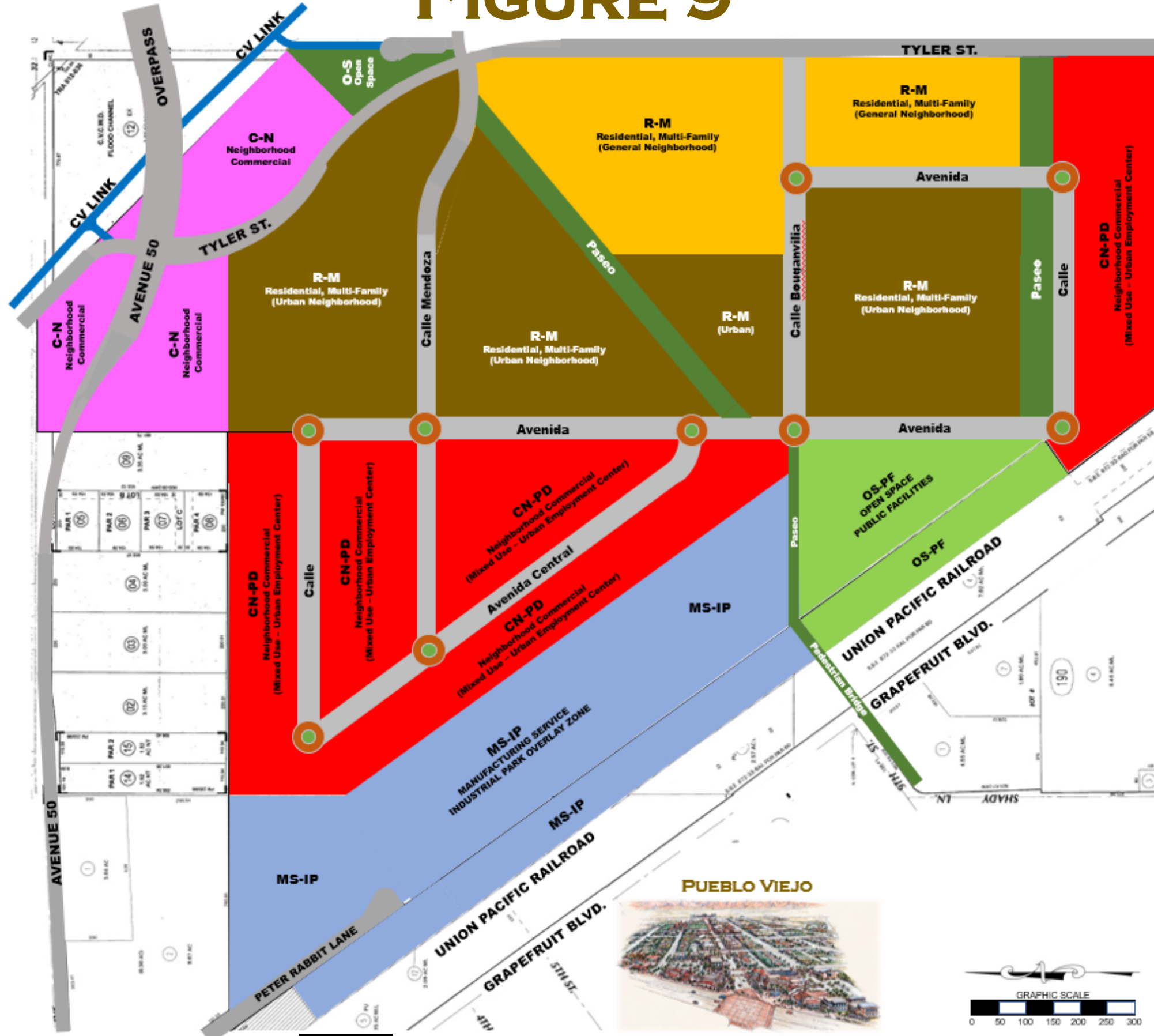
General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04

CITY OF COACHELLA – PROPOSED ZONING

CONSISTENT WITH GENERAL PLAN 2035 LAND USE ELEMENT

FIGURE 9



ZONA CENTRAL DOWNTOWN EXPANSION CITY OF COACHELLA, CA

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General Plan 2035 Land Use
Subarea 6 – Downtown Expansion

CZ 18-10
EA 18-04



Chapter 3 Environmental Analysis

	<p>INITIAL STUDY / NEGATIVE DECLARATION</p>
---	--

Project Title:	Zona Central – Downtown Expansion
Case No.	Change of Zone – CZ 18-10 Environmental Assessment – EA 18-04
Assessor’s Parcel No.	778-030-007, 012, 014; 778-017-010, 011; 778-180-002, 003, 004; 778-019-001, 002
Project Location:	South of Avenue 50, West of Tyler Street, approximately ¼ mile north of Avenue 52, in the City of Coachella, CA; Riverside County
Property Owners /Project Sponsor’s Name and Address:	John Powell, Jr. President and CEO Peter Rabbit Farms, Inc. 85-810 Peter Rabbit Lane, Coachella CA 92236
General Plan Designation(s):	SubArea 6 – Downtown Expansion
Current Zoning:	M-S; Manufacturing Service R-S; Residential Single Family
Lead Agency Name and Address:	City of Coachella – Development Services Department 53990 Enterprise Way Coachella, California 92236
Lead Agency Contact Person:	Luis Lopez, Development Services Director
Phone Number:	(760) 398-3502
Date Prepared	September 3, 2019



Environmental Factors Potentially Affected:

The environmental factors checked below would be potentially affected by this project, as indicated by the checklist and corresponding discussion on the following pages.

<input type="checkbox"/>	Aesthetics	<input type="checkbox"/>	Agriculture / Forestry Resources	<input type="checkbox"/>	Air Quality
<input type="checkbox"/>	Biological Resources	<input type="checkbox"/>	Cultural Resources	<input type="checkbox"/>	Energy
<input type="checkbox"/>	Geology /Soils	<input type="checkbox"/>	Greenhouse Gas Emissions	<input type="checkbox"/>	Hazards & Hazardous Materials
<input type="checkbox"/>	Hydrology / Water Quality	<input type="checkbox"/>	Land Use / Planning	<input type="checkbox"/>	Mineral Resources
<input type="checkbox"/>	Noise	<input type="checkbox"/>	Population / Housing	<input type="checkbox"/>	Public Services
<input type="checkbox"/>	Recreation	<input type="checkbox"/>	Transportation	<input type="checkbox"/>	Tribal Cultural Resources
<input type="checkbox"/>	Utilities / Service Systems	<input type="checkbox"/>	Wildfire	<input type="checkbox"/>	Mandatory Findings of Significance

DETERMINATION: The City of Coachella Development Services Department

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a “potentially significant impact” or “potentially significant unless mitigated” impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Luis Lopez
Development Services Director

Date



PURPOSE OF THIS INITIAL STUDY

This Initial Study has been prepared in accordance with the CEQA Guidelines Section 15063(c) to provide a preliminary analysis of a proposed project's actions and to determine if the project, as proposed, may have a significant effect upon the environment. The findings determined from the preliminary analysis are presented in the form of the Initial Study, which will be used in support of the preparation of a Mitigated Negative Declaration.

EVALUATION OF ENVIRONMENTAL IMPACTS

- 1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on project-specific screening analysis).
- 2) All answers must take into account the whole action involved, including offsite as well as onsite, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level mitigation measures from Section XVII, "Earlier Analyses," may be cross-referenced).
- 5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analysis Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
- 7) Supporting Information Sources: A list of references used during the preliminary analysis and research should be attached with this document. In addition, other sources used or individuals contacted should be cited in the discussion.
- 8) The explanation of each issue should identify:
 - a) The significance criteria or threshold, if any, used to evaluate each question; and
 - b) The mitigation measure identified, if any, to reduce the impacts to less than significance.



I. AESTHETICS

Would the project:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: 2035 Coachella General Plan, 2015; "California Scenic Highway Mapping System"; Coachella Municipal Code.)

Setting

The City of Coachella, including the project site, is located in the Coachella Valley, which typically enjoys views of the surrounding San Bernardino and San Jacinto Mountain Ranges. The San Bernardino and San Jacinto Mountain Ranges have a significant rise over the valley floor and are visible from most locations in the City. The foothills of the San Bernardino Mountains extend along the northerly and easterly portion of the City, beginning approximately 3.8 miles northeast of the subject property.

Discussion of Impacts

a, b) No Impact. The subject property is located approximately 3.8 miles southwest of the San Bernardino Mountain foothills, which are considered a scenic vista for much of the Coachella Valley. From the subject property, scenic views of the San Bernardino Mountains are to the north, northeast, and east. Views of the San Jacinto Mountains are to the south, and west. Existing lower elevation mountain views are obstructed, to some extent, by surrounding commercial/industrial developments that range from one- to two- story buildings. However, middle and upper elevations of the mountains are visible above the surrounding structures.

Certain sections of old Highway 99 (Dillon Road between Grapefruit Blvd and Interstate 10), Old Highway 86 (Harrison Street south of Grapefruit Blvd), and Old Highway 111 (Grapefruit Boulevard) represent visual corridors that provide a glimpse into the historic context of the pre-Freeway era of the eastern Coachella Valley. However, the project site is not located within a state scenic highway or locally designated scenic corridor. The project site does not contain scenic resources such as rock outcroppings or trees.

At this time, there is no construction and no new development proposed for the project site. At the time of development, proposed projects will be required to adhere to the development requirements of the City of Coachella Municipal Code, City of Coachella General Plan, California Environmental Quality Act (CEQA), and area-wide project development guidelines. The Municipal



Code, General Plan, and applicable development guideline will limit the building mass and height of future development to that which is outlined in said documents. Deviation from approved development regulations will require additional public review and discretionary acts by the local agency.

- c) **No Impact.** The area surrounding the subject site is characterized by one- and two-story commercial, industrial, and residential structures. As previously discussed, no development is proposed at this time, and any future development is subject to State and Local codes and regulations in effect at the time of development. While proposed structures may be greater in mass compared to existing structures in the area, the anticipated design review, architectural review, variations in building massing and high-quality architectural treatment will improve the overall visual character of the site. Therefore, no impacts associated with visual character are expected.
- d) **No Significant.** While build out of the proposed project may generate increased levels of light and glare from interior and exterior building lighting, safety and security lighting, landscape lighting, and vehicles accessing the site. Future lighting and glare levels are not expected to exceed typical levels within the surrounding light industrial/urban environment and will be regulated by City lighting standards. The project will be designed according to the City's Zoning Ordinance and will properly shield light fixtures to minimize spillage onto adjacent properties to ensure that project-related light and glare impacts will be less than significant.

Mitigation Measures

None.



II. AGRICULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526) or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: 2035 Coachella General Plan, Figure 3-6, 2015; "Riverside County Important Farmland 2014 Map," sheet 2 of 3, California Department of Conservation, updated November, 2016.)

Setting

The Project site is located in an area that is designated for mixed industrial, commercial, and residential uses. Properties to the west are largely developed with storage and agricultural-related uses such as packing houses. To the south is the CVWD Maintenance yard and offices. Properties to the east are a variety of residential uses. According to the 2016 Riverside County Important Farmland map, the Project site is designated as Urban and Built-Up Land. According to the California Department of Conservation, Urban and Built-Up Land is defined as land occupied by structures with a building density of at least 1 unit to 1.5 acres, or approximately 6 structures to a 10-acre parcel. This Project site and vicinity is used for industrial, commercial, construction, institutional, public administration, railroad and other transportation yards, cemeteries, airports, golf courses, sanitary landfills, sewage treatment, water control structures, etc.. City of Coachella General Plan 2035 also allows for a variety of residential uses in this vicinity. The Project site is not under a Williamson Act Contract as shown on the 2015/2016 Williamson Act Lands map for Riverside County.

Discussion of Impacts

a,b,e) No Impact. The westerly portion of the project site is partially developed with industrial agriculture facilities. The eastern portion of the site is actively farmed with row crops, typically carrots and leafy greens. The subject site is designated as mixed use employment and residential in the Coachella General Plan 2035 and Urban and Built-up Land on the Riverside County Important Farmland Map 2016. The site is not designated as Prime Farmland, Unique



Farmland, or Farmland of Statewide or Local Importance by the California Department of Conservation. There are no lands with a Williamson Act contract in the immediate project vicinity. The site bounded by lands that are not in agricultural uses. There will be no impacts to agricultural resources.

The General Plan Land Use Plan designated Subarea 16 – South Coachella specifically to maintain agricultural operations within the City beyond the 2035 timeframe of the General Plan. Additionally, the land use plan provides that small scale agricultural operations are allowed in subareas 15 and 17. To address the extensive conversion of agricultural resources in the 2035 General Plan, a comprehensive policy program was developed. Per Policy 2.12, Subarea 10 is considered a High Priority Development Area, which is targeted for future growth. The City aims to reduce impacts on agricultural resources by focusing new development in High Priority Development Areas and Growth Expansion Areas, and prohibiting or limiting development of land in Subareas 15 and 16. The City aims to contain agricultural resources in subareas 15 and 16 until the growth areas are at least 60% developed. It is not the intent of the General Plan to preserve all agricultural activities within the City, but manage the transition from agricultural land to urban uses.

For future development projects on this site, the presence of active farmland may require an agricultural land evaluation using the LESA Model. The LESA Model was developed to provide lead agencies with a methodology to ensure that potentially significant effects on the environment caused by agricultural land conversions are quantitatively and consistently considered in the environmental review process. The LESA Model evaluates measures of soil resource quality, a given project's size, water resource availability, surrounding agricultural lands, and surrounding protected resource lands.

As there is no proposed development project, and therefore no displacement of active farmland, there is no impact to agricultural resources

- c-d) No Impact.** There are no forest lands on or near the site; therefore the project would not impact any forest or timberlands.

Mitigation Measures

None.



III. AIR QUALITY		Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:					
a)	Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b)	Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c)	Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d)	Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e)	Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015, "Coachella Valley Multiple Species Habitat Conservation Plan," 2007)

Setting

The project site is located in the Salton Sea Air Basin (SSAB), which is under the jurisdiction of the South Coast Air Quality Management District (SCAQMD). All development within the SSAB is subject to SCAQMD's 2016 Air Quality Management Plan (2016 AQMP) and the 2003 Coachella Valley PM₁₀ State Implementation Plan (2003 CV PM₁₀ SIP). The SCAQMD operates and maintains regional air quality monitoring stations at numerous locations throughout its jurisdiction. The proposed site is located within Source Receptor Area (SRA) 30, which includes monitoring stations in Palm Springs and Indio. The Indio site has been operational since 1985 and the Palm Springs site since 1987.

Regional Air Quality

The project site is located in the Coachella Valley region of Riverside County and is part of the Salton Sea Air Basin (SSAB) which is under the jurisdiction of the South Coast Air Quality Management District (SCAQMD). Criteria air pollutants are contaminants for which the state and federal air quality standards have been established. The Salton Sea Air Basin exceeds state and federal standards for fugitive dust (PM₁₀) and ozone (O₃), and is in attainment/unclassified for PM_{2.5}. Ambient air quality in the SSAB, including the project site, does not exceed state and federal standards for carbon monoxide, nitrogen dioxides, sulfur dioxide, lead, sulfates, hydrogen sulfide, or Vinyl Chloride.

South Coast Air Quality Management District

The SCAQMD develops rules and regulations, establishes permitting requirements for stationary sources, inspects emission sources, and enforces such measures through educational programs or fines, when necessary. SCAQMD is responsible for controlling emissions primarily from stationary sources and maintaining air quality monitoring stations throughout the South Coast Air Basin and a portion of the



Salton Sea Air Basin. SCAQMD is also responsible for developing, updating, and implementing the AQMP for the region, in coordination with SCAG. SCAQMD also has roles under CEQA.

SCAQMD Rules

The AQMP for the basin establishes a program of rules and regulations administered by SCAQMD to obtain attainment of the state and federal standards. During construction and operation, the project must comply with applicable rules and regulations. The following are rules the project may be required to comply with, either directly, or indirectly:

SCAQMD Rule 402 prohibits a person from discharging from any source whatsoever such quantities of air contaminants or other material which cause injury, detriment, nuisance, or annoyance to any considerable number of persons or to the public, or which endanger the comfort, repose, health or safety of any such persons or the public, or which cause, or have a natural tendency to cause, injury or damage to business or property.

SCAQMD Rule 403 governs emissions of fugitive dust during construction and operation activities. Compliance with this rule is achieved through application of standard Best Management Practices, such as application of water or chemical stabilizers to disturbed soils, covering haul vehicles, restricting vehicle speeds on unpaved roads to 15 miles per hour, sweeping loose dirt from paved site access roadways, cessation of construction activity when winds exceed 25 mph, and establishing a permanent ground cover on finished sites.

Rule 403 requires that fugitive dust be controlled with best available control measures so that the presence of such dust does not remain visible in the atmosphere beyond the property line of the emission source. In addition, SCAQMD Rule 403 requires implementation of dust suppression techniques to prevent fugitive dust from creating a nuisance off-site. Applicable dust suppression techniques from Rule 403 are summarized below. Implementation of these dust suppression techniques can reduce the fugitive dust generation (and thus the PM10 component). Compliance with these rules would reduce impacts on nearby sensitive receptors. Rule 403 measures may include but are not limited to the following:

- Apply nontoxic chemical soil stabilizers according to manufacturers' specifications to all inactive construction areas (previously graded areas inactive for 10 days or more).
- Water active sites at least three times daily. (Locations where grading is to occur will be thoroughly watered prior to earthmoving).
- Cover all trucks hauling dirt, sand, soil, or other loose materials, or maintain at least 0.6 meters (2 feet) of freeboard (vertical space between the top of the load and top of the trailer) in accordance with the requirements of California Vehicle Code section 23114.
- Reduce traffic speeds on all unpaved roads to 15 miles per hour (mph) or less.
- Suspension of all grading activities when wind speeds (including instantaneous wind gusts) exceed 25 mph.
- Bumper strips or similar best management practices shall be provided where vehicles enter and exit the construction site onto paved roads or wash off trucks and any equipment leaving the site each trip.
- Replanting disturbed areas as soon as practical.
- During all construction activities, construction contractors shall sweep on-site and off-site streets if silt is carried to adjacent public thoroughfares, to reduce the amount of particulate matter on public streets. All sweepers shall be compliant with SCAQMD Rule 1186.1, Less Polluting Sweepers.



SCAQMD Rule 403.1 is supplemental to Rule 403 requirements and shall apply only to fugitive dust sources in the Coachella Valley. The general requirements for Rule 403.1 are as follows:

1. Any person who is responsible for any active operation, open storage pile, or disturbed surface area, and who seeks an exemption pursuant to Rule 403, paragraph (g)(2) shall be required to determine when wind speed conditions exceed 25 miles per hour. The wind speed determination shall be based on either District forecasts or through use of an on-site anemometer as described in subdivision (g).
2. Any person involved in active operations in the Coachella Valley Blowsand Zone shall stabilize new man-made deposits of bulk material within 24 hours of making such bulk material deposits. Stabilization procedures shall include one or more of the following: (A) Application of water to at least 70 percent of the surface area of any bulk material deposits at least 3 times for each day that there is evidence of wind driven fugitive dust; or (B) Application of chemical stabilizers in sufficient concentration so as to maintain a stabilized surface for a period of at least 6 months; or
 - a. (3) Installation of wind breaks of such design so as to reduce maximum wind gusts to less than 25 miles per hour in the area of the bulk material deposits. (3) Any person involved in active operations in the Coachella Valley Blowsand Zone shall stabilize new deposits of bulk material originating from off-site undisturbed natural desert areas within 72 hours.
 - b. Stabilization procedures shall include one or more of the following: (A) Application of water to at least 70 percent of the surface area of any bulk material deposits at least 3 times for each day that there is evidence of wind driven fugitive dust; or (B) Application of chemical stabilizers in sufficient concentration so as to maintain a stabilized surface for a period of at least six months.
3. A person who conducts or authorizes the conducting of an active operation shall implement at least one of the control actions specified in Rule 403, Table 2 for the source category "Inactive Disturbed Surface Areas" to minimize wind driven fugitive dust from disturbed surface areas at such time when active operations have ceased for a period of at least 20 days.
4. Any person involved in agricultural tilling or soil mulching activities shall cease such ties when wind speeds exceed 25 miles per hour. The wind speed determination shall be based on either District forecasts or through use of an on-site anemometer as described in subdivision (g).

In addition, Fugitive Dust Control Plan and other requirements for construction projects including earth moving activities are listed herein:

1. Any person who conducts or authorizes the conducting of an active operation with a disturbed surface area of more than 5,000 square feet shall not initiate any earth- moving activities unless a fugitive dust control plan is prepared and approved by the Executive Officer in accordance with the requirements of subdivision (f) and the Rule 403.1 Implementation Handbook. These provisions shall not apply to active operations exempted by paragraph (i)(4).
2. Any operator required to submit a fugitive dust control plan under paragraph (e)(1) shall maintain a complete copy of the approved fugitive dust control plan on-site in a conspicuous place at all times and the fugitive dust control plan must be provided upon request.
3. Any operator required to submit a fugitive dust control plan under paragraph (e)(1) shall install and maintain signage with project contact information that meets the minimum standards of the Rule 403.1 Implementation Handbook prior to initiating any type of earth-moving activities.
4. Any operator required to submit a fugitive dust control plan under paragraph (e)(1) for a project with a disturbed surface area of 50 or more acres shall have an Dust Control Supervisor that: (A) is employed by or contracted with the property owner or developer; and (B) is on-site or is



available to be on-site within 30 minutes of initial contact; and (C) has the authority to expeditiously employ sufficient dust mitigation measures to ensure compliance with all Rule 403 and 403.1 requirements; and (D) has completed the AQMD Coachella Valley Fugitive Dust Control Class and has been issued a valid Certificate of Completion for the class.

5. Failure to comply with any of the provisions of an approved fugitive dust control plan shall be a violation of this rule.

SCAQMD Rule 445 prohibits permanently installed wood burning devices into any new development. A wood burning device means any fireplace, wood burning heater, or pellet-fueled wood heater, or any similarly enclosed, permanently installed, indoor or outdoor device burning any solid fuel for aesthetic or space-heating purposes, which has a heat input of less than one million British thermal units per hour.

SCAQMD Rule 481 applies to all spray painting and spray coating operations and equipment. The rule states that a person shall not use or operate any spray painting or spray coating equipment unless one of the following conditions is met:

1. The spray coating equipment is operated inside a control enclosure, which is approved by the Executive Officer. Any control enclosure for which an application for permit for new construction, alteration, or change of ownership or location is submitted after the date of adoption of this rule shall be exhausted nor greater than 300 feet per minute, or through a water wash system designed to be equally effective for the purpose of air pollution control.
 - Coatings are applied with high-volume low-pressure, electrostatic and/or airless spray equipment.
 - An alternative method of coating application or control is used which has effectiveness equal to or greater than the equipment specified in the rule.

SCAQMD Rule 1108 governs the sale, use, and manufacturing of asphalt and limits the volatile organic compound (VOC) content in asphalt used in the South Coast Air Basin. This rule would regulate the VOC content of asphalt used during construction. Therefore, all asphalt used during construction of the project must comply with SCAQMD Rule 1108.

SCAQMD Rule 1113 governs the sale, use, and manufacturing of architectural coating and limits the VOC content in paints and paint solvents. This rule regulates the VOC content of paints available during construction. Therefore, all paints and solvents used during construction and operation of the project must comply with SCAQMD Rule 1113.

SCAQMD Rule 1143 governs the manufacture, sale, and use of paint thinners and solvents used in thinning of coating materials, cleaning of coating application equipment, and other solvent cleaning operations by limiting their VOC content. This rule regulates the VOC content of solvents used during construction. Solvents used during the construction phase must comply with this rule.

SCAQMD Rule 1186 limits the presence of fugitive dust on paved and unpaved roads and sets certification protocols and requirements for street sweepers that are under contract to provide sweeping services to any federal, state, county, agency or special district such as water, air, sanitation, transit, or school district.

SCAQMD Rule 1303 governs the permitting of re-located or new major emission sources, requiring Best Available Control Measures and setting significance limits for PM10 among other pollutants.



SCAQMD Rule 1401, New Source Review of Toxic Air Contaminants, specifies limits for maximum individual cancer risk, cancer burden, and non-cancer acute and chronic hazard index from new permit units, relocations, or modifications to existing permit units, which emit toxic air contaminants.

SCAQMD Rule 2202, On-Road Motor Vehicle Mitigation Options, is to provide employers with a menu of options to reduce mobile source emissions generated from employee commutes, to comply with federal and state Clean Air Act requirements, Health & Safety Code Section 40458, and Section 182(d)(1)(B) of the federal Clean Air Act. It applies to any employer who employs 250 or more employees on a full or part- average.

Local Policies

Local jurisdictions, such as the City of Coachella, have the authority and responsibility to reduce air pollution through its police power and decision-making authority. The General Plan contains the following goals aimed at reducing air pollution:

The Sustainability and Natural Environment Chapter contains the following goals aimed at reducing air pollution:

Goal 11 Air Quality. Healthy indoor and outdoor air quality through reduced, locally generated pollutant emissions.

The Safety Chapter of the City's General Plan Updated contains the following goals aimed at reducing air pollution:

Goal 6 Clean Environment. A community protected from the harmful effects of pollution and hazardous materials, hazardous waste and environmental contamination.

Goal 7 Severe Weather Hazards. A community that is minimally affected by high winds, dust storms, extreme temperatures and drought.

Discussion of Impacts

- a, b) No Impact.** The project site is located in the Salton Sea Air Basin (SSAB) and will be subject to SCAQMD's 2012 Air Quality Management Plan (2012 AQMP) and the 2003 Coachella Valley PM₁₀ State Implementation Plan (2003 CV PM₁₀ SIP). The AQMP is based, in part, on the land use plans of the jurisdictions in the region. The AQMP is a comprehensive plan that establishes control strategies and guidance on regional emission reductions for air pollutants. The proposed project is consistent with the City of Coachella land use designations assigned to the subject property. Therefore, the proposed project is consistent with the intent of the AQMP and will not conflict with or obstruct implementation of the applicable air quality plan. No impact associated with compliance with applicable management plans is expected.
- c) No Impact.** The project site is located in the Coachella Valley portion of the Salton Sea Air Basin, which is classified as a "non-attainment" area for PM₁₀ and ozone. In order to achieve attainment in the region, the 2003 Coachella Valley PM₁₀ Management Plan was adopted, which established strict standards for dust management for development proposals. Future development proposals may contribute to an incremental increase in regional PM₁₀ and ozone emissions. However, given



its limited size and scope, cumulative impacts are not expected to be considerable. Project construction and operation emissions will not exceed SCAQMD thresholds for PM₁₀ or ozone precursors (NO_x and CO). Therefore, the project will not conflict with any attainment plans and will result in no impacts.

- d) **No Impact.** The nearest sensitive receptors are single-family residences located east of the project site. The City of Coachella and the project property are located within Source Receptor Area 30 (Coachella Valley). Based on the project's size and proximity to sensitive receptors, the 5-acre site tables at a distance of 500 meters should be used to determine if future project proposals have on-site emission concentrations for project construction and the associated LST.

As there are no development proposal included with this application **Localized Significance Threshold (pounds per day)**, LSTs will not be exceeded under unmitigated conditions for all criteria pollutants. Therefore, air quality impacts to nearby sensitive receptors are of no impact.

- e) **No Impact.** At this time, no development is proposed. If necessary, future development projects will be required to be equipped with sufficient odor absorbing ventilation and exhaust systems under Coachella Municipal Code, Section 5.68.170. Compliance with the Code will ensure that distinctive odors associated with operation are not detected outside the facility. Future development projects are not expected to generate objectionable odors during any phase of construction or at project buildout. Short term odors associated with paving and construction activities could be generated; however, any such odors would be quickly dispersed below detectable levels as distance from the construction site increases. Future development will be required to conform to all applicable odor control requirements at the time of permitting, there no impact is expected at this time.

Mitigation Measures

None



IV. BIOLOGICAL RESOURCES	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Setting

The subject site has been previously disturbed by existing development and is surrounded on all sides by development, including paved roads, parking lots, maintenance yards, retention basins, and other commercial and residential uses. The subject property is located in the Coachella Valley, which is subject to the provisions of the Coachella Valley Multiple Species Habitat Conservation Plan (CVMSHCP).

The Environmental Impact Report (EIR) for the City of Coachella’s General Plan Update 2035 and the City of Coachella’s 2035 General Plan were referenced to analyze potential impacts to biological resources associated with the proposed development. The proposed project is not located within a MSHCP Criteria Cell, Group, or Linkage Area, or a predetermined Survey Area for narrow endemic or criteria area plant species, or a Survey Area for amphibians or mammals.



Discussion of Impacts

- a) **No Impact.** The site is currently in operation as an Industrial Agriculture Facility with associated facilities. The site is located in a predetermined survey area for burrowing owl. However, the site is currently developed and portions that are undeveloped have been greatly disturbed due to cultivation efforts. Therefore a 30-day CVMSHCP preconstruction survey is not required immediately prior to the initiation of onsite construction.

The project site is not located in or adjacent to an existing or proposed MSHCP Conservation area. The proposed development will have no impact to species listed in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service biological resources.

- b-c) **No Impact.** The project site does not contain any streams, riparian habitat, marshes, protected wetlands, vernal pools or sensitive natural communities protected by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service. No project-related impacts will occur.

- d) **No Impact.** The subject property is located in an urban area and surrounded by roadways and commercial development. Due to surrounding human activity for many decades, the site does not contain trees or other biological species or features that are suitable for a migratory wildlife corridor. No project-related impacts will occur.

- e-f) **No Impact.** The subject property is located within the boundaries of the CVMSHCP. However, based on Section 4.48.110 (B and D) of the CVMSHCP, the site may not be required to pay the Development Mitigation Fee because the site has previously been developed and will result in the conversion of an existing permitted use to a new permitted use. No project-related impacts will occur.

The site is not within or adjacent to a CVMSHCP-designated Conservation Area, therefore no additional mitigation measures or provisions are required. The project will not conflict with any policies or ordinances that protect biological species, or any habitat conservation plans or natural community conservation plans. No project-related impacts will occur.

Mitigation Measures - None.



V. CULTURAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Cause a substantial adverse change in the significance of a historical resource as defined in 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Source: City of Coachella General Plan, 2015)

Setting

The City of Coachella sits on the shoreline of ancient Lake Cahuilla, a large intermittent freshwater lake created by the Colorado River. Its shorelines continually changed as the lake was filled and emptied by the river, and when it was full it would have attracted human settlement with its plentiful resources. Settlement along the lakeshore in the Coachella Valley was particularly intensive, with evidence of large-scale, multi-seasonal occupation.

The cultural growth in the City of Coachella was relatively slow until the arrival of reliable water sources. The oldest cultural resources reported in the city are from the “Paleo-Indian Period” which dates back to at least 11,000 B.C. However, these resources are sparsely distributed within the city boundaries.

The City of Coachella contains a significant amount of archeological resources due to its rich cultural history and historical settlements within its boundaries. The city was once the site of Native Americans land and some tribal land still exists there. In addition, the city still has present Native American population.

Due to its historical cultural and archaeological resources, most of the City is designated as “Medium sensitivity to historical resource sensitivity” (Coachella 2035 General Plan DEIR, Figure 4.4-2). As cultural resources are sensitive to the Native American population, tribal consultation is an ongoing process prior to the adoption of any Negative Declaration, Mitigated Negative Declaration, or Environmental Impact Report.

Discussion of Impacts

- a) **No Impact.** The project site is highly disturbed and has operated as an industrial agriculture for many years. The project site is not located in the area identified as historically significant by the Riverside County Historical Commission of the Riverside Parks Department, National Register of Historic Places, California Register of Historical Resources, and the City. Notable local historic sites include the Coachella Valley Water District building and the city of Coachella’s downtown area.



The Coachella Valley Water District building is the only registered historical resource in the City, located approximately 1/2 mile west of the subject site. Coachella's downtown area is approximately 1/4 mile west of the project site. No project related impacts are anticipated.

- b) **No Impact.** Although the project site has been disturbed by past activities, the depth of previous ground disturbance is unclear. Based on the level of sensitivity of the project vicinity, there is potential to uncover subsurface archaeological remains during future project construction. Additionally, because the consultation process with Native American Tribes is ongoing, and because the City of Coachella has received regular correspondence requesting consultation from the Twentynine Palms Band of Mission Indians who have trust land territory in the vicinity of the site, the owner may be required to hire a Native American grading monitor during all future grading and excavation activities on the site, in coordination with the Twentynine Palms Band of Mission Indians. However, as previously stated, there are no development proposals attached to this Change of Zone and therefore no impact to any archaeological resources at this time.
- c) **No Impact.** The project site is designated as a site with "Undetermined Sensitivity" for paleontological resources on the Paleontological Resources Sensitivity Map (2035 General Plan, Figure 4.4-3). The project site is not known to contain unique paleontological or geologic features. Additionally, the site has been developed for a number of years, and surfaces scraped or covered with concrete, asphalt or buildings. No adverse impacts resulting from the project are anticipated.
- d) **No Impact.** No cemeteries or human remains are known to occur onsite. The site is currently developed and it is unlikely that human remains will be uncovered during project development. Nonetheless, the discovery of human remains is always a possibility during ground disturbances. State of California Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the Riverside County Coroner has made a determination of origin and disposition pursuant to PRC Section 5097.98. Should remains be uncovered during grading of the site, California law requires that all activity stop, that the coroner be notified, and that he or she determine the nature of the remains, and whether additional Native American consultation will be required. However, as previously stated, because consultation with Native American Tribes is an ongoing process, and because there are no development proposals attached to this Change of Zone and therefore no impact to any human remains or other resources at this time.

Mitigation Measures

None



VI. GEOLOGY AND SOILS

Would the project:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: The City of Coachella 2035 General Plan, 2015)

Setting

The project site is located within the Salton Trough that is characterized as a northwest-southeast structural depression extending from the Gulf of California to the Banning Pass. The Salton Trough is dominated by several northwest trending faults, most notably the San Andreas Fault system. The Salton Trough is dominated by the Santa Rosa – San Jacinto Mountains on the southwest, the San Bernardino Mountains on the north, the Little San Bernardino – Chocolate –Orocopia Mountains on the east, and extends through the Imperial Valley into the Gulf of California on the south.

The Salton Trough is an internally draining area with no readily available outlet to the Gulf of California portions well below sea level. Between about 300AD and 1600 AD, the Salton Trough was inundated by the Colorado River’s water, forming ancient Lake Cahuilla. Since that time, the floor of the Trough has been repeatedly flooded with other “fresh” water lakes, the most recent being the current Salton Sea. The sole outlet for these waters is evaporation, leaving behind vast amounts of terrestrial sediment materials.



Discussion of Impacts

- a) **No Impact.** The subject property is not located in an Alquist-Priolo Earthquake Fault Zone. The earthquake faults nearest to the site of the proposed project which can generate earthquakes of magnitude > 5.5 are presented in **Table 5, Closest Known Active Faults**. Surface rupture is expected to occur along pre-existing, known active fault traces. However, surface rupture could potentially splay or step from known active faults or rupture along unidentified traces.

Table 1 Closest Known Active Faults

Fault Name	Distance (Km)	Maximum Event
San Andreas - Coachella	3.5	7.2*
San Andreas - Southern	3.5	7.2*
San Jacinto - Anza	36.7	7.2
Burnt Mountain	36.9	6.5
Eureka Peak	37.2	6.4
San Jacinto – Coyote Creek	39.7	6.8
San Andreas – San Bernardino	41.1	7.5*
Pinto Mountain	52.0	7.2
Source: Sladden Engineering, Geotechnical Investigation Proposed Cannabis Cultivation Complex, Table 1, July, 2016.		
Notes:		
* 8.2 for multiple segment rupture		

The project site is located in a seismically active region where earthquakes originating on local and regional seismic faults can produce severe ground shaking. The project site has been subjected to past ground shaking by faults that traverse through the region. Strong activity from nearby active faults is expected to produce strong seismic shaking during the design life of the proposed project.

The City of Coachella requires buildings proposed for the site to be constructed in accordance with the most recent edition of the California Building Code (CBC) and Coachella Municipal Code. In order to reduce impacts associated with ground shaking on people and buildings, the City implements the latest seismic safety design standards outlined in the 2013 edition of the California Building Code for Site Class D, as outlined in Appendix B, *Geotechnical Report*.

The City has adopted several modifications to the CBC in accordance to local geology. The Coachella Municipal Code provides regulations for collapse-resistant design, which will be enforced during structure design and construction. Therefore, project-related impacts associated with seismic ground shaking are less than significance.

The project site is located in an area that has a high susceptibility to liquefaction (2035 General Plan EIR Figure 4.5-5). For liquefaction to occur, groundwater levels must be within 50 feet of the ground surface. Therefore, fine-grained sand in this region may be prone to consolidation under building loads and severe ground shaking.



The project site lies in a Coachella Valley floor, which is just outside the landslide and rockfall hazard area (2035 General Plan EIR Figure 4.5-6). The site is situated on relatively level ground and is not immediately adjacent to any slopes or hillsides that could be potentially susceptible to slope instability. There are no signs of slope instability were observed at or near the proposed project site. Therefore, no impacts associated with landslides are anticipated.

- b) **Less Than Significant Impact.** The Coachella Valley floor, including the subject property, is susceptible to wind erosion (Riverside County General Plan Figure S-8). Future development projects that require demolition of the existing buildings and structures, construction, and other ground disturbances by heavy machinery that could result in the loss of some topsoil and generate particulate matter. Grading and construction will also require removal of the topsoil; however, project-related impacts are expected to be less than significant. The project will be required to implement measures to control fugitive dust under ASCQMP Rule 403.1 (see Air Quality, Section III), which will minimize potential adverse impacts associated with soil erosion. No impacts are anticipated at this time.
- c) **No Impact.** In arid climatic regions, granular soils have potential to collapse upon wetting. This collapse (hydro-consolidation) phenomena is the result of the lubrication of soluble cements (carbonates) in the soil matrix causing the soil to densify from its loose configuration during deposition. The site is not susceptible to landslides due to its relatively flat terrain and distance from mountainous slopes. Although tectonic subsidence has been documented in the Coachella Valley, it is not known to occur in the project vicinity. There are no impacts associated with soil instability at this time.
- d) **No Impact.** Expansive soils typically contain large amounts of clay that expand when water is absorbed and shrink when they dry. As there are no development proposals at this time, there is no risk of volumetric changes in the subgrade soils.
- e) **No Impact.** The proposed project will connect to the City's existing sewer system. No alternative wastewater disposal systems are proposed. No adverse impacts associated with wastewater disposal systems will occur.

Mitigation Measures

none



VII. GREENHOUSE GAS EMISSIONS

Would the project:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Generate greenhouse gas emissions, either directly or indirectly, that may have significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: 2035 General Plan, 2015; City of Coachella Climate Action Plan, June 2014.)

Setting

Greenhouse gas emissions are generated by both moving and stationary sources, including vehicles, the production of electricity and natural gas, water pumping and fertilizers. State law mandates that all cities decrease their greenhouse gas emissions to 40 percent below 1990 levels by the year 2035.

Assembly Bill 32

In 2006, the California State Legislature adopted Assembly Bill 32 (AB 32), the California Global Warming Solutions Act of 2006. AB 32 requires CARB, to adopt rules and regulations that would achieve GHG emissions equivalent to statewide levels in 1990 by 2020 through an enforceable statewide emission cap which will be phased in starting in 2012.

Senate Bill 32

California Global Warming Solutions Act of 2006: Emissions Limit (SB 32), was adopted on September 8, 2016. SB 32 strengthens the greenhouse gas emissions reduction targets set under AB 32, requiring California to reduce its GHG emissions to 40% below 1990 levels by December 31, 2030.

Coachella Climate Action Plan (CAP)

The City of Coachella adopted a Climate Action Plan (CAP) as part of the General Plan Update in 2015. The goal of the CAP is to reduce GHG emissions within City operations and throughout the community. The CAP GHG inventory was prepared using the Clean Air and Climate Protection (CACP) Software, which is the industry standard for climate action planning developed by Local Governments for Sustainability (also known as ICLEI, International Council for Local Environmental Initiatives). Various utility and service companies provided City-specific data for the years 1990, 2005, and 2010, which were used to populate the CACP software. This data includes electricity usage, natural gas usage, waste treatment, solid waste disposal, public transit data, and estimated citywide vehicle miles traveled.

Discussion of Impacts

a-b) No Impact. The proposed project will not generate additional greenhouse gas (GHG) emissions as there are no development proposals associated with this Change of Zone. For future development proposals, is anticipated that CalEEMod model will be used to quantify air quality emission projections, including greenhouse gas emissions. Construction related greenhouse gas emissions (if any) will be temporary and will end once the project is completed. Operation of the



proposed project may create on-going greenhouse gas emissions through area source emissions, such as vehicle trips, landscaping and off-gassing from the pavement.

It is recognized that GHG impacts are intrinsically cumulative. Future project construction will be conducted in a manner that is consistent with applicable rules and regulation pertaining to the release and generation of GHG's. The emission of GHG generated as a result of the proposed project will have a less than significant impact on the environment and will not conflict with any applicable GHG plans, policies or regulations.

Additionally, the proposed project will be required to comply with Emission Reduction Strategies in Chapter 4 of the CAP. Reduction strategies include: state programs, General Plan goals and policies, and additional measures associated with the CAP. These policies include a requirement for the project to install a solar photovoltaic system for all buildings capable of generating 20% of the energy demands for a typical light industrial use. This requirement will be included in the project's standard conditions of approval. After 2020, industrial projects will be required to generate up to 50% of the typical light industry energy demand with solar power. By 2035, the combination of state and local greenhouse gas emissions measures is likely to reduce emissions by 639,630 MTCO₂e. Through a combination of proposed federal, state, and city-level General Plan policies, Coachella can anticipate emissions reductions of 338,046 MTCO₂e per year from the business-as-usual scenario in 2020. These actions put Coachella 43 percent below 2035 business-as-usual emission levels. Therefore, compliance with the CAP will put the proposed project on track to achieve the greenhouse gas reduction target adopted under SB 32, and impacts will be less than significant.

Mitigation Measures

None.



VIII. HAZARDS AND HAZARDOUS MATERIALS

Would the project:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: City of Coachella, General Plan Update EIR. July 2014)

Setting

The Property lies within the north area of Coachella, south of Avenue 50, east of Highway 111 and west of Highway 86 and the Coachella Valley Stormwater Channel. Immediately west of the project site is the Southern Pacific Railroad with Highway 111 adjacent west. The Cabazon Indian Reservation has significant parcels north of the project site. Access to the project site is from Avenue 50 and Tyler Street. The property is graded flat and is partially improved for industrial agriculture operations.



Discussion of Impacts

- a-b) No Significant Impact.** There are no development proposals attached to this application for Zone Change, therefore the proposed project will not result in the development of any additional facilities. Future projects may use cleaners, solvents, fertilizers and pesticides may be used on-site for routine cleaning and cultivation. However, none of these will be used in sufficient quantities as to pose a threat onto humans or to cause a foreseeable chemical release into the environment.

Future project construction may involve the use of heavy equipment to be provided with fueling, oiling, lubricating and other potential flammable substances that could lead to fuel and oil spills. The contractor will be required to identify in their Storm Water Pollution Prevention Plan (SWPPP) a staging area for storing of these materials.

The proposed project would not result in a risk of explosion or accidental release of hazardous substances. The use and handling of hazardous materials during construction activities would occur in accordance with applicable Federal, State, and local laws including California Occupational Health and Safety Administration (CalOSHA) requirements. Impacts will be less than significant.

- c) No Impact.** There are no schools located within 0.25 miles of the project site. The nearest schools are Valle Del Sol Elementary School located approximately ½ mile east of the proposed site and Bobby Duke Middle School located approximately ½ mile west of the proposed site. There will be no hazardous materials-related impacts to schools.

- d) No Impact.** As there are no development proposal associated with this Change of Zone, no information was obtained from the following databases:

1. Enforcement and Compliance History Online (ECHO),
2. Enforcement Action Listing (ENF),
3. Hazardous Waste Information System (HAZNET)
4. National Pollutant Discharge Elimination System (NPDES), and
5. Environmental Data Resources, Inc. Historical Auto database (EDR Hist Auto).

- e-f) No Impact.** The Jacqueline Cochran Regional Airport is located approximately 4.0 miles southeast of the project site and is not located within the boundaries of the airport's land use compatibility plan. The project site is not located in the vicinity of a private airstrip. The project will not result in safety hazards for people living or working in the area.

- g) No Impact.** The proposed project will not alter the existing circulation pattern in the project area or adversely impact evacuation plans. Main access will be from Avenue 50 and Tyler Street. Project specific emergency access will be provided based upon future development requirements.

Proposed parking and circulation plans will be reviewed by the Fire and Police Departments to assure that the project's ingress/egress driveways and roads are adequate for accommodating emergency vehicles. A construction traffic plan will be required to be submitted to the Fire Department for review prior to development in order to assure that the project does not interfere with emergency access during development.



- h) No Impact.** The project area is not near any wildlands and is not susceptible to wildland fires; therefore, the project would not expose people or structures to significant injury, loss or death due to wildfires.

Mitigation Measures

None



IX. HYDROLOGY AND WATER QUALITY

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned storm-water drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (Source:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Inundation by seiche, tsunamis, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: 2035 Coachella General Plan; City of Coachella, 2015 Urban Water Management Plan)



Setting

The Coachella Water Authority (CWA) provides domestic water to the project area. The CWA operates a water supply, storage, and delivery system consisting of wells, reservoirs, booster stations, and distribution lines. Currently, groundwater for the city lies in a Whitewater River Watershed, which also supplies water to other Coachella Valleys' cities. The City's wells draw from the Coachella Valley Groundwater Basin. The six groundwater wells that currently supply water to the City are capable of providing 1,200 to 2,500 gallons per minute (gpm) each. The well water production reports recorded 7,330 acre-feet pumped in 2010, or approximately 11 million gallons per day (mgd) on average.

No onsite drainage has been developed on the existing project site. Storm flows currently drain in a northwest-to-southeast fashion via sheet flow. The project site and areas surrounding it are subject to City requirements relating to flood control. The City implements standard requirements for the retention of storm flows, and participates in the National Pollution Discharge Elimination System (NPDES) to protect surface waters from pollution.

Discussion of Impacts

- a) **No Impact.** The project site is located in the Whitewater River Watershed Municipal Storm-water Management Area, which is regulated by the Riverside County Flood Control and Water Conservation District to monitor water quality standards. The CWA is required to meet water quality requirements in its production and delivery of domestic water. The Coachella Sanitary District (CSD) is also regulated by the Board and must maintain strict water quality standards in the treatment of effluent. Wastewater will be transported to and processed at the Wastewater Treatment Plant (WWTP) located in the southern most portion of the City on Avenue 54.

With regards to storm-water runoff, any proposed development project must include implementation of a Storm Water Management Plan (SWPPP) for the evaluation of construction and operations impacts. Prior to site disturbance, the applicant must apply to the State Water Resources Control Board for coverage under the Construction General Permit (Order No. 99-08-DWQ) (CAS000002), which applies to all storm-water discharges from projects where clearing, grading, and excavation result in soil disturbance of at least one acre or more. Likewise, during operation of both the residential and commercial components of the project, the operator(s) must comply with an approved Water Quality Management Plan (WQMP) that sets forth the requirements for capturing, retaining and treating storm-water and nuisance water from the site. In addition, the CSD will impose conditions of approval requiring compliance with its regulations and standards as they relate to the release of fertilizers or pesticides which may be released by the facility in its cultivation practices. The imposition of conditions of approval, local, state, and federal standard requirements and the requirements of law will assure that impacts associated with water quality standards are not significant.

- b) **No Impact.** Water will be required during both construction (short-term) and operation (long-term). The City of Coachella does not currently have water demand factors. However, the Coachella Valley Water District has developed demand factors by land use that the City has deemed appropriate for future analysis.

Future development projects will be required to comply with the City's water demand management measures incorporated in the 2015 Urban Water Management Plan, including the



installation of water-efficient irrigation systems. Implementation of these and other applicable requirements will assure that water-related impacts are reduced to less than significant levels.

- c-e) No Impact.** The subject site is generally flat and contains no rivers or streams. Future development projects will be required to comply with the City's requirements as they relate to storm water retention, including the preparation of a project-specific hydrology study and water quality management plan. Implementation of onsite drainage improvements and applicable requirements will ensure that drainage and storm-water will not create or contribute water which would exceed the capacity of existing or planned storm-water drainage systems or provide substantial additional sources of polluted runoff.
- f) No Impact.** Future proposed projects will be required to comply with all applicable water quality standards. Future projects will connect to the City's sewer system and the proposed subsurface storm-water conveyance system will help minimize potential water quality degradation. Adherence to the City's standard requirements related to water quality will ensure impacts will be less than significant.
- g-h) No Impact.** The project site is not located in the 100-year floodplain and will not place housing or other structures in an area that would impede or redirect flows (General Plan 2015 Figure 4.7-2). According to Flood Insurance Rate Maps (FIRM) prepared by the Federal Emergency Management Agency (FEMA), the site is located in Zone X, which represents "areas outside of 0.2% annual chance flood. More specifically, the site is within an area of reduced flood risk due to levee. (FIRM Map No. 06065C2260G, August 28, 2008). There will be no impact.
- i) No Impact.** The subject property lies approximately 0.2 miles west of Coachella Valley Storm-water Channel/Whitewater River (General Plan Figure 4.7-2). It is required by the California State Water Code to be monitored for structural safety, and potential risks and planned responses associated with reservoir failure are addressed in the City's Local Hazard Mitigation Plan. Therefore, impacts to the subject property are considered to be less than significant.
- j) No Impact.** The proposed project site is not located in the vicinity of a body of water that can produce seiche, tsunami, or mudflow. There will be no impact.

Mitigation Measures

None.



X. LAND USE AND PLANNING

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: City of Coachella 2035 General Plan, 2015; Coachella Municipal Code)

Setting

On-site and Vicinity Land Uses

As described in the Project Description provided earlier in this document, and as graphically portrayed in previous Figures 3 thru 5, the site of the proposed project has been previously developed as an industrial agriculture facility with a number of associated and ancillary uses.

Lands surrounding the project site are either occupied by uses similar in type to those currently found on the site of the proposed project (particularly to the west), or in agriculture (east of the existing industrial facilities). There are established residential neighborhoods to the north and east of the project site.

General Plan Land Use and Zoning Designations

Future development of the project site and all lands within the City of Coachella are subject to: 1) land use and other development related goals and policies contained in the recently adopted City of Coachella General Plan, 2035; and 2) the codified regulations, standards and other criteria provided in the Coachella Municipal Code. The current General Plan Land Use designation for the project site is defined by Subarea 6 – Downtown Expansion. This General Plan land use designation allows for a broad range of light and heavy commercial and industrial land uses as well as a variety of urban and general residential land uses. The project site is currently zoned M-S and R-S, which is inconsistent with the General Plan Land Use and Zoning designations (see previous Figures 6 and 7).

Discussion of Impacts

- a) **No Impact.** The project site is currently developed and is located in an area of like commercial, industrial, and residential land uses. The surrounding land uses operate independently of one another and the current uses on the project site. The proposed project will not physically divide any established communities. Given the foregoing, the proposed project will not result in any impacts to Land Use in this regard and no further assessment of this issue is warranted.



- b) **No Impact.** The subject property occurs in an area designated for mixed use Industrial, commercial and residential development in the General Plan. A Conditional Use Permit and Variance required by the City of Coachella for certain proposed commercial and industrial uses. For future proposed development, all activities will be conducted pursuant to the City's Municipal Code requirements and standards to avoid any conflict with any land use plan, policy, or regulation. As a consequence, no impact is expected.
- c) **No Impact.** As described earlier in this document under Biological Resources, the project site is located within the boundaries of the Coachella Valley Multiple Species Habitat Conservation Plan (MSHCP). However, since the project site is currently developed, the proposed new development on the subject site is not required to perform preconstruction surveys for burrowing owl or pay the MSHCP Local Development Mitigation Fee. No conservation plan-related conflict is expected.

Mitigation Measures

None.



XI. MINERAL RESOURCES

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015; California Division of Mines and Geology, 1987)

Setting

Existing or potential mineral resources in the City include sand and gravel, clay, oil and gas, and geothermal. The project site is located in the City’s industrial core. The proposed development will have no impact on mineral resources.

Discussion of Impacts

a-b) No Impact. In the General Plan (2015), the project site is designated as Mineral Zone MRZ-1, which indicates an area where available geological information indicates that little likelihood exists for presence of significant mineral resources; however the significance of these deposits cannot be evaluated from available data (Riverside County 2003).

The project site occurs in an industrial setting and is not designated for mineral resource land uses. Also, it has been previously developed and the proposed project would not result in the loss of available known mineral resources. No project-related impacts to mineral resources are anticipated.

Mitigation Measures

None.



XII. NOISE

Would the project result in:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015; "Riverside County Airport Land Use Compatibility Plan, Volume 1, Policy Document," adopted by Riverside County Airport Land Use Commission, October 14, 2004.)

Setting

The project site is south of Avenue 48, a major roadway as defined by the City's General Plan Circulation Element. However, due to the close proximity Highway 111, and the nearby 86-S Expressway, the noise levels may be more intensive than typical streets.

Discussion of Impacts

- a) **No Impact.** Any future development proposals will be required to comply with the City of Coachella' Noise Control Ordinance Chapter 7.04.070. The main noise source is vehicular traffic on adjacent roadways (Avenue 50, Tyler Street, Hwy 111). The nearest sensitive receptors are single-family residences located west of the project site.
- b, c) **No Impact.** Ground-borne vibration and/or ground-borne noise may be generated during construction of future projects, which could be felt by adjacent land uses. The primary source of ground-borne vibration will be operation of heavy equipment, such as bulldozers; however, the impacts will be temporary and will end once construction is complete. Long-term operation of the project is not expected to generate ground-borne vibrations or noise.



- d) **No Impact.** For future development projects, during the construction phase of the proposed project, noise will be generated by heavy demolition and construction machinery and equipment, such as dozers, loaders, scrapers, and dump trucks. Short-term impacts could temporarily exceed acceptable noise thresholds.

The City will require that construction activity comply with Section 7.04.070 of the Municipal Code, which limits construction activity to between 7 a.m. to 7 p.m. on weekdays and 8 a.m. to 5 p.m. on Saturdays. No activity is permitted on Sundays and holidays. These requirements will reduce noise impacts to less sensitive daytime hours and assure that short-term construction noise impacts will be less than significant. However, these impacts will be short-lived and temporary. As there is no development proposal attached to this Change of Zone, no impacts are anticipated.

- e-f) **No Impact.** The Bermuda Dunes Airport (UDD) is located approximately 7 miles northwest of the subject property and its noise contours are not located in the vicinity of the proposed project site. No impacts will occur.



XIII. POPULATION AND HOUSING

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Source: Coachella 2035 General Plan, 2015)

Setting

The City of Coachella had a population of 45,407 as of in January 1, 2016, and an average household size of 4.68 persons, based on the E-5 Population and Housing Estimates prepared by the California Department of Finance. The City is composed of a mix of single family and multi-family development, but the majority (71%) of housing units are single family homes.

As there is no development proposed at this time, there will be no impact on population and housing. When the site is developed as proposed by the General Plan, it may result in a need for additional employees for each phase of development. Employment generation is likely to be filled by existing residents, or new residents to the area who will move based on anticipated growth rates.

Discussion of Impacts

- a) **No Impact.** At this time, the proposed project will not generate substantial population growth in the area. Any new employment generation is likely to be filled by existing residents, or new residents to the area who will move based on anticipated growth rates. The proposed project will improve Avenue 50 and Tyler Street along project boundaries, however improvements are consistent with General Plan roadway classification and will not result in the extension of infrastructure that will generate new development. Therefore, any future impacts will be less than significant.
- b-c) **No Impact.** There are no residential units on site or in proximity to the subject property. No residents will be displaced, and no replacement housing will be required elsewhere.



XIV. PUBLIC SERVICES

	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project result in:				
Substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
a) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015, Coachella Website, Development Impact Fees, Accessed November 13, 2016, <http://www.coachella.org/Home/ShowDocument?id=2732>.)

Discussion of Impacts

a) No Impact. The City contracts with Riverside County Fire Department (RCFD) for local fire protection service. The nearest fire station is Station 79 located at 1377 Sixth Street, approximately ½ mile east of the site.

For future development, the Fire Department will review the project site plan to ensure it meets applicable fire standards and regulations. No construction of new or expanded fire services or facilities are required for the proposed project. Nonetheless, the project applicant will be required to pay development impact fees to support future development and maintenance of fire protection services. Therefore, with Fire Department review and payment of the Development Impact Fees, any future impacts to fire protection services will be less than significant.

b) No Impact. The Riverside County Sheriff’s Department is contracted to provide comprehensive law enforcement services through the City of Coachella Police Department. Local headquarters is located at 82-625 Airport Boulevard, approximately 3.0 miles southeast of the subject property. The nearest police station is the Indio Police Department, which is located at 46800 Jackson Street, Indio.

Future development proposals will be required to pay development impact fees to support future development and maintenance of police protection services and to comply with all Police Department regulations and procedures. Therefore, with payment of the Development Impact Fees, impacts to police protection services will be less than significant.



- c) **No Impact.** The proposed project will not require the construction of a new school facility. However, future development proposals may be required to pay a Level II Fee for commercial industrial development to the Coachella Valley Unified School District, to support development of future facilities due to development within the City. Payment of necessary development fees to CVUSD will ensure that development of the project will not significantly impact school services.
- d-e) **No Impact.** The project is not expected to impact local and/or regional parks. No additional public facilities are required for the proposed project to accommodate the employees. Nonetheless, future development proposals will be required to pay development impact fees to support future development and maintenance of miscellaneous public services. Therefore, with payment of the Development Impact Fees, impacts to public services will be less than significant.



XV. RECREATION

Would the project:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015)

Setting

The City operates eight public parks around the City. The City’s largest park is Bagdouma Park, located approximately 1 mile west of the site, which consists of 34 acres and has the following amenities; baseball/softball fields, tennis courts, a swimming pool, restrooms, a pavilion and playground, tables and benches, bleachers, parking, open grass, soccer/football fields, basketball courts, and drinking fountains. The City also operates a skate park at the southwest corner of Bagdad Avenue and Shady Lane, approximately 1.5 miles southwest of the site.

Discussion of Impacts

a-b) No Impact. The project does not include any development proposal and is therefore not growth-inducing and would not increase the use of existing neighborhood or regional parks or other recreational facilities, nor will it require the construction or expansion of recreational facilities. Future development proposals that may include any residential development will require that the applicant payment Development Impact Fees (DIF) associated with park improvements. However, at this time, the proposed Project will have no impact on recreational facilities within the City of Coachella.

Mitigation Measures

None.



XVI. TRANSPORTATION/TRAFFIC

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015; Coachella General Plan Update EIR, Traffic Section, 2013)

Setting

The project site is located south of Avenue 50 and west of Tyler Street, in the City’s mixed use district shown as SubArea 6 in the General Plan. This area is proposed for a wide variety of employment and residential uses.

As there is no development proposal attached to this application, an analysis was not conducted to analyze potential impacts to transportation and traffic. Future traffic analysis should be based upon a variety of sources, including the General Plan Circulation Element.

Discussion of Impacts

a-b) No Impact.

Existing Conditions

Existing roadways in the vicinity of the project site include Grapefruit Boulevard/State Route 111, State Route 86, Tyler Street, and Avenue 50.

Future Traffic Impacts

Future project proposals, which should be consistent with the General Plan and surrounding land uses will be required to provide average daily traffic estimates pursuant to the General Plan EIR.



- c) **No Impact.** The Bermuda Dunes Airport (UDD) is located approximately 7.0 miles northwest of the subject property. The project will have no impact on the facilities or operations of regional airports and will not result in a change in air traffic patterns.
- d) **No Impact.** Future projects will be developed in accordance with City design guidelines and will not create a substantial increase in hazards due to a design feature. The project's access points will be located with adequate sight distances, and project-generated traffic will be consistent with existing traffic in the area. Furthermore, the City of Coachella will perform a plan check of the proposed site plan to ensure that all design features are consistent with City guidelines. Therefore, no project related impact is anticipated.
- e) **No Impact.** Future emergency access for specific development projects will be provided as required by local development regulations. Regional access to the project site will be provided via major arterials, secondary arterials and a variety of local roads. Prior to construction, both the Fire Department and Police Department will review the project site plan to ensure safety measures are addressed, including emergency access. Therefore, the future projects will not result in inadequate emergency access.
- f) **No Impact.** SunLine Transit Agency provides bus transit services throughout the Coachella Valley, including the City of Coachella and proposed project site. SunLine operates an existing bus route on Grapefruit Boulevard and Avenue 50, with a bus stop located approximately 0.33 miles west of project site. Future residents and employees will have access to existing bus service.

The General Plan encourages pedestrian friendly amenities in the private drive/private street design of new industrial parks. The proposed project will incorporate numerous walkways with parking lot shade trees throughout the various portions for the new industrial buildings. Therefore, the project is not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities. No project related impact is anticipated.

The Coachella General Plan identifies bicycle facilities along Avenue 50 connecting to the proposed CV Link. As this area of Coachella continues to grow as a residential and employment additional bicycle, walking and NEV facilities may be added to serve the area. To promote alternative transportation, future projects may be conditioned to provide a designated and secure location for bicycle storage.

Mitigation Measures

None.



VIII. UTILITIES AND SERVICE SYSTEMS

Would the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

(Sources: Coachella 2035 General Plan, 2015; City of Coachella 2015 Sewer Master Plan, June 2015; Coachella General Plan Update Draft EIR, 4.16 Water Supply and Wastewater, 4.14 Public Utilities, 2014.)

Setting

Water

The Coachella Water Authority (CWA) provides domestic water to the project area. The CWA operates a water supply, storage, and delivery system consisting of wells, reservoirs, booster stations, and distribution lines. Currently, groundwater. The City lies in a Whitewater River Watershed, which also supplies water to other Coachella Valleys' cities. The City's wells draw from the Coachella Valley Groundwater Basin. The six groundwater wells that currently supply water to the City can provide 1,200 to 2,500 gallons per minute (gpm) each. The well water production reports recorded 7,330 acre-feet pumped in 2010, or approximately 11 million gallons per day (mgd) on average.

Wastewater

The Coachella Sanitary District (CSD) is the primary service provider for the incorporated area of the City. The CSD was established in 1936 and maintains approximately 340,000 linear feet of wastewater conveyance pipeline, powered by two pump stations and serviced by a 4.9 MGD capacity wastewater treatment plant (WWTP) and currently serves an average daily demand of approximately 2.9 MGD. The



Coachella Sanitary District's sole WWTP is in the southern most portion of the City on 54th Avenue. This location takes advantage of the natural gravity flow that runs from north to southeast.

Solid Waste

Burrtec provides the City with solid waste collection services. Burrtec provides curbside pickup for regular trash, green waste and recyclables and offers bulky Item Pick-up, Christmas Tree Recycling, Electronic Waste, and Used Motor Oil Collection upon request. Solid waste that is collected from the City is routed to the Coachella Transfer Station, operated by Burrtec, located north of Interstate 10 and east of Dillon Road on Landfill Road. Waste is then routed to one of the County's six landfills which are operated by the Riverside County Waste Management Department.

Discussion of Impacts

- a) **No Impact.** Future development projects will be served by the CSD for wastewater treatment services. Disposal of wastewater from operations and employee sanitary and break facilities to the City's existing wastewater system. CSD is required to comply with all applicable Regional Water Quality Control Board requirements, which will ensure that impacts associated with wastewater treatment requirements are less than significance.

With regard to stormwater runoff, the future projects must include implementation of a SWPPP for evaluation of construction impacts, and will retain on-site the stormwater of a 100-year storm event. Prior to site disturbance, the applicant must apply to the State Water Resources Control Board for coverage under the Construction General Permit (Order No. 99-08-DWQ) (CAS000002), which applies to all stormwater discharges from projects where clearing, grading, and excavation result in soil disturbance of at least one acre or more. Likewise, during operation of both the residential and commercial components of the project, the operator(s) must comply with an approved WQMP that sets forth the requirements for capturing, retaining and treating stormwater and nuisance water from the site.

- b) **No Impact.** Sanitary sewage from future developments will be connected to a future sanitary sewer lines located east of the project site. The wastewater will then be transported to the Waste Water Treatment Plant (WWTP) on Avenue 54. No new wells or additional water or sewer infrastructure or entitlements will be required.
- c) **No Impact.** Storm water drainage infrastructure within the City consists of a network of natural and improved streams, storm drains, storm channels, and catch basins intended to manage stormwater that flows into Whitewater Storm Water Channel. Existing drainage onsite runs in a northwest-to-southeastern fashion. Future onsite drainage improvements must include the installation of a series of catch basin/inlets and storm drain piping that will collect and convey the site runoff to a proposed underground retention storage facility.
- d) **No Impact.** Coachella Water Authority (CWA) will provide domestic water services to future development projects. The City of Coachella does not currently have water demand factors. However, the Coachella Valley Water District has developed demand factors by land use that the City has deemed appropriate for this analysis. Future projects are expected to have a water demand considerably less than that of typical land uses in the area, and impacts associated with domestic water demand are expected to be less than significant.



- e) **No Impact.** The Coachella Sanitary Division WWTP has a capacity of 4.9 MGD after the completion of its Phase 2 expansion in 2012. The WWTP currently treats approximately 2.9 mgd of wastewater, resulting in an available capacity of approximately 2 mgd. The Coachella WWTP implements all requirements of the Regional Water Quality Control Board (RWQCB), State Water Resource Control Board and City of Coachella 2015 Sewer System Master Plan pertaining to water quality and wastewater discharge.
- f-g) **No Impact.** Future development projects may generate solid waste associated with demolition, construction and operation. Trash and recyclables will be transported to the Coachella Valley Transfer Station (CVTS) for sorting and processing by Burrtec. Some of this material such as dimensional lumber can be diverted to the Coachella Valley Compost (CVC) facility, near the CVTS, for composting.

Additionally, facility operators, such as Burrtec, are required to meet all local, regional, state, and federal standards for solid waste disposal. Compliance with applicable regulations will further reduce the project's impacts on solid waste generation. Therefore, impacts associated with solid waste disposal from future development are expected to be less than significant.



XIX. MANDATORY FINDINGS OF SIGNIFICANCE

NOTE: If there are significant environmental impacts which cannot be mitigated and no feasible project alternatives are available, then complete the mandatory findings of significance and attach to this initial study as an appendix. This is the first step for starting the environmental impact report (EIR) process.

Does the project:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Setting

The environment setting for the project site is summarized within Section I through XVIII of the Initial Study for each environmental issue.

Discussion of Impacts

a, b, c) No Impact.

As previously discussed, this Change of Zone (CZ 18-10) is intended to bring this property into conformance with the provisions of the land use element in the City of Coachella 2035 General Plan. This project relies primarily on the information available in the General Plan and associated Environmental Documents for support and approval. However, future development proposals are in no way exempt from additional review pursuant to the City of Coachella's Zoning and Development Code and the California Environmental Quality Act.

Mitigation Measures

None



References

Aesthetics

- City of Coachella 2035 General Plan, 2015.
- Coachella Municipal Code.
- “California Scenic Highway Mapping System,” California Department of Transportation

Agriculture and Forest Resources

- 2035 Coachella General Plan, Figure 3-6, 2015.
- “Riverside County Important Farmland 2014 Map,” sheet 2 of 3, California Department of Conservation, updated November 2016.

Air Quality

- City of Coachella 2035 General Plan, 2015.
- SCAQMD, 2016 Air Quality Management Plan, Revised October 7, 2016.

Biological

- City of Coachella 2035 General Plan, 2015
- “Coachella Valley Multiple Species Habitat Conservation Plan,” 2007

Cultural Resources

- City of Coachella 2035 General Plan, 2015

Geology and Soils

- The City of Coachella 2035 General Plan, 2015

Greenhouse Gases

- The City of Coachella 2035 General Plan, 2015.
- City of Coachella Climate Action Plan, June 2014.

Hazards and Hazardous Materials

- Riverside County Airport Land Use Compatibility Plan Policy Document, Jacqueline Cochran Regional Airport chapter, Amended September 2006.
- City of Coachella, General Plan Update EIR. July 2014

Hydrology and Water Quality

- Flood Insurance Rate Map #06065C2260G, Federal Emergency Management Agency, August 28, 2008.
- The City of Coachella 2035 General Plan, 2015
- Coachella General Plan Update Draft EIR, 4.16 Water Supply and Wastewater, 2014.

Land Use and Planning

- City of Coachella 2035 General Plan, 2015.
- Coachella Municipal Code.

**Mineral Resources**

- City of Coachella 2035 General Plan, 2015.
- California Division of Mines and Geology, 1987

Noise

- The City of Coachella 2035 General Plan, 2015.
- "Riverside County Airport Land Use Compatibility Plan, Volume 1, Policy Document," adopted by Riverside County Airport Land Use Commission, October 14, 2004.

Population and Housing

- City of Coachella 2035 General Plan, 2015.

Public Services

- Coachella 2035 General Plan, 2015, Coachella Website, Development Impact Fees

Recreation

- The City of Coachella 2035 General Plan, 2015

Transportation/Traffic

- City of Coachella 2035 General Plan 2015.
- Coachella General Plan Update Draft EIR, 4.9 Circulation, June 2014

Tribal Cultural Resources

- City of Coachella, General Plan Update EIR, Cultural Resources section, July, 2014.

Utilities and Services

- Coachella 2035 General Plan, 2015; City of Coachella 2015 Sewer Master Plan, June 2015.
- Coachella General Plan Update Draft EIR, 4.16 Water Supply and Wastewater, 2014.
- Coachella General Plan Update Draft EIR, 4.14 Public Utilities, 2014.



TWENTY-NINE PALMS BAND OF MISSION INDIANS

Item 15.

46-200 Harrison Place . Coachella, California . 92236 . Ph. 760.863.2444 . Fax: 760.863.2449

September 20, 2019

Luis Lopez, Development Services Director
City of Coachella – Development Services Department
1515 Sixth Street
Coachella, CA 92236

RE: Request to Consult on the Zona Central Re-Zoning Project in Coachella, California

Dear Mr. Lopez:

This letter in regards to a Notice of Intent to Adopt a Negative Declaration for a Change of Zone No. 18-10. This Change of Zone entails an amendment to the Official Zoning Map by changing the zoning on approximately 206.2 acres of vacant agricultural land in the R-S (Residential Single-Family) and partially-developed land in the M-S (Manufacturing Service) zone to provide consistency with the policies of the City of Coachella General Plan 2035.

After review of the project area the Tribal Historic Preservation Office (THPO), established to protect and preserve cultural resources that are within the ancestral territory of the Chemehuevi, has some concerns for the project. The project is within the Chemehuevi Traditional Use Area (TUA) and in the vicinity of known cultural resources. For this reason, the While portions of the project may have been disturbed, the THPO requests the completed cultural report for this project. After review of this report, the THPO may provide additional recommendations.

If you have any questions, please do not hesitate to contact the Tribal Historic Preservation Office at (760) 775-3259 or by email: TNPConsultation@29palmsbomi-nsn.gov.

Sincerely,

Anthony Madrigal, Jr.
Director of the Tribal Historic Preservation Office

cc: Darrell Mike, Twenty-Nine Palms Tribal Chairman
Sarah Bliss, Twenty-Nine Palms Cultural Resources Manager



AUGUSTINE BAND OF CAHUILLA INDIAN

Item 15.

PO Box 846 84-481 Avenue 54 Coachella CA 92236

Telephone: (760) 398-4722

Fax (760) 369-7161

Tribal Chairperson: Amanda Vance

Tribal Vice-Chairperson: William Vance

Tribal Secretary: Victoria Martin

October 8, 2019

Luis Lopez
City of Coachella
Development Services Department
1515 Sixth Street
Coachella, CA 92236

**RE: Project Title/Location: Zona Central- Downtown Expansion
South of Avenue 50, West of Tyler Street, Approximately ¼ north of Avenue 52
In the City of Coachella, CA**

Dear Mr. Lopez-

Thank you for the opportunity to offer input concerning the development of the above-identified project. We appreciate your sensitivity to the cultural resources that may be impacted by your project, and the importance of these cultural resources to the Native American peoples that have occupied the land surrounding the area of your project for thousands of years. Unfortunately, increased development and lack of sensitivity to cultural resources has resulted in many significant cultural resources being destroyed or substantially altered and impacted. Your invitation to consult on this project is greatly appreciated.

At this time, we are unaware of specific cultural resources that may be affected by the proposed project, however in the event you should discover any cultural resources during the development of this project please contact our office immediately for further evaluation.

Very truly yours,

Augustine Band of Cahuilla Indians

A handwritten signature in blue ink, appearing to read "Victoria Martin".

Victoria Martin
Tribal Secretary



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September 25, 2019

Mr. Luis Lopez
Director
Development Services Department
City of Coachella
1515 6th Street
Coachella, CA 92236

SUBJECT: Zona Central Project in Coachella, CA (CZ 18-10 & EA 18-04)

Dear Mr. Lopez:

On September 13, 2019, the Imperial Irrigation District received from the City of Coachella Development Services Department, a request for agency comments on the conceptual land use plan for the Zona Central project (Change of Zone No. 18-10, Environmental Assessment No. 18-04). The City of Coachella proposes to amend the official zoning map by changing the zoning on approximately 206.2 acres of vacant agricultural land and partially-developed land in the Manufacturing Service zone to provide consistency in the policies of the City of Coachella General Plan 2035 and specifically in its Land Use & Community Character Element and its Housing Element. The land is located south of Avenue 50, west of Tyler Street, approximately ¼ mile north of Avenue 52 in Coachella, CA.

The Imperial Irrigation District has reviewed the project information and has the following comments:

1. IID will not begin any engineering or estimate costs to provide electrical service for any project within the Zona Central area until the owner/developer submits a customer project application, detailed loading information, panel sizes, project schedule and estimated in-service date. The application is available for download at the district website <http://www.iid.com/home/showdocument?id=12923>.
2. However, based on the information provided, IID has performed a preliminary assessment of the development area and has determined that there will be a substantial impact to the IID electrical system as the area develops; the cumulative impact of office, R&D, retail and services uses and possibly a college or university, in addition to multi-family residential uses potentially located on the edges of the Zona Central area, significantly increase the electrical demand on district's system.
3. The environmental analysis in the Initial Environmental Study does not address, even conceptually, the level of adequacy of the existing electrical utility infrastructure to service the development area; neither in the *Project Description* section (Chapter 2, section 2.3) nor in the *Utilities and Services Systems* section (Chapter 3, section XVII) of the IES. Electrical service is a factor of utmost importance in the implementation and success of any new project. The deficient assessment of a project's potential impact on this resource

could adversely affect the capability of the district to provide electrical service in an efficient and timely manner.

4. Chapter 3, section XVII, titled *Utilities and Services Systems*, of the IES should be amended to reflect a "Potentially Significant Impact" with the following mitigation measures:
 - a) Based on preliminary information received, it is anticipated that the additional power load for the development of the Zona Central area will require the acquisition, design and construction of a substation, transmission and distribution line extensions, distribution getaways and distribution feeders all of which shall be at the expense of the developer. A minimally-dimensioned substation site of 315 ft. by 315 ft. will be required at a location approved by IID. Additional requirements include grading, fencing, applicable permits, zoning change, landscaping (if required by the City), and all rights-of-way and easements for the substation site and access rights for both ingress and egress, as well as for power line facilities (transmission and distribution), all of which shall be at the expense of the developer.
 - b) Underground distribution line extensions will be required to serve the Zona Central area as well, including such items as all rights-of-way and easements for the line extensions and any line relocations or reconfigurations needed, all of which shall be at the expense of the developer.
5. Once the district is provided with the required information as stated in comment no. 1, IID can carry out a thorough assessment to determine the specific requirements to supply electrical service to a project. Likewise, IID will determine the availability of temporary construction power from existing power lines based on construction schedules and or phasing.
6. For additional information regarding electrical service for a project, the applicant should be advised to contact the IID Energy - La Quinta Division Customer Operations, 81-600 Avenue 58 La Quinta, CA 92253, at (760) 398-5841 and speak with the project development planner assigned to the area.
7. It is important to note that IID's policy is to extend its electrical facilities only to those developments that have obtained the approval of a city or county planning commission and such other governmental authority or decision-making body having jurisdiction over said developments.
8. The applicant will be required to provide rights-of-way and easements for any power line extensions needed to serve the project.
9. Line extensions are made in accordance with IID Regulations:
No. 2 (<http://www.iid.com/home/showdocument?id=2540>),
No. 13 (<http://www.iid.com/home/showdocument?id=2553>),
No. 15 (<http://www.iid.com/home/showdocument?id=2555>) and
No. 20 (<http://www.iid.com/home/showdocument?id=2560>).
10. Any construction or operation on IID property or within its existing and proposed right of way or easements including but not limited to: surface improvements such as proposed

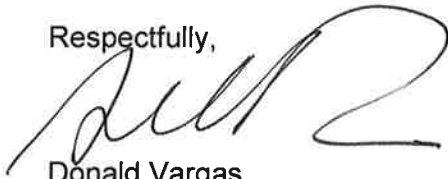
new streets, driveways, parking lots, landscape; and all water, sewer, storm water, or any other above ground or underground utilities; will require an encroachment permit, or encroachment agreement (depending on the circumstances). A copy of the IID encroachment permit application and instructions for its completion are available at <http://www.iid.com/departments/real-estate>. The IID Real Estate Section should be contacted at (760) 339-9239 for additional information regarding encroachment permits or agreements.

11. Relocation of existing IID facilities to accommodate the project and/or to accommodate street widening improvements imposed by the City will be deemed project-driven and all costs, as well as securing of rights of way and easements for relocated facilities, shall be borne by the applicant.
12. Public utility easements over all private and public roads and an additional ten (10) feet in width on both side of the private and public roads shall be dedicated to IID for the construction, operation, and maintenance of electrical infrastructure.
13. Any new, relocated, modified or reconstructed IID facilities required for and by the project (which can include but is not limited to electrical utility substations, electrical transmission and distribution lines, etc.) need to be included as part of the project's CEQA and/or NEPA documentation, environmental impact analysis and mitigation. Failure to do so will result in postponement of any construction and/or modification of IID facilities until such time as the environmental documentation is amended and environmental impacts are fully mitigated. **Any mitigation necessary as a result of the construction, relocation and/or upgrade of IID facilities is the responsibility of the project proponent.**
14. Dividing a project into two or more pieces and evaluating each piece in a separate environmental document (Piecemealing or Segmenting), rather than evaluating the whole of the project in one environmental document, is explicitly forbidden by CEQA, because dividing a project into a number of pieces would allow a Lead Agency to minimize the apparent environmental impacts of a project by evaluating individual pieces separately, each of which may have a less-than-significant impact on the environment, but which together may result in a significant impact. Segmenting a project may also hinder developing comprehensive mitigation strategies. In general, if an activity or facility is necessary for the operation of a project, or necessary to achieve the project objectives, or a reasonably foreseeable consequence of approving the project, then it should be considered an integral project component that should be analyzed within the environmental analysis. The project description should include all project components, including those that will have to be approved by responsible agencies. The State CEQA Guidelines define a project under CEQA as "the whole of the action" that may result either directly or indirectly in physical changes to the environment. This broad definition is intended to provide the maximum protection of the environment. CEQA case law has established general principles on project segmentation for different project types. For a project requiring construction of offsite infrastructure, the offsite infrastructure must be included in the project description. *San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus* (1994) 27 Cal.App. 4th 713.

15. Applicant should be advised that landscaping can be dangerous if items are planted too close to IID's electrical equipment. In the event of an outage, or equipment failure, it is vital that IID personnel have immediate and safe access to its equipment to make the needed repairs. For public safety, and that of the electrical workers, it is important to adhere to standards that limit landscaping around electrical facilities. IID landscaping guidelines are available at <https://www.iid.com/energy/vegetation-management>.

Should you have any questions, please do not hesitate to contact me at (760) 482-3609 or at dvargas@iid.com. Thank you for the opportunity to comment on this matter.

Respectfully,



Donald Vargas
Compliance Administrator II

Enrique B. Martinez – General Manager
Mike Pacheco – Manager, Water Dept.
Marilyn Del Bosque Gilbert – Manager, Energy Dept.,
Jamie Asbury – Deputy Manager, Energy Dept., Operations
Charles Berry – Mgr., Energy Dept., Distr. Services & Maintenance Operations
Enrique De Leon – Asst. Mgr., Energy Dept., Distr., Planning, Eng. & Customer Service
Vance Taylor – Asst. General Counsel
Robert Laurie – Outside Counsel
Michael P. Kemp – Superintendent, Regulatory & Environmental Compliance
Laura Cervantes – Supervisor, Real Estate

From: Markwardt, Nancy [mailto:nmarkwardt@cabazonindians-nsn.gov]
Sent: Monday, September 16, 2019 3:08 PM
To: Luis Lopez
Subject: Response to the Notice of Intent to Adopt a Mitigated Negative Declaration - Zona Central - Downtown Expansion

Greetings:

Thank you for reaching out directly to the Cabazon Band of Mission Indians. There is no presence of Native American cultural resources that may be impacted by your future project, the Zona Central – Downtown Expansion south of Avenue 50; west of Tyler Street which is approximately ¼ mile north of Avenue 52 in the City of Coachella, CA.

Good luck with your future project.

Nancy Markwardt
Recording Administrator/Office Manager
Cabazon Band of Mission Indians
84-245 Indio Springs Parkway
Indio, Ca 92203
Office: (760) 342-2593
Fax: (760) 347-7880

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